

PLANNING COMMISSION AGENDA

CHAIRPERSON:

Mary Beatie



VICE CHAIRPERSON:

Chris Tavaréz

COMMISSIONERS: Bill Davis, Charlie Norman, Adam Peck, Chris Tavaréz, Mary Beatie

MONDAY, MAY 12, 2025

VISALIA COUNCIL CHAMBERS

LOCATED AT 707 W. ACEQUIA AVENUE, VISALIA, CA

MEETING TIME: 7:00 PM

1. CALL TO ORDER –
2. THE PLEDGE OF ALLEGIANCE –
3. ROLL CALL –
4. CITIZEN'S COMMENTS – This is the time for citizens to comment on subject matters that are not on the agenda but are within the jurisdiction of the Visalia Planning Commission. You may provide comments to the Planning Commission at this time, but the Planning Commission may only legally discuss those items already on tonight's agenda.

The Commission requests that a five (5) minute time limit be observed for Citizen Comments. You will be notified when your five minutes have expired.
5. AGENDA COMMENTS OR CHANGES –
6. CONSENT CALENDAR - All items under the consent calendar are to be considered routine and will be enacted by one motion. For any discussion of an item on the consent calendar, it will be removed at the request of the Commission and made a part of the regular agenda.

7. PUBLIC HEARING – Cristobal Carrillo, Associate Planner

Conditional Use Permit No. 2024-46: A request by Maria Lomeli to establish three residential units within the D-MU (Downtown Mixed Use) Zone.

Environmental Assessment Status: The project is Categorically Exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15301, Categorical Exemption No. 2024-75.

Project Location: The project site is located at 405 North West Street (APN: 093-175-010).

8. PUBLIC HEARING – Josh Dan, Senior Planner

Conditional Use Permit 2025-11: A request to construct a fast-food restaurant measuring 2,003 square feet with drive-thru with indoor dining, on a parcel measuring 0.79-acres in the Mixed-Use Commercial (C-MU) Zone.

Environmental Assessment Status: The project is Categorically Exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15332, Categorical Exemption No. 2025-26.

Project Location: The project site is located along South Demaree Street, approximately 296-feet south of West Caldwell Avenue (Address: N/A) (APN:121-580-008).

9. CITY PLANNER UPDATE –

- a. Housing Element APR Memo: 2024 Housing Element Annual Progress Report (APR) and Program Reporting.

10. ADJOURNMENT

The Planning Commission meeting may end no later than 11:00 P.M. Any unfinished business may be continued to a future date and time to be determined by the Commission at this meeting. The Planning Commission routinely visits the project sites listed on the agenda.

For Hearing Impaired – Call (559) 713-4900 (TTY) 48-hours in advance of the scheduled meeting time to request signing services.

Any written materials relating to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the City Office, 315 E. Acequia Visalia, CA 93291, during normal business hours.

APPEAL PROCEDURE

THE LAST DAY TO FILE AN APPEAL IS THURSDAY, MAY 22, 2025, BEFORE 5:00 PM

According to the City of Visalia Zoning Ordinance Section 17.02.145 and Subdivision Ordinance Section 16.04.040, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal form with applicable fees shall be filed with the City Clerk at 220 North Santa Fe Street, Visalia, CA 93291. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

THE NEXT REGULAR MEETING WILL BE HELD ON TUESDAY, MAY 27, 2025



REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: May 12, 2025

PROJECT PLANNER: Cristobal Carrillo, Planner
Phone No.: (559) 713-4443
E-mail: cristobal.carrillo@visalia.city

SUBJECT: **Conditional Use Permit No. 2024-46:** A request by Maria Lomeli to establish three residential units within the D-MU (Downtown Mixed Use) Zone. The project site is located at 405 North West Street (APN: 093-175-010).

STAFF RECOMMENDATION

The Historic Preservation Advisory Committee (HPAC) and staff recommend approval of Conditional Use Permit No. 2024-46 based upon the findings and conditions in Resolution No. 2024-84. Staff's recommendation is based on the project's consistency with the City's General Plan and Zoning Ordinance.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2024-46 based on the findings and conditions in Resolution No. 2024-84.

PROJECT DESCRIPTION

The Conditional Use Permit (CUP) is a request to convert a former carriage house on a property with two existing single-family residences into a residential triplex as follows:

- A 1,068 square foot residential unit on the first floor;
- Two residential units (984 square feet and 568 square feet, respectively) on the second floor.

Floor Plans depicting the layout of the triplex are provided in Exhibit "B". The two largest units will contain two bedrooms, a kitchen, and bathroom. The smallest unit will contain a bedroom, bathroom, and kitchen. Three single car garages located on the first floor will remain, for use by the occupants. Conversion of the building into a triplex has already been conducted and is the subject of an ongoing Code Enforcement case (CE230386).



Conversion of the building into a triplex has already been conducted and is the subject of an ongoing Code Enforcement case (CE230386).

The project site is located within the Historic District, and is not listed on the Local Register of Historic Structures. Based on the site's location within the Historic District, review of exterior alterations and the CUP request was required by the HPAC. The HPAC conducted its review and recommended approval of the CUP request on September 25, 2024. The HPAC approval letter is included as Exhibit "E".

BACKGROUND INFORMATION

General Plan Land Use Designation	Downtown Mixed Use
Zoning	D-MU (Downtown Mixed Use) Zone
Surrounding Zoning and Land Use	North: D-MU / Single and multifamily residences South: D-MU / West School Avenue, Offices, single family residences East: D-MU / North West Street, multifamily residences, offices West: D-MU / Offices, Davorn's Restaurant, Frank's Liquor
Environmental Review	Categorical Exemption No. 2024-75
Special Districts	Historic District
Site Plan Review	2024-057

RELATED PROJECTS

Conditional Use Permit No. 2018-20: On October 22, 2018, the Planning Commission reviewed and approved CUP No. 2018-20, a request by Claudia Lomeli to allow a residential duplex in an existing building in the PA (Professional Administrative Office) Zone, located at 405 North West Street. The CUP approval expired on October 22, 2020, as no Building Permits were issued to enact the use.

Historic Preservation Advisory Committee Item No. 2024-26: On September 25, 2024, the HPAC reviewed and approved HPAC Item No. 2024-26, a request by Denee Fiore for a Conditional Use Permit to establish three residential units, and conduct exterior alterations, to a structure located at 405 North West Street. The item passed by a vote of 7-0. Conditions applied by the HPAC can be viewed in Exhibit "E" and are included as CUP Condition of Approval No. 3. The HPAC staff report is included in Exhibit "F".

PROJECT EVALUATION

Staff recommends approval of Conditional Use Permit No. 2024-46, as conditioned, based on the project's consistency with the General Plan Land Use, Housing Element, and Zoning Ordinance.

General Plan Consistency

The project is consistent with Land Use Element Policy LU-P-90 and Housing Element HE Policy 2.4. LU-P-90 allows for a range of retail, restaurant, entertainment, office, public/community facilities, and urban residential uses. Specifically, it allows residential development at a minimum density of 15 dwelling units per gross acre. If approved, the proposed development will be at a residential density of 5.1 units per acre, consistent with the Downtown Mixed Use land use designation and D-MU zoning district. HE Policy 2.4 states that the City shall provide a wide range of housing types (e.g., mixed use, flats, podium townhouses, condominiums, row houses, small-lot single-family residential, and live-work lofts) in the Downtown and East Downtown areas, as well as encourage both affordable and market-rate housing opportunities. The proposal, through the addition of three residential units on the project, will assist in meeting the goal to provide varied housing types in the Downtown area.

Land Use Compatibility

The Visalia Zoning Matrix identifies uses such residences as conditionally permitted within the D-MU Zone, requiring submittal and approval of a Conditional Use Permit (CUP). Through the CUP process, potential impacts can be addressed thereby ensuring compatibility between the proposed use and existing surrounding uses.

The surrounding area consists primarily of single and multi-family residential uses, with an office and restaurant to the northwest and west, respectively. The project site itself contains two separate single-family residences that are currently occupied. Exterior alterations proposed would restore the historic appearance of the structure, thereby improving compatibility with the surrounding Historic District. Given the above, the residential use of the site is considered compatible with the site and surrounding land uses.

Landscaping

A survey of the surrounding sites to the north and south of the project site indicated that street trees are predominant in the area. During review of the proposal by the HPAC, it was noted that street trees are an amenity that assists in beautifying historic neighborhoods. In order to ensure preservation of the



trees, the HPAC, in its advisory capacity to the Planning Commission, included as part of its recommendation a request that the Commission apply a condition of approval requiring the applicant to maintain street trees along the project site frontage. Staff notes that per consultation with City of Visalia Engineering staff, installation and maintenance of streets is already required per City standards. Compliance with these standards is included as recommended Condition No. 4.

Parking

The parking requirement for multi-family dwellings is 1.5 parking stalls per unit (VMC Section 17.34.020.A.2). If the proposal is approved, there will be a total of five residential units onsite (i.e., two single-family residences and the proposed triplex), requiring a total of 7.5 onsite parking stalls. Per the sites current configuration, the parking demand is not met. However, pursuant to Assembly Bill 2097, which went into effect January 1, 2023, public agencies are prohibited from imposing minimum parking requirements on sites that are located within a half-mile radius of a major transit stop. Visalia Transit operates bus service along Route 1, which runs through and contains bus stops on Main Street and Mooney Boulevard. Route 1 meets the definition of a “major transit stop”. The project site is located within a half mile of multiple Route 1 bus stops. Therefore, the site meets parking provisions as allowed under AB 2097.

Furthermore, to address additional onsite parking for future tenants of the triplex, the site contains a three car garage for the proposed triplex, with additional paved area in front of the garage which will allow for tandem parking (see Exhibit “A”). Parking for the two single-family homes which face West School Avenue is accommodated through use of on-street parking stalls located along the street frontage.

Environmental Review

The requested action is considered Categorical Exempt under Section 15301 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2024-75). Projects determined to meet this classification consist of the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of existing public or private structures, facilities, mechanical equipment, or topographical features, involving negligible or no expansion of the use beyond that existing at the time of the lead agency's determination. The types of "existing facilities" itemized in the Class 1 are not intended to be all-inclusive of the types of projects.

RECOMMENDED FINDINGS

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located.
 - b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity.
3. That the project is considered Categorical Exempt under Section 15301 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2024-75).

RECOMMENDED CONDITIONS OF APPROVAL

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2024-057, incorporated herein by reference.
2. That the use shall be operated in substantial compliance with the site plan, floor plan, building elevations, and operational statement, in Exhibits "A", "B", "C", and "D".
3. That the applicant shall comply with all conditions of Exhibit "E", the Approval Letter for Historic Preservation Advisory Committee Item No. 2024-26.
4. That the applicant shall plant and maintain street trees along the project site street frontage, in compliance with City Standards.
5. That substantial changes to the site plan and/or operational plan, or an intensification of the use, may require evaluation by the Site Plan Review committee, Historic Preservation Advisory Committee, and/or an amendment to this Conditional Use Permit.
6. That all other federal, state and city laws, codes and ordinances be complied with.

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal with applicable fees shall be in writing and shall be filed with the City Clerk at 220 North Santa Fe Street Visalia California. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

Attachments:

- Related Plans and Policies
- Resolution No. 2024-84
- Exhibit "A" – Site Plan
- Exhibit "B" – Floor Plan
- Exhibit "C" – Building Elevations
- Exhibit "D" – Operational Statement
- Exhibit "E" – HPAC Item No. 2024-26 Approval Letter
- Exhibit "F" – HPAC Item No. 2024-26 Staff Report
- Categorical Exemption No. 2024-75
- Site Plan Review Comments No. 2024-057
- Historic District and Local Register Map
- General Plan Land Use Map
- Zoning Map
- Aerial Photo
- Location Map

RELATED PLANS AND POLICIES

VISALIA MUNICIPAL CODE

Chapter 17.38 CONDITIONAL USE PERMITS

17.38.010 Purposes and powers.

In certain zones conditional uses are permitted subject to the granting of a conditional use permit. Because of their unusual characteristics, conditional uses require special consideration so that they may be located properly with respect to the objectives of the zoning ordinance and with respect to their effects on surrounding properties. In order to achieve these purposes and thus give the zone use regulations the flexibility necessary to achieve the objectives of this title, the planning commission is empowered to grant or deny applications for conditional use permits and to impose reasonable conditions upon the granting of such permits.

17.38.020 Application procedures.

- A. Application for a conditional use permit shall be made to the planning commission on a form prescribed by the commission which shall include the following data:
1. Name and address of the applicant;
 2. Statement that the applicant is the owner of the property or is the authorized agent of the owner;
 3. Address and legal description of the property;
 4. The application shall be accompanied by such sketches or drawings as may be necessary by the planning division to clearly show the applicant's proposal;
 5. The purposes of the conditional use permit and the general description of the use proposed;
 6. Additional information as required by the historic preservation advisory committee.
 7. Additional technical studies or reports, as required by the Site Plan Review Committee.
 8. A traffic study or analysis prepared by a certified traffic engineer, as required by the Site Plan Review Committee or Traffic Engineer, that identifies traffic service levels of surrounding arterials, collectors, access roads, and regionally significant roadways impacted by the project and any required improvements to be included as a condition or mitigation measure of the project in order to maintain the required services levels identified in the General Plan Circulation Element.
- B. The application shall be accompanied by a fee set by resolution of the city council sufficient to cover the cost of handling the application.

17.38.030 Lapse of conditional use permit.

A conditional use permit shall lapse and shall become void twenty-four (24) months after the date on which it became effective, unless the conditions of the permit allowed a shorter or greater time limit, or unless prior to the expiration of twenty-four (24) months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site that was the subject of the permit. A permit may be renewed for an additional period of one year; provided, that prior to the expiration of twenty-four (24) months from the date the permit originally became effective, an application for renewal is filed with the planning commission. The commission may grant or deny an application for renewal of a conditional use permit. In the case of a planned residential development, the recording of a final map and improvements thereto shall be deemed the same as a building permit in relation to this section.

17.38.040 Revocation.

Upon violation of any applicable provision of this title, or, if granted subject to a condition or conditions, upon failure to comply with the condition or conditions, a conditional use permit shall be suspended automatically. The planning commission shall hold a public hearing within sixty (60) days, in accordance

with the procedure prescribed in Section 17.38.080, and if not satisfied that the regulation, general provision or condition is being complied with, may revoke the permit or take such action as may be necessary to insure compliance with the regulation, general provision or condition. Appeals of the decision of the planning commission may be made to the city council as provided in Section 17.38.120.

17.38.050 New application.

Following the denial of a conditional use permit application or the revocation of a conditional use permit, no application for a conditional use permit for the same or substantially the same conditional use on the same or substantially the same site shall be filed within one year from the date of denial or revocation of the permit unless such denial was a denial without prejudice by the planning commission or city council.

17.38.060 Conditional use permit to run with the land.

A conditional use permit granted pursuant to the provisions of this chapter shall run with the land and shall continue to be valid upon a change of ownership of the site or structure that was the subject of the permit application subject to the provisions of Section 17.38.065.

17.38.065 Abandonment of conditional use permit.

If the use for which a conditional use permit was approved is discontinued for a period of one hundred eighty (180) days, the use shall be considered abandoned and any future use of the site as a conditional use will require the approval of a new conditional use permit.

17.38.070 Temporary uses or structures.

- A. Conditional use permits for temporary uses or structures may be processed as administrative matters by the city planner and/or planning division staff. However, the city planner may, at his/her discretion, refer such application to the planning commission for consideration.
- B. The city planner and/or planning division staff is authorized to review applications and to issue such temporary permits, subject to the following conditions:
 - 1. Conditional use permits granted pursuant to this section shall be for a fixed period not to exceed thirty (30) days for each temporary use not occupying a structure, including promotional enterprises, or six months for all other uses or structures.
 - 2. Ingress and egress shall be limited to that designated by the planning division. Appropriate directional signing, barricades, fences or landscaping shall be provided where required. A security officer may be required for promotional events.
 - 3. Off-street parking facilities shall be provided on the site of each temporary use as prescribed in Section 17.34.020.
 - 4. Upon termination of the temporary permit, or abandonment of the site, the applicant shall remove all materials and equipment and restore the premises to their original condition.
 - 5. Opening and closing times for promotional enterprises shall coincide with the hours of operation of the sponsoring commercial establishment. Reasonable time limits for other uses may be set by the city planner and planning division staff.
 - 6. Applicants for a temporary conditional use permit shall have all applicable licenses and permits prior to issuance of a conditional use permit.
 - 7. Signing for temporary uses shall be subject to the approval of the city planner.
 - 8. Notwithstanding underlying zoning, temporary conditional use permits may be granted for fruit and vegetable stands on properties primarily within undeveloped agricultural areas. In reviewing applications for such stands, issues of traffic safety and land use compatibility shall be evaluated and mitigation measures and conditions may be imposed to ensure that the stands are built and are operated consistent with appropriate construction standards, vehicular access and off-street parking. All fruits and vegetables sold at such stands shall be grown by the owner/operator or purchased by said party directly from a grower/farmer.
 - 9. Fruit/Vegetable stands shall be subject to site plan review.

- C. The City Planner shall deny a temporary use permit if findings cannot be made, or conditions exist that would be injurious to existing site, improvements, land uses, surrounding development or would be detrimental to the surrounding area.
- D. The applicant or any interested person may appeal a decision of temporary use permit to the planning commission, setting forth the reason for such appeal to the commission. Such appeal shall be filed with the city planner in writing with applicable fees, within ten (10) days after notification of such decision. The appeal shall be placed on the agenda of the commission's next regular meeting. If the appeal is filed within five (5) days of the next regular meeting of the commission, the appeal shall be placed on the agenda of the commission's second regular meeting following the filing of the appeal. The commission shall review the temporary use permit and shall uphold or revise the decision of the temporary use permit, based on the findings set forth in Section 17.38.110. The decision of the commission shall be final unless appealed to the council pursuant to Section 17.02.145.
- E. A privately owned parcel may be granted up to six (6) temporary use permits per calendar year.

17.38.080 Public hearing--Notice.

- A. The planning commission shall hold at least one public hearing on each application for a conditional use permit.
- B. Notice of the public hearing shall be given not less than ten days nor more than thirty (30) days prior to the date of the hearing by mailing a notice of the time and place of the hearing to property owners within three hundred (300) feet of the boundaries of the area occupied or to be occupied by the use that is the subject of the hearing, and by publication in a newspaper of general circulation within the city.

17.38.090 Investigation and report.

The planning staff shall make an investigation of the application and shall prepare a report thereon that shall be submitted to the planning commission. The report can recommend modifications to the application as a condition of approval.

17.38.100 Public hearing--Procedure.

At the public hearing the planning commission shall review the application and the statement and drawing submitted therewith and shall receive pertinent evidence concerning the proposed use and the proposed conditions under which it would be operated or maintained, particularly with respect to the findings prescribed in Section 17.38.110. The planning commission may continue a public hearing from time to time as it deems necessary.

17.38.110 Action by planning commission.

- A. The planning commission may grant an application for a conditional use permit as requested or in modified form, if, on the basis of the application and the evidence submitted, the commission makes the following findings:
 - 1. That the proposed location of the conditional use is in accordance with the objectives of the zoning ordinance and the purposes of the zone in which the site is located;
 - 2. That the proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity.
- B. A conditional use permit may be revocable, may be granted for a limited time period, or may be granted subject to such conditions as the commission may prescribe. The commission may grant conditional approval for a permit subject to the effective date of a change of zone or other ordinance amendment.
- C. The commission may deny an application for a conditional use permit.

17.38.120 Appeal to city council.

The decision of the City planning commission on a conditional use permit shall be subject to the appeal provisions of section 17.02.145.

17.38.130 Effective date of conditional use permit.

A conditional use permit shall become effective immediately when granted or affirmed by the council, or ten days following the granting of the conditional use permit by the planning commission if no appeal has been filed.

Chapter 17.19 MIXED USE ZONES

17.19.010 Purpose and intent.

- A. The several types of mixed zones included in this chapter are designed to achieve the following:
1. Encourage a wide mix of commercial, service, office, and residential land uses in horizontal or vertical mixed use development projects, or on adjacent lots, at key activity nodes and along corridors.
 2. Maintain Visalia's downtown Conyer Street to Tipton and Murray Street to Mineral King Avenue including the Court-Locust corridor to the Lincoln Oval area) as the traditional, medical, professional, retail, government and cultural center;
 3. Provide zone districts that encourage and maintain vibrant, walkable environments.
- B. The purposes of the individual mixed use zones are as follows:
1. Mixed Use Commercial Zone—(C-MU). The purpose and intent of the mixed use commercial zone district is to allow for either horizontal or vertical mixed use development, and permit commercial, service, office, and residential uses at both at key activity nodes and along corridors. Any combination of these uses, including a single use, is permitted.
 2. Mixed Use Downtown Zone—(D-MU). The purpose and intent of the mixed use downtown zone district is to promote the continued vitality of the core of the community by providing for the continuing commercial development of the downtown and maintaining and enhancing its historic character. The zone is designed to accommodate a wide mix of land uses ranging from commercial and office to residential and public spaces, both active and passive. The zone is intended to be compatible with and support adjacent residential uses, along with meeting the needs of the city and region as the urban center of the city; to provide for neighborhood, local, and regional commercial and office needs; to accommodate the changing needs of transportation and integrate new modes of transportation and related facilities; and to maintain and enhance the historic character of the city through the application of architectural design features that complement the existing historic core of the city. (Ord. 2017-01 (part), 2017)

17.19.015 Applicability.

The requirements in this chapter shall apply to all property within the C-MU and D-MU zone districts. (Ord. 2017-01 (part), 2017)

17.19.020 Permitted uses.

Permitted uses in C-MU and D-MU zones shall be determined by [Table 17.25.030](#) in Section [17.25.030](#). (Ord. 2017-01 (part), 2017)

17.19.030 Conditional and temporary uses.

Conditional and temporary uses in the C-MU and D-MU zones shall be determined by [Table 17.25.030](#) in Section [17.25.030](#). (Ord. 2017-01 (part), 2017)

17.19.040 Required conditions.

- A. A site plan review permit must be obtained for any development in any C-MU and D-MU zones, subject to the requirements and procedures in [Chapter 17.28](#).
- B. All businesses, services and processes shall be conducted entirely within a completely enclosed structure, except for off-street parking and loading areas, gasoline service stations, outdoor dining areas, nurseries, garden shops, Christmas tree sales lots, bus depots and transit stations, electric distribution substation, and recycling facilities;

C. All products produced on the site of any of the permitted uses shall be sold primarily at retail on the site where produced. (Ord. 2017-01 (part), 2017)

17.19.050 Off-street parking and loading facilities.

Off-street parking and off-street loading facilities shall be provided as prescribed in [Chapter 17.34](#). (Ord. 2017-01 (part), 2017)

17.19.060 Development standards in the C-MU zones outside the downtown area.

The following development standards shall apply to property located in the C-MU zone and located outside the Downtown Area, which is defined as the area that is south of Murray Avenue, west of Ben Maddox Way, north of Mineral King Avenue, and east of Conyer Street:

- A. Minimum site area: five (5) acres.
- B. Maximum building height: fifty (50) feet.
- C. Minimum required yards (building setbacks):
 - 1. Front: fifteen (15) feet;
 - 2. Rear: zero (0) feet;
 - 3. Rear yards abutting an R-1 or R-M zone district: fifteen (15) feet;
 - 4. Side: zero (0) feet;
 - 5. Side yards abutting an R-1 or R-M zone district: fifteen (15) feet;
 - 6. Street side yard on corner lot: ten (10) feet.
- D. Minimum required landscaped yard (setback) areas:
 - 1. Front: fifteen (15) feet;
 - 2. Rear: five (5) feet;
 - 3. Rear yards abutting an R-1 or R-M zone district: five (5) feet;
 - 4. Side: five (5) feet (except where a building is located on side property line);
 - 5. Side yards abutting an R-1 or R-M zone district: five (5) feet;
 - 6. Street side on corner lot: ten (10) feet.
- E. The provisions of [Chapter 17.58](#) shall also be met, if applicable. (Ord. 2017-01 (part), 2017)

17.19.070 Development standards in the D-MU zone and in the C-MU zones inside the downtown area.

The following development standards shall apply to property located in the D-MU and C-MU zone and located inside the Downtown Area, which is defined as the area that is south of Murray Avenue, west of Ben Maddox Way, north of Mineral King Avenue, and east of Conyer Street:

- A. Minimum site area: No minimum.
- B. Maximum building height: one hundred (100) feet.
- C. Minimum required yards (building setbacks):
 - 1. Front: zero (0) feet;
 - 2. Rear: zero (0) feet;
 - 3. Rear yards abutting an R-1 or R-M zone district: zero (0) feet;
 - 4. Side: zero (0) feet;
 - 5. Side yards abutting an R-1 or R-M zone district: zero (0) feet;
 - 6. Street side yard on corner lot: zero (0) feet.
- D. Minimum required landscaped yard (setback) areas:
 - 1. Front: five (5) feet (except where a building is located on property line);
 - 2. Rear: zero (0) feet;
 - 3. Rear yards abutting an R-1 or R-M zone district: zero (0) feet;
 - 4. Side: five (5) feet (except where a building is located on side property line);
 - 5. Side yards abutting an R-1 or R-M zone district: five (5) feet except where a building is located on side property);
 - 6. Street side on corner lot: five (5) feet (except where a building is located on property line).
- E. The provisions of [Chapter 17.58](#) shall also be met, if applicable.
(Ord. 2024-07 § 8, 2024: Ord. 2017-13 (part), 2017: Ord. 2017-01 (part), 2017)

Chapter 17.56

Historic Preservation District

17.56.010 Purposes and intent.

A. There is created a historic district, the boundaries of which are shown on the two maps entitled, "Historic District Overlay," which accompany the ordinance codified in this chapter and which are on file in the office of the city clerk, on the 19th day of November, 1979 and dated 19th day of November, 1979. Said maps are adopted and made a part of this chapter.

B. This chapter is enacted to preserve and promote the public health, safety and welfare of the residents of the city, and to express the commitment of the city to assure that the city's cultural heritage, as reflected in its historic structures, sites, and features is not destroyed, through:

1. The protection and preservation of historic structures;
2. The preservation and maintenance of historic residential areas as cohesive neighborhood units;
3. The enhancement of property values in the older areas of the city;
4. The assurance that the community's cultural heritage, as reflected in the environment, is not lost;
5. The encouragement of the development of vacant and incompatibly developed properties in accordance with the character of the historic district;
6. The involvement of residents of the older areas in planning their own neighborhoods. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7700)

17.56.020 Components of the chapter.

This chapter shall include:

1. The ordinance, which specifies the historic district overlay designation, design evaluation criteria, and the formation, powers and duties of a historic preservation advisory committee;
2. Creation of a local register of historic structures, sites and features that may be modified from time to time by resolution of the city council.
3. Two maps that designate the historic district overlay. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7701)

17.56.030 Definitions.

A. All definitions, general and specific, set forth in Section 17.04.030 shall be applicable to this chapter.

B. Definitions.

"Construction" means any building activity requiring the issuance of a building permit, except for any activity that does not affect the exterior appearance of the structure.

"Enlargement" means construction that results in the expansion of the gross floor area of a structure.

"Historic structure" means a structure listed on the Local Register.

"Local register" means the listing of local historic structures, sites and features adopted by the city council and maintained by the historic preservation advisory committee, and incorporated herein by reference. This designation is inclusive of all structures and features located within the boundaries of the property for which the Local register designation has been applied. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7702)

“Local register, Background” means properties that may not be historically significant or unique in their construction, but which contribute positively to the “visual fabric” of the City of Visalia.

“Local register, Exceptional” means properties with preeminent historical significance, considered for nomination to the California Register of Historical Resources and/or National Register of Historic Places.

“Local register, Focus” means properties having significant value, of good to excellent quality, considered for local recognition and protection.

17.56.040 Regulation of structures.

No structure shall be constructed, altered or enlarged that is located in the historic district or that is listed on the local register, unless such a permit is issued pursuant to the terms of this chapter. No structure listed on the local register shall be moved or demolished unless a permit is issued pursuant to the terms of this chapter. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7703)

17.56.050 Creation of historic preservation advisory committee.

In order to execute the purposes declared in this chapter, there is created a historic preservation advisory committee.

A. Committee Membership.

1. The historic preservation advisory committee shall consist of seven members appointed by the city council to serve without compensation. All committee members shall be residents of the city at the time of their appointment and will lose their position if they move outside the city limits during their term of office. Members shall be appointed on the basis of:

- a. Relevant professional or business qualifications;
- b. Ownership of property within the historic district;
- c. Practical experience in restoration or preservation;
- d. Exceptional civic interest.
- e. Terms of office shall be for two years.

2. Vacancies that may occur on the committee shall be filled by appointment of a new member of the city council for the duration of the unexpired term of office. The Council has the option of appointing up to two Historic Preservation Advisory Committee alternates. Should a mid-term vacancy occur, an alternate may automatically fill the unexpired term. The council shall also have the power to remove any member from the committee by an affirmative vote of three council members.

3. Four members of the committee shall constitute a quorum for the transaction of business.

B. Procedures for the Review of Applications. The operating procedures of the historic preservation advisory committee shall be prescribed from time to time by resolution of the city council, for the purpose of carrying into effect the standards and specifications of this chapter. The committee may adopt, amend, and repeal rules and regulations governing the conduct of its meetings, as long as said rules do not violate the procedures established by the city council or the terms of this chapter.

C. Duties and Responsibilities. The historic preservation advisory committee shall review applications only as specified in this chapter, consistent with the rules and regulations adopted from time to time by resolution of the city council (as referred to in Section 17.56.050(B)). Applications shall be approved or disapproved based solely on the considerations set forth in this chapter. It is the intent of this chapter that the historic preservation advisory committee shall encourage applicants to make alterations and repairs to structures in the spirit of the architectural style of the structure. The duties and responsibilities of the historic preservation advisory committee shall include the following:

1. It shall be the duty of the historic preservation advisory committee to review all proposed zoning actions (zone changes, conditional use permits, special zoning exceptions, planned unit developments and variances) within the historic district. The committee may recommend approval, conditional approval, modification or disapproval of an application based upon the expected impact of the proposed zoning action on the historic or architectural significance of the affected structure(s), neighborhood, or the entire historic district. The committee's recommendation shall be forwarded to the planning commission for its consideration.

2. It shall be the duty of the historic preservation advisory committee to review all applications for site plan review permits within the historic district for compliance with the provisions of this chapter. Items that shall be subject to review by the committee include but are not limited to vehicular access, location and screening of parking, setbacks, location of service use areas, walls and landscaping. The committee may recommend approval, conditional approval, disapproval or resubmittal of the site plan review permit application. The committee's recommendation shall be forwarded to the site plan review committee for its consideration.

3. It shall be the duty of the historic preservation advisory committee to review all applications for the construction or exterior alteration or enlargement of structures within the historic district or on the local register. The committee shall have the power to approve, modify or disapprove such applications before a building permit can be issued, subject to the provisions of Sections 17.56.100 and 17.56.110.

4. It shall be the duty of the historic preservation advisory committee to review all applications for sign permits within the historic district or for properties listed on the local register. The committee may recommend approval, conditional approval or denial of the sign permit application based upon the proposed design and/or materials, but not upon the proposed size or location. The application shall then be presented to the proper issuing authority for sign permits, pursuant to Chapter 17.48. Sign permits shall be issued only in compliance with the recommendation of the committee. Approval by the committee in no way implies approval by the issuing authority for sign permits, whose approval must also be secured pursuant to Chapter 17.48.

5. It shall be the duty of the historic preservation advisory committee to review all applications for the moving or demolition of structures in the historic district or listed on the local register. The committee shall have the power to approve, conditionally approve, or disapprove such applications, subject to the provisions of Section 17.56.130.

6. It shall be the duty of the historic preservation advisory committee to compile and update the historic survey and inventory, and to nominate properties to the local register, the California Register of Historical Resources, and the National Register of Historic Places. In selecting properties for nomination to the local register, the board shall consider:

- a. Architectural significance and style;
- b. Historic significance, including age of structure, original owners, and events related to the structure, site or original owners.

The committee shall review the local register every five years, make recommendations for the addition or deletion of structures or sites, and submit said recommendations to the planning commission and city council for certification.

7. Permits may be issued for air conditioners, electrical work and plumbing work that is visible from a public right-of-way when the chief building official determines that the work insignificantly affects the exterior of a structure, or that reasonable alternatives as to location or screening have been employed. The building official may forward to the historic preservation advisory committee applications for permits for this type of work when it appears that the appearance of a structure may be significantly altered. This subsection shall not apply to the following types of permit applications:

1. Reroofing with like materials;

2. Residing with like materials;
3. Swimming pools;
4. Masonry repairs with like materials;
5. Chimney repair with like materials. (Ord. 2017-01 (part), 2017: Ord. 2008-11 § 1, 2008: Ord. 2001-13 § 4 (part), 2001: prior code § 7704)
8. Sites within the historic district or on the local register shall reserve a minimum 50% of the required front yard setback area for the purposes of landscaping.

17.56.060 Appeal.

Any person or persons jointly or severally aggrieved by a decision of the historic preservation advisory committee may make an appeal in writing therefrom to the city council within ten days of said action. The city council, after proceeding in the manner as provided therein and with the same power and authority there invested in passing upon appeals before it under the provisions of law and this chapter and in the exercise thereof, may reverse, affirm or modify or affirm as modified the action of the historic preservation advisory committee. Appeals of a decision of the site plan review committee shall be filed with the planning commission in the manner prescribed in Section 17.28.050. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7705)

17.56.070 Demolition or moving of historic structures.

A. The demolition of structures listed on the local register, and the moving of local register structures from their sites, shall be discouraged. The historic preservation advisory committee shall review all applications for demolition or moving permits for structures on the local register and for any structures within the historic district boundaries.

B. After due consideration, the committee shall exercise one of the options listed below:

1. The committee may approve the demolition permit if it finds that the structure is a hazard to public health or safety, as determined by the building official or his designee, in consultation with the historic preservation advisory committee.

2. The committee may decide that up to a six-month moratorium be placed upon the processing of the demolition or moving permit, in order to allow time for the applicant and the committee to find alternative uses for the structure and to seek alternative solutions to the demolition or moving of the structure. If no alternatives are found, after the six-month moratorium has expired, the committee must approve the application.

3. The committee may deny an application for demolition. Denial of a demolition permit by the committee is subject to appeal to the city council pursuant to Section 17.56.060.

4. The committee may approve the application.

C. When an application is acted upon, the committee shall notify the building official of the approval, conditional approval or denial. Upon receipt of said notification, the building official shall process the application accordingly. If, after six months from the date of filing of the application, the building official has not received such notification, a permit may be issued. Approval of a permit application by the committee in no way implies approval by the building official, whose approval must also be secured. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7706)

17.56.080 Ordinary maintenance and repair.

Nothing in this chapter shall be construed to prevent ordinary maintenance or repair of any structure within the historic district; provided such work involves no change that requires issuance of a building permit. Nothing in this chapter shall be construed to prevent the construction, reconstruction, alteration or demolition of any feature that, in the view of the proper authority acting lawfully, is required for the public

safety because of an unsafe or dangerous condition. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7708)

17.56.090 Exceptions to Visalia Municipal Code requirements.

Due to the peculiar conditions of design and construction in historic neighborhoods where structures were sometimes built close to lot lines, and where ownership patterns have changed over the years, it is sometimes in the public interest to retain the historic appearance of a neighborhood by making an exception to normal setback, parking, landscaping, fencing and screening requirements of the Visalia Municipal Code, where such an exception does not interfere with the public health or safety. Within the historic district, where it is deemed that such an exception is warranted and will not adversely affect neighboring properties, the historic preservation advisory committee may initiate and/or recommend to the planning commission that such exception to Visalia Municipal Code requirements be made. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7709)

17.56.100 Building design compatibility criteria.

This section contains criteria for reviewing all applications for new construction and sign permits within the historic district, and for any rehabilitation, renovation, alteration, reconstruction, or enlargement affecting the exterior appearance of any structure within the historic district that is not listed on the local register, which requires the issuance of a building permit. Each application shall be considered in terms of its compatibility or complementariness with a majority of structures in the immediately surrounding area. In reviewing an application, the historic preservation advisory committee shall consider the following general design standards and principles:

A. Height and Scale. New buildings should be constructed to a height within a reasonable average height of existing adjacent buildings.

B. Spacing of Buildings on Street. A rhythm of recurrent building masses to separations should be retained.

C. Relationship of Materials and Textures. Choice of building materials and texture (smooth and rough) should enhance the desired neighborhood qualities such as compatibility, similarity and continuity.

D. Relationship of Architectural Details and Roof Shapes. Choice of architectural details and roof shape should insure compatible appearance with surrounding structures.

E. Landscaping. Landscaping should reflect the predominant quality and quantity of landscaping within the surrounding area. The concern here is more with mass and continuity.

F. Directional Expression of Front Elevations. Structural shape, placement of openings, and architectural details should be used to give a compatible appearance with adjacent structures that may be horizontal, vertical or nondirectional in nature. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7710)

17.56.110 Local register structures.

This section contains criteria for reviewing all applications for building permits for exterior rehabilitation, renovation, alteration, reconstruction, or enlargement of any local register structure, and for any interior modification that requires the issuance of a building permit for a publicly owned and publicly accessible local register structure. In reviewing an application, the historic preservation advisory committee shall consider the following general standards and principles:

A. Every reasonable effort shall be made to provide a compatible use for a property that requires minimal alteration of the building structure, or site and its environment, or to use a property for its originally intended purpose.

B. The distinguishing original qualities or character of a building, structure, or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features should be avoided when possible.

C. All buildings, structures and sites shall be recognized as products of their own time. Alterations that have no historic basis and that seek to create an earlier appearance shall be discouraged.

D. Changes, which may have taken place in the course of time, are evidence of the history and development of a building, structure, or site and its environment. These changes may have acquired significance in their own right, and this significance shall be recognized and respected.

E. Distinctive stylistic features or examples of skilled craftsmanship, which characterize a building, structure or site, shall be treated with sensitivity.

F. Deteriorated architectural features shall be repaired rather than replaced, wherever possible. In the event replacement is necessary, the new material should match the material being replaced in composition, design, color, texture and other visual qualities. Repair or replacement of missing architectural features should be based on accurate duplications of features, substantiated by historical, physical or pictorial evidence rather than on conjectural designs or the availability of different architectural elements from other buildings or structures.

G. The surface cleaning of structures shall be undertaken with the gentlest means possible. Sandblasting and other cleaning methods that will damage the historic building materials shall not be undertaken, without prior approval of the historic preservation advisory committee.

H. Every reasonable effort shall be made to protect and preserve archaeological resources affected by, or adjacent to any acquisition, protection, stabilization, preservation, rehabilitation, restoration or reconstruction project. (Ord. 2710 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7711)

17.56.120 Fencing and wall design criteria.

This section contains criteria for reviewing all applications for new fencing and walls within the historic district or on the local register, and for any application for rehabilitation, renovation, alteration, reconstruction, or enlargement of fencing and walls within the historic district or on the local register. In reviewing an application, the historic preservation advisory committee shall consider the following general standards and principles:

A. Fencing proposals shall be used to form continuous cohesive walls of enclosure along the street, and shall be compatible with the architectural style of the main building and historic district.

B. If historic walls or fencing exist on a property, preference should be given to repair of the existing fencing or wall to restore its original appearance to the extent possible. If removal and replacement of historic fencing and walls is proposed, preference should be given to the use of like materials to preserve the appearance of the original fencing or wall.

C. Proposals for new fencing or walls should be compatible with the character and architectural elements of the historic structure onsite and its period of original construction.

D. Appropriate materials for fencing and walls shall include, but not be limited to, landscape masses, wood, wood pickets, wrought iron, cast iron, masonry walls, brick, stone, decorative wire, and vinyl materials that mimic the appearance of painted wood.

E. Proposals for chain link fencing in areas visible from the street shall be discouraged. The historic preservation advisory committee may grant an application for chain link fencing along public streets if, on the basis of the application and the evidence submitted, the committee makes one of the following findings:

1. That the proposed chain link fencing is compatible with the historic structure and the neighborhood;

2. That sufficient evidence has been provided indicating that the applicant will experience an economic hardship should the chain link fencing not be permitted to be placed onsite;

17.56.130 Role of building official.

A. The building official shall refuse to issue all building or sign permits based upon an application disapproved by the historic preservation advisory committee, unless such application is later approved by the city council. The building official may approve any application approved or conditionally approved by the committee at such time as any conditions specified in such conditional approval are clearly indicated by the applicant on the plans presented to the building official for approval. If an appeal to the city council is filed within ten days from the date of committee approval of an application, no permit shall be issued until the outcome of said appeal is finally determined by the city council.

B. After a building permit has been issued, the building official or his/her designee shall from time to time inspect the construction, alteration or enlargement approved by the committee and shall take such action as is necessary to assure compliance with the approved plans. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7712)

17.56.140 Separability.

The provisions of this chapter shall be deemed to be severable, and if any of its provisions shall be held unconstitutional by any court of competent jurisdiction, the decision of the court shall not impair any of the remaining provisions. (Ord. 2017-01 (part), 2017: Ord. 2001-13 § 4 (part), 2001: prior code § 7707)

RESOLUTION NO. 2024-84

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2024-46, A REQUEST BY MARIA LOMELI TO ESTABLISH THREE RESIDENTIAL UNITS WITHIN THE D-MU (DOWNTOWN MIXED USE) ZONE. THE PROJECT SITE IS LOCATED AT 405 NORTH WEST STREET (APN: 093-175-010).

WHEREAS, Conditional Use Permit No. 2024-46, is a request by Maria Lomeli to establish three residential units within the D-MU (Downtown Mixed Use) Zone. The project site is located at 405 North West Street (APN: 093-175-010); and

WHEREAS, the Historic Preservation Advisory Committee of the City of Visalia, after duly published notice, held a public hearing before said Committee on September 25, 2024; and

WHEREAS, the Historic Preservation Advisory Committee of the City of Visalia considered the Conditional Use Permit in accordance with Section 17.56.050.B of the Zoning Ordinance of the City of Visalia, and based on evidence contained in the staff report and testimony presented at the public hearing, recommended approval of the Conditional Use Permit to the Visalia Planning Commission, via approval of Historic Preservation Advisory Committee Item No. 2024-26; and,

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on May 12, 2025; and

WHEREAS, the Planning Commission of the City of Visalia finds the Conditional Use Permit to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, the Planning Commission finds the project to be Categorically Exempt consistent with the California Environmental Quality Act (CEQA) and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT RESOLVED that the project is exempt from further environmental review pursuant to CEQA Section 15301.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located.

- b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity.
3. That the project is considered Categorically Exempt under Section 15301 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2024-75).

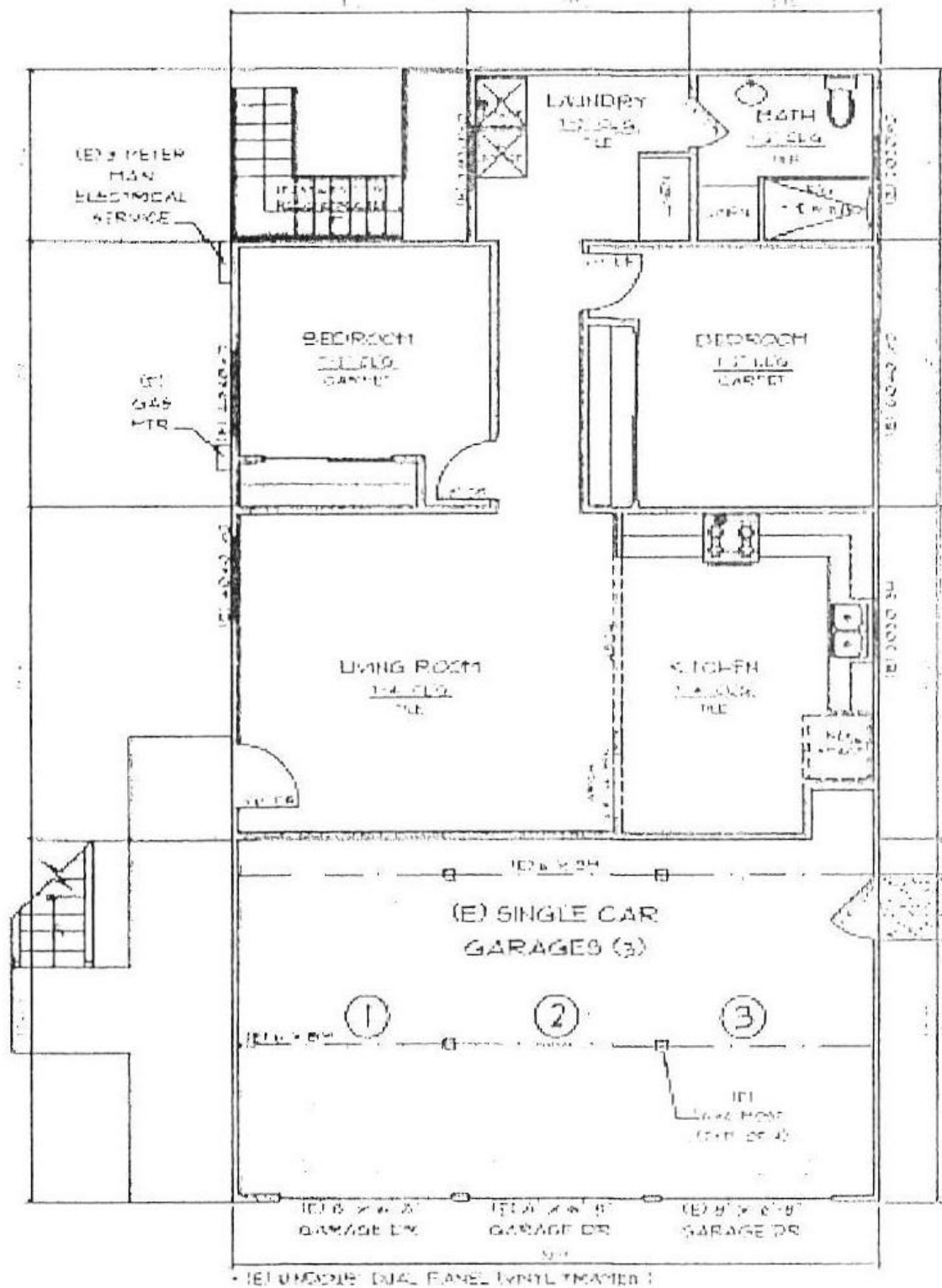
BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2024-057, incorporated herein by reference.
2. That the use shall be operated in substantial compliance with the site plan, floor plan, building elevations, and operational statement, in Exhibits “A”, “B”, “C”, and “D”.
3. That the applicant shall comply with all conditions of Exhibit “E”, the Approval Letter for Historic Preservation Advisory Committee Item No. 2024-26.
4. That the applicant shall plant and maintain street trees along the project site street frontage, in compliance with City Standards.
5. That substantial changes to the site plan and/or operational plan, or an intensification of the use, may require evaluation by the Site Plan Review committee, Historic Preservation Advisory Committee, and/or an amendment to this Conditional Use Permit.
6. That all other federal, state and city laws, codes and ordinances be complied with.

EXHIBIT "A"



EXHIBIT "B"



FLOOR PLAN

405 N. WEST ST.

(1ST. FLOOR / 918 S.F.)

NTS

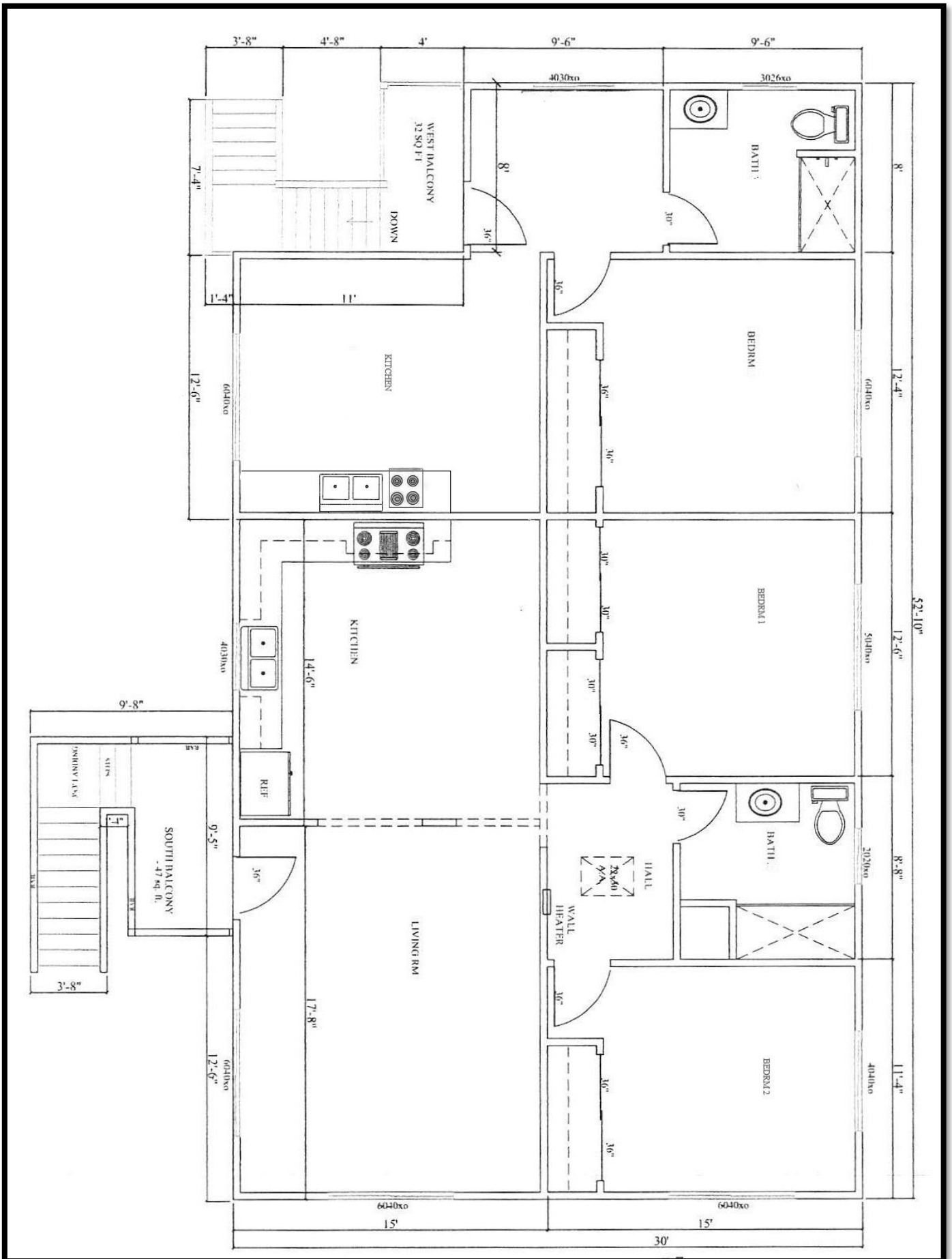


EXHIBIT C

405 N West St Visalia CA 93291



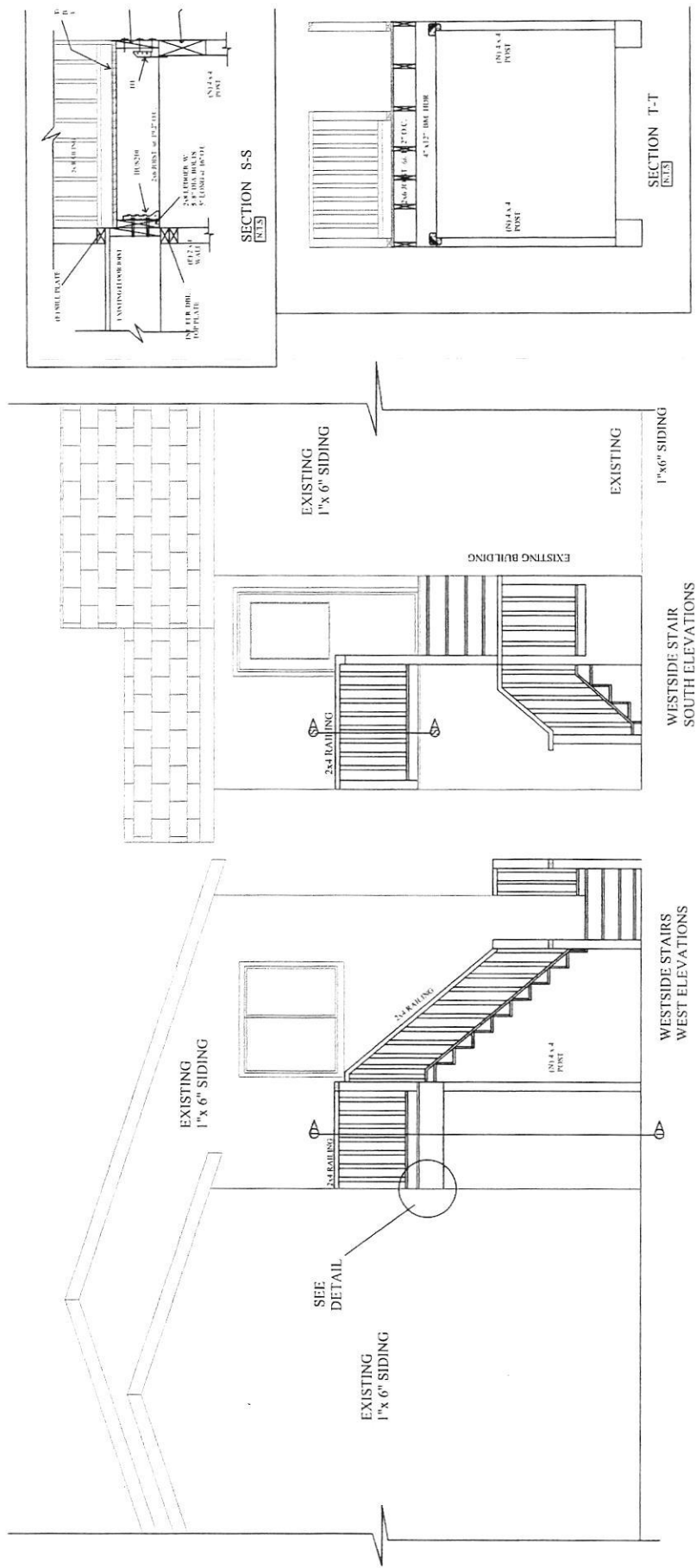
Southside & South East of the building: Staircases with premature updates, and exterior paint and trim for approval.



North Side of the Building: Remove/relocated HVAC (premature installation) to ground level. Will provide covering for wire, cables, lines, etc.



Westside of the building: Repave/repair sidewalk and driveway. Remove window screen coverings from all windows and replace with stationary screens. Remove Maple tree located in front of the driveway entrance.



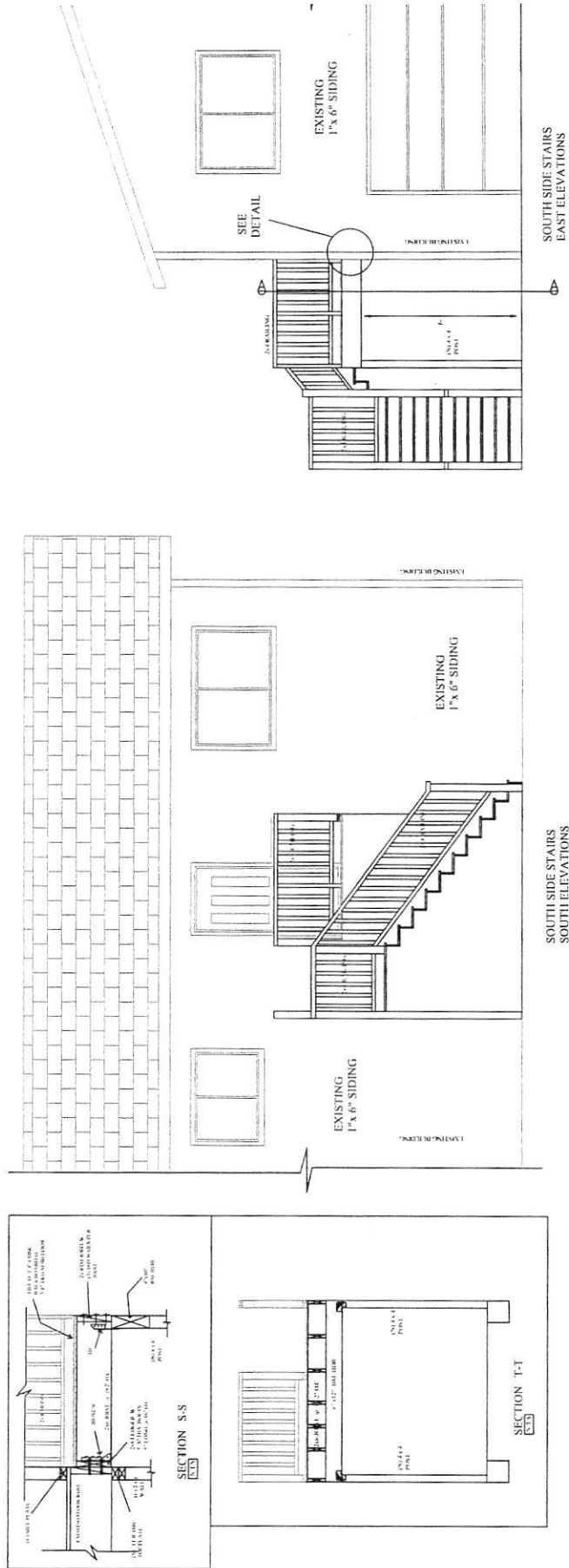


EXHIBIT D

Operational Statement (Updated)

Re: 405 N. West St. Visalia, CA 93291

Overview:

Residential apartment building has been updated for residential use. Property has three (3) individual units with one downstairs 2 bedroom 1 bathroom apartment and two apartment units upstairs with one 2 bedroom 1 bathroom unit and one 1 bedroom 1 bathroom unit. The residence also included three (3) attached single car garages located on the east of the residence.

Staircase:

Staircases located on the west and the south of the residence have been re-built to provide suitable access to the upstairs units.

Window Screens:

Building, window trim, staircases, and garages have been painted. New window screens were installed for each apartment unit. Per Historic District, window screens are too dark and not standard. Will have replaced with approval with standard stationary screens.

HVAC:

Each apartment unit has newer HVAC window units installed for residential use. HVAC units will be removed from the windows pre the Historic Districts request and relocated on the premises by either placing 5 feet away from the building or installed on the roof of the building.

Updated work to the residence was completed in 2019 under expired building permits. The units are rent ready and will need approval with the City of Visalia Code Enforcement and be brought into compliance with the city's regulations.

PAVEMENT/SIDEWALK

Remove damage sidewalk concrete located in front of the garages, remove tree in the center the sidewalk due to roots uplifting the walkway. Replay new concrete on the sidewalk/driveway area.



October 24, 2024

The Equity Group
Attn: Denee Fiore
420 North Court Street
Visalia, CA 93291

RE: Historic Preservation Advisory Committee Item No. 2022-26 (405 North West Street)

On September 25, 2024, the Historic Preservation Advisory Committee (HPAC) reviewed your request for a Conditional Use Permit establish three residential units, and conduct exterior alterations, on a property within the D-MU (Downtown Mixed Use) Zone. The project site is located at 405 North West Street (APN: 093-175-010). The HPAC approved the exterior alterations, and recommended approval of the Conditional Use Permit to the Visalia Planning Commission, with a recommendation that the Planning Commission include a Condition of Approval requiring placement of street trees along the project street frontage, based upon the following findings and conditions:

Findings:

1. The site is within the Historic District and is not listed in the Local Register of Historic Structures.
2. That the proposal is consistent with residential and commercial uses in the Historic District and the surrounding area.
3. That the proposal is in keeping with the purpose and intent of the Historic Preservation Element and Ordinance.
4. That the proposal will not be injurious to the surrounding properties or character of the Historic District due to its compatibility with the surrounding area.
5. That the Conditional Use Permit request to establish three residential units in the D-MU (Downtown Mixed Use) Zone is consistent with the Zoning Ordinance, Historic Preservation Element, and Historic Preservation Ordinance.

Conditions:

1. That the project shall be developed in substantial compliance with the site plan in Exhibit "A", Floor Plans in Exhibit "B", Building Elevations in Exhibit "C" and "D", HPAC Project Application in Exhibit "E", and Staircase Plans in Exhibit "F", except as modified below:
 - a. That the existing window screens can remain, and that the applicant shall place vertical wood dividers on all the window screens, to mimic the appearance of the existing windows.
2. That the project shall be developed in compliance with all development standards of the D-MU Zone.

3. That the applicant shall comply with all requirements of Site Plan Review No. 2024-057.
4. That the project undergoes the appropriate City permitting process.
5. That any significant changes to the proposed use, or any changes to the exterior of onsite structures, be brought back to the Historic Preservation Advisory Committee prior to any review by the Planning Commission and/or issuance of a Building Permit.
6. That all other City codes, ordinances, standards, and regulations shall be met.
7. That the approval from the Historic Preservation Advisory Committee shall lapse and become void twelve months after the date on which it became effective, unless the conditions of the approval allowed a shorter or greater time limit, or unless prior to the expiration of twelve months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site that was the subject of the approval.

There is a 10-day appeal period for this action from the date of approval. No permits may be issued until the appeal period has lapsed with no appeal of the action. Following completion of the appeal period, a Conditional Use Permit shall be filed for review and approval by the Visalia Planning Commission, and a Building Permit obtained from the City of Visalia for the exterior alterations conducted, prior to the commencement of any work onsite. The Building Department is located at 315 E. Acequia Avenue, Visalia CA 93291.

If you have any questions, please contact me at (559) 713-4443 or e-mail cristobal.carrillo@visalia.city

Regards,

A handwritten signature in blue ink, appearing to read 'Cristobal Carrillo', with a long horizontal flourish extending to the right.

Cristobal Carrillo,
Associate Planner

EXHIBIT F



REPORT TO THE CITY OF VISALIA HISTORIC PRESERVATION ADVISORY COMMITTEE

HEARING DATE: September 25, 2024

PROJECT PLANNER: Cristobal Carrillo, Associate Planner
Phone: (559) 713-4443
E-mail: cristobal.carrillo@visalia.city

SUBJECT: **Historic Preservation Advisory Committee Item No. 2024-26:** A request by Denée Fiore for a Conditional Use Permit to establish three residential units, and conduct exterior alterations, on a property within the D-MU (Downtown Mixed Use) Zone. The project site is located at 405 North West Street (APN: 093-175-010).

STAFF RECOMMENDATION

Staff recommends that the Historic Preservation Advisory Committee (HPAC) take the following actions:

1. Approve the exterior alterations to the building.
2. Recommend approval of the Conditional Use Permit (CUP) request to the Visalia Planning Commission.
3. Recommend that the Planning Commission include a Condition of Approval requiring the reinstallation of street trees along the North West Street property frontage.



SITE DATA

The site is zoned D-MU (Downtown Mixed Use) and contains an unpermitted residential triplex (the subject of this proposal), and two separate single-family residences.

The project site is located within the Historic District and is not listed on the Local Register of Historic Structures.

RELATED PROJECTS

HPAC Item No. 2018-11: On October 10, 2018, the HPAC considered a request to allow a residential duplex and exterior alterations for an existing building located at 405 North West

Street. The Committee approved the request subject to approval of a Conditional Use Permit by the Visalia Planning Commission.

Conditional Use Permit No. 2018-20: On October 22, 2018, the Visalia Planning Commission considered a request to allow a residential duplex in an existing building located at 405 North West Street, within the PA (Professional Administrative/Office) Zone. The Commission approved the request subject to conditions of approval requiring compliance with HPAC conditions.

B194031: On September 4, 2019, a Building Permit request was submitted to convert the building at 405 North West Street into a duplex. A request for revisions to the construction plans was never fulfilled and the Building Permit request expired on April 27, 2021.

CE230386: On March 8, 2023, Neighborhood Preservation staff cited the property owner of 405 North West Street for conducting exterior alterations without HPAC approval or Building Permit issuance. During a site inspection staff discovered that the structure had been converted into a triplex and that the units were occupied. Further research by Planning Division staff determined that the previous CUP approval had expired due to lack of action by the property owner to obtain a Building Permit. Due to the addition of a third unit to the building, the applicant was directed by staff to submit to the Site Plan Review Committee for approval of the residential units.

Site Plan Review No. 2024-057: On March 27, 2024, a request to convert an existing building into a triplex at 405 North West Street was approved to proceed with City processes by the Site Plan Review Committee. The approval required the applicant to submit a CUP request for review by the HPAC and Planning Commission. The comments for Site Plan Review No. 2024-057 are included with this report.

PROJECT DESCRIPTION

Per the project application, the applicant is requesting to conduct the following:

1. Convert the existing building into a residential triplex, with one 1,068 square foot residential unit on the first floor, and two residential units (984 square feet and 568 square feet, respectively) on the second floor (see Floor Plans in Exhibit “B”). The two largest units will contain two bedrooms, a kitchen, and bathroom. The smallest unit will contain a bedroom, bathroom, and kitchen. Three single car garages located on the first floor will remain.
2. Remove all existing window screens and replace them with conventional window screens as depicted in Exhibit “D”.
3. Add HVAC piping covers to the walls, as depicted in Exhibit “D”.
4. Minor modifications to the existing staircases on the southern and western building exteriors (see stairs in Exhibit “C”).

Additional exterior alterations and site improvements not subject to HPAC review are also proposed as follows:

1. Relocation of two wall mounted HVAC units from the northern building exterior, to ground level (see Exhibit “C”).
2. Removal of street trees along North West Street.
3. Repair of curb, gutter, and sidewalk along the North West Street frontage.

Conversion of the building into a triplex and modifications to the staircases have already been conducted.

HPAC 2024-26 - 405 North West Street – Three Residential Units In D-MU Zone, Exterior Alterations

DISCUSSION

Zoning actions such as a CUP require a recommendation from the HPAC to the Visalia Planning Commission. Per the Historic Preservation Ordinance, the HPAC may recommend approval, conditional approval, modification, or disapproval of an application based upon the expected impact of the proposed zoning action on the historic or architectural significance of the affected structure, neighborhood, or the entire Historic District. Staff's recommendation to the HPAC is based on the considerations listed below.

Land Use Compatibility

Residential units are "conditionally permitted" for use in the D-MU Zone, requiring approval of a CUP via a public hearing with the Visalia Planning Commission. The surrounding area consists primarily of single and multi-family residential uses, with an office and restaurant to the northwest and west, respectively. The project site itself contains two separate single-family residences that are currently occupied. Exterior alterations proposed would also restore the historic appearance of the structure, thereby improving compatibility with the Historic District. As such, the residential use of the site is considered compatible with the site and surrounding land uses.

Development Standards

No physical alterations are proposed to the structure itself that would conflict with development standards for the D-MU Zone. The applicant has stated that HVAC units attached to the building will be removed and relocated to the ground. However, the new location of the HVAC units has not been identified. Condition No. 2 is recommended, requiring the applicant to comply with all setback standards for the D-MU Zone, to be verified during Building Permit review.

For parking, the Visalia Municipal Code requires 1.5 parking stalls per unit for "multifamily zones". Since three units are proposed, the multifamily parking rate is considered appropriate. This would require placement of 4.5 parking stalls onsite. The project site contains three single car garages and a driveway that, once right of way improvements are performed, will be able to accommodate approximately 3 more vehicles, for a total of 6 parking stalls onsite. As such, staff believes that the required parking ratio has been exceeded. As it stands, pursuant to California Assembly Bill No. 2097, which was signed into law January 1, 2023, public agencies are prohibited from imposing minimum parking requirements on sites that are located within a half-mile radius of a major transit stop. The City of Visalia Transit operates bus services along "Route 1", which makes bus stops along Main Street and Mooney Boulevard. Route 1 meets the definition of a "major transit stop". Since the project site is located within a half-mile of Route 1 bus stops, the site meets parking provisions per Assembly Bill No. 2097. Thus, no additional parking is required outside what is provided onsite.

Architectural Compatibility

The building's change of use will have no effect on the architectural compatibility of the site with the Historic District and surrounding streetscape. Exterior alterations involving the removal of existing window screens will assist in restoring the buildings original appearance, as the existing screens contain thick mesh and broad wood borders that completely obstruct views of the windows. The addition of covers for HVAC piping will assist in obscuring unsightly views of pipes running along building exteriors. And modifications to stairs proposed appear to be minor and would not significantly affect its current appearance. Relocation of HVAC units, though not reviewable, will assist in removing visual impediments to the building exterior.

Landscaping

As stated the by applicant, street trees located along the project site street frontage will be removed to allow for the repair of an existing driveway, curb, gutter, and sidewalk. It is unclear whether the repairs will leave any landscaping areas within the right of way. An examination of the surrounding sites to the north and south indicates that street trees are predominant in the area. Street trees are considered an amenity that assists in beautifying neighborhoods and emphasizing the historic nature of properties on which they reside. However, since street trees are located in the right of way, they are not strictly under the jurisdiction of the HPAC. The Planning Commission conversely is able to apply conditions in matters concerning right of way areas, when they relate to land use actions. As such, it is recommended that the HPAC include in its recommendation to the Planning Commission a request that a condition of approval be added requiring the applicant to reinstall street trees along the North West Street property frontage.

FINDINGS AND CONDITIONS

Staff recommends that the Committee approve the exterior alterations proposed and recommend approval of HPAC Item No. 2024-26 to the Visalia Planning Commission, with a request to add a condition of approval requiring reinstallation of street trees, based upon the following findings:

1. The site is within the Historic District and is not listed in the Local Register of Historic Structures.
2. That the proposal is consistent with residential and commercial uses in the Historic District and the surrounding area.
3. That the proposal is in keeping with the purpose and intent of the Historic Preservation Element and Ordinance.
4. That the proposal will not be injurious to the surrounding properties or character of the Historic District due to its compatibility with the surrounding area.
5. That the Conditional Use Permit request to establish three residential units in the D-MU (Downtown Mixed Use) Zone is consistent with the Zoning Ordinance, Historic Preservation Element, and Historic Preservation Ordinance.

And subject to the following conditions:

1. That the project shall be developed in substantial compliance with the site plan in Exhibit "A", Floor Plans in Exhibit "B", Building Elevations in Exhibit "C" and "D", and HPAC Project Application in Exhibit "E".
2. That the project shall be developed in compliance with all development standards of the D-MU Zone.
3. That the applicant shall comply with all requirements of Site Plan Review No. 2024-057.
4. That the project undergoes the appropriate City permitting process.
5. That any significant changes to the proposed use, or any changes to the exterior of onsite structures, be brought back to the Historic Preservation Advisory Committee prior to any review by the Planning Commission and/or issuance of a Building Permit.
6. That all other City codes, ordinances, standards, and regulations shall be met.
7. That the approval from the Historic Preservation Advisory Committee shall lapse and become void twelve months after the date on which it became effective, unless the conditions of the approval allowed a shorter or greater time limit, or unless prior to the

HPAC 2024-26 - 405 North West Street – Three Residential Units In D-MU Zone, Exterior Alterations

expiration of twelve months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site that was the subject of the approval.

ATTACHMENTS

- Exhibit “A” – Site Plan
- Exhibit “B” – Floor Plans
- Exhibit “C” – Building Elevations
- Exhibit “D” – Proposed Window Screens and HVAC Piping Covers
- Exhibit “E” – HPAC Project Application
- Site Plan Review No. 2024-057 Revise & Proceed Comments
- Aerial Map
- Historic District and Local Register Map

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.56.060, an appeal to the City Council may be submitted within ten days following the date of a decision by the Historic Preservation Advisory Committee (HPAC). An appeal shall be in writing and shall be filed with the City Clerk at 220 N. Santa Fe Street. The appeal shall specify errors or abuses of discretion by the HPAC, or decisions not supported by the evidence in the record. The appeal form can be found on the city’s website www.visalia.city or from the City Clerk.

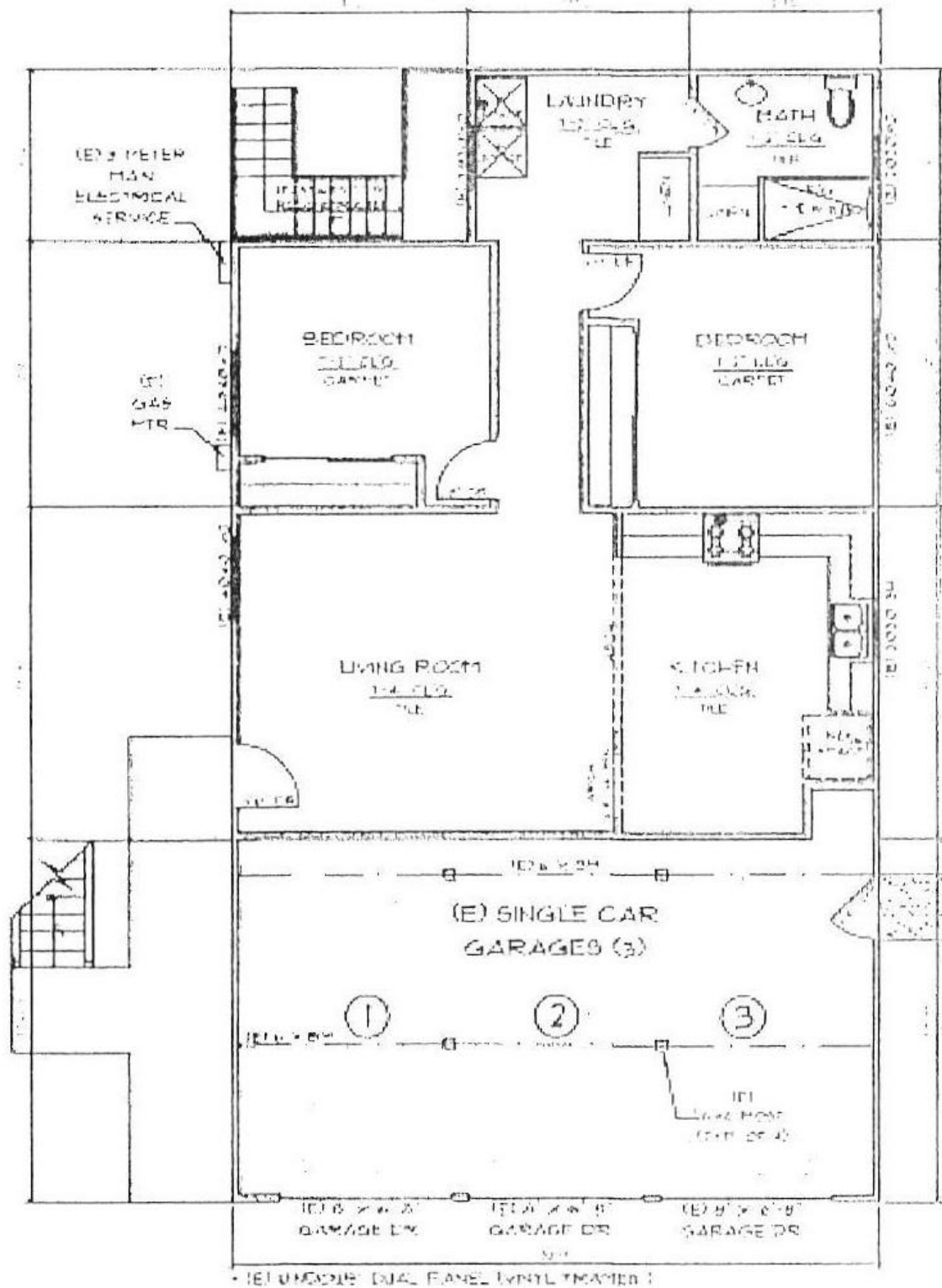
EXHIBIT "A"



**BUILDING TO
BE MODIFIED**



EXHIBIT "B"

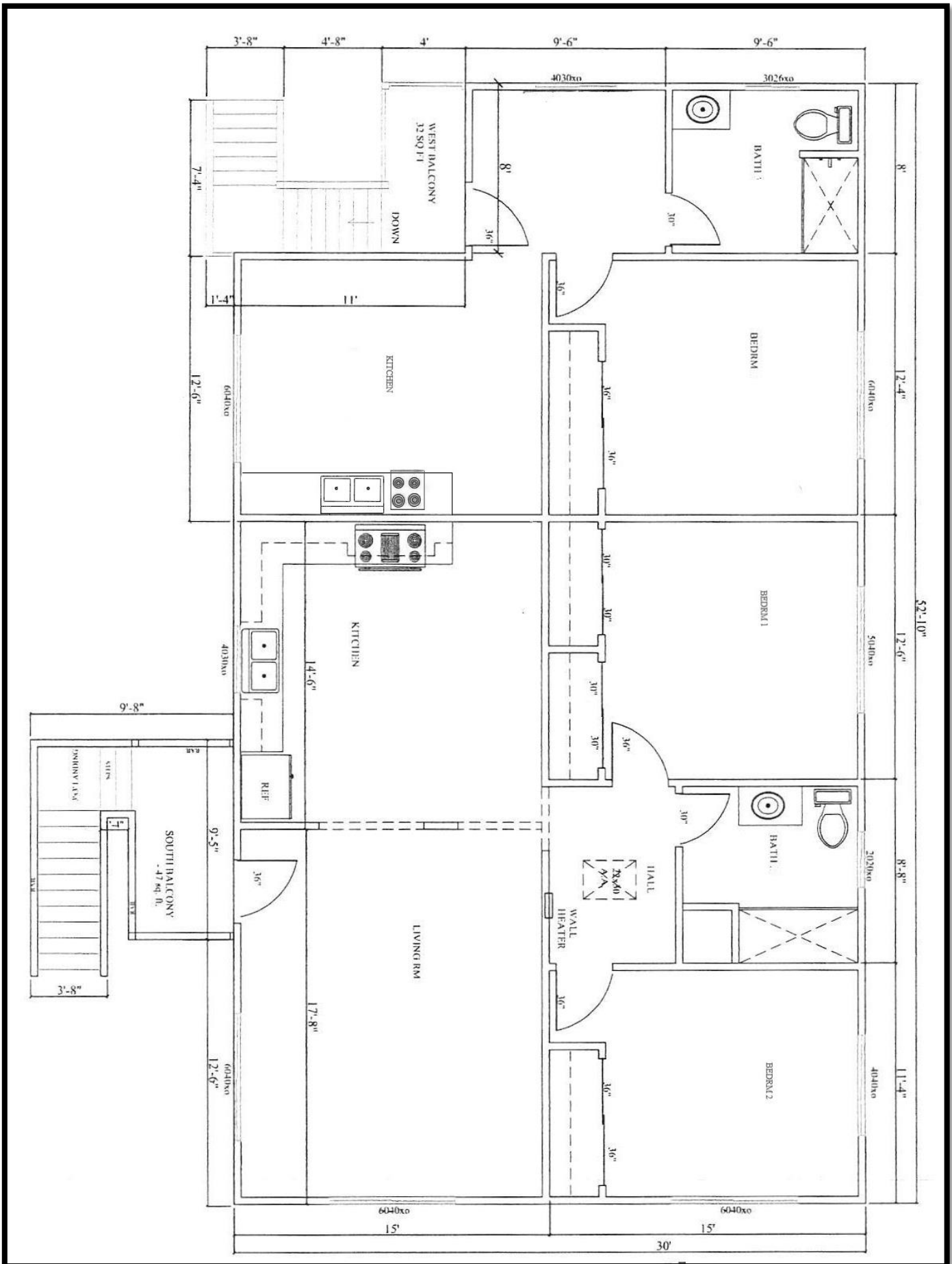


FLOOR PLAN

405 N. WEST ST.

(1ST. FLOOR / 918 S.F.)

NT 5



405 N West St Visalia CA 93291



Southside & South East of the building: Staircases with premature updates, and exterior paint and trim for approval.



North Side of the Building: Remove/relocated HVAC (premature installation) to ground level. Will provide covering for wire, cables, lines, etc.



Westside of the building: Repave/repair sidewalk and driveway. Remove window screen coverings from all windows and replace with stationary screens. Remove Maple tree located in front of the driveway entrance.

EXHIBIT “D”





Project Address: 405 N West St Visalia CA 93291
 HPAC NO.: _____

CITY OF VISALIA
HISTORIC PRESERVATION ADVISORY COMMITTEE
 REVIEW APPLICATION

LOCATION OF PROJECT: 405 N West St Visalia CA 93291 DATE: 09/11/2024
 APPLICANT/PROJECT CONTACT: Denee Fiore, Equity Group PHONE: 559-419-0641
 APPLICANT ADDRESS: 420 N Court St Visalia, CA 93291
 E-MAIL ADDRESS: denee@equitygroupinc.com APN#: 093-175-010-000
 PROPERTY OWNER: Maria Claudia Lomeli
 GENERAL DESCRIPTION OF PROJECT: Remove and relocate HVAC units on 2 of the upstairs units, repair the driveway/sidewalk by the street, install standard window screens, obtain permits for premature work for the property (i.e. paint, updated staircase), submit for approval second story plans for review/approval.
 BRIEF NARRATIVE/REASON FOR PROJECT: Property had prior premature updates that need approval with both HAPC and City of Visalia requirements. HVAC units were installed. Need to be removed off the building and relocated, Concrete repair for the driveway/sidewalk, remove current window coverings and replaced with standard stationary screens, approval for premature work to the stairways, exterior paint, and HVAC units.
 NAME & PHONE NUMBER OF PERSON ATTENDING THE MEETINGS: Denee Fiore, Equity Group
 RELATIONSHIP TO PROPERTY OWNER: Property Manager

REQUIRED MATERIALS:

Completed application
 Completed Agency Authorization form (if represented by an agent)**
 2 copies of site plan, elevations, landscape plans, etc. (as necessary)
 1 copy of 8-1/2" x 11" reduction of all plans
 NOTE: Additional materials may be requested, as necessary

**If the property owner is not the applicant, an Agency Authorization must also be submitted.

Please provide the following information as it pertains to your project (drawings and a plot plan must be submitted with this application. These plans are for the HPAC process only. Building permits require a separate application and separate materials).

1. Type of Action (check one):

_____ New Construction _____ Signs
 X _____ Alteration to existing structure _____ Moving-New Location
 _____ Other: _____
 _____ Demolition - (May require inspection by the City of Visalia Building Division)
 For further information contact Historic Preservation staff representative.

If moving or demolition permit – it is not necessary to complete the following questions.

2. Exterior Elevations:

a. Proposed Materials on exterior elevation (type and description of siding and trims):

Premature work for updated staircases and exterior paint and trim needing approval and sign off of permitted work with the City of Visalia.

Repave the driveway and sidewalk. Concrete is cracked and lifting in areas on the sidewalk. Relocated HVAC units (premature installation), provide covering for lines/wires

b. Description and type of proposed windows and doors (include material of window frame):

Remove current window screen coverings and replaced with standard stationary screens for all windows

- c. If masonry is used as an exterior material, please provide the following information:

Material: _____

Size: _____

Color: _____

3. Roof: (Please indicate proposed changes to):

Style: _____

Pitch: _____

Material: _____

4. Proposed Building Height:

Height to eave: _____

Height to peak of roof: _____

5. Setbacks: (Measurement from curb and property line to proposed structures)

- a. Setbacks on proposed project:

Front: _____

Rear: _____

Sides: _____

- b. Setbacks on adjacent properties (distance from curb is sufficient)

Front only: _____

6. Landscaping: (Indicate any mature trees on plans)

Removal of Maple tree located in front of the driveway entrance. Tree roots are causing the sidewalk to lift.

7. Signs: (If applicable)

Please provide a sketch of the proposed sign indicating colors and materials and a plot plan showing the location of the sign on the property.

Signed: _____

Agent/Property Owner

Date: _____

9/11/2024

If the application is approved, a building permit may not be issued and construction may not begin until 10 days after the Committee's decision. This is to allow appeals to be submitted, as prescribed in Article 26, Section 7712 of the City of Visalia Historic Preservation District Ordinance.

Applications for Historic Preservation must be submitted to the City of Visalia Planning Division NO LESS THAN 8 CALENDAR DAYS PRIOR to the meeting.

Applicants or their representative are requested to attend the meeting.

The Historic Preservation Advisory Committee meets every 2nd and 4th Wednesday of the month (except holidays and special circumstances) at 5:30 p.m. in the City Hall East Conference Room.

For additional information or assistance in completing this application, please feel free to call (559) 713-4359.



April 9, 2024

Site Plan Review No. 2024-057:

Pursuant to Zoning Ordinance Chapter 17.28 the Site Plan Review process has found that your application complies with the general plan, municipal code, policies, and improvement standards of the city. A copy of each Departments/Divisions comments that were discussed with you at the Site Plan Review meeting are attached to this document.

Based upon Zoning Ordinance Section 17.28.070, this is your Site Plan Review determination. However, your project requires discretionary action as stated on the attached Site Plan Review comments. You may now proceed with filing discretionary applications to the Planning Division.

This is your Site Plan Review Permit; your Site Plan Review became effective **March 27, 2024**. A site plan review permit shall lapse and become null and void one year following the date of approval unless, prior to the expiration of one year, a building permit is issued by the building official, and construction is commenced and diligently pursued toward completion.

If you have any questions regarding this action, please call the Community Development Department at (559) 713-4359.

Respectfully,

A handwritten signature in blue ink, appearing to read "Paul Bernal".

Paul Bernal
Community Development Director
315 E. Acequia Ave.
Visalia, CA 93291

Attachment(s):

- Site Plan Review Comments



MEETING DATE

March 27, 2024

SITE PLAN NO.

2024-057

PARCEL MAP NO.

SUBDIVISION

LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with

☐ Planning

☐ Engineering prior to resubmittal plans for Site Plan Review.

☐ Solid Waste

☐ Parks and Recreation

☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☐ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.

☒ Your plans must be reviewed by:

☐ CITY COUNCIL

☐ REDEVELOPMENT

☒ PLANNING COMMISSION

☐ PARK/RECREATION

☒ CUP

☒ HISTORIC PRESERVATION

☐ OTHER – Lot Line Adjustment

☒ **ADDITIONAL COMMENTS:**

If you have any questions or comments, please call the Site Plan Review Hotline at (559) 713-4440
Site Plan Review Committee

SITE PLAN REVIEW COMMENTS

Cristobal Carrillo, Planning Division, (559) 713-4443

Date: March 27, 2024

SITE PLAN NO: 2024-057

PROJECT: 405 N West Apartment

DESCRIPTION: Requesting Approval With City of Visalia Code Enforcement For Premature Construction Work Completed At The Site.

APPLICANT: Dennee Fiore – Applicant

APN: 093-175-010

LOCATION: West of North West Street, approximately 90 feet north of West School Avenue.

GENERAL PLAN: Downtown Mixed Use

ZONING: D-MU (Mixed Use Downtown)

Planning Division Recommendation:

- ☒ Revise and Proceed
☐ Resubmit

Project Requirements

- Conditional Use Permit
- Historic Preservation Advisory Committee Review
- Building Permit

**Reference Site Plan
Review No. 2018-169
and Conditional Use
Permit No. 2018-20.**

PROJECT SPECIFIC INFORMATION: March 27, 2024

1. A Conditional Use Permit (CUP) shall be required to establish residential uses in the D-MU Zone.
2. The project site is located within the Historic District. Review by the Historic Preservation Advisory Committee (HPAC) of the CUP and exterior alterations request shall be required prior to review by the Visalia Planning Commission.
3. A Site Plan shall be provided. The plan shall identify which structures are to be altered. The site plan shall also be revised to depict the correct number of residential units proposed.
4. Provide an Operational Statement describing the proposed project with the Building Permit submittal. This shall include the correct number of units proposed on the project site and all exterior alterations proposed/conducted to the buildings.
5. Provide Building Elevations. Elevations shall identify all proposed alterations to any structures onsite, and materials to be used.
6. It is highly recommended that the garage doors have a classic carriage house appearance.
7. Provide Floor Plans for the buildings to be altered. Floor plans shall depict the correct number of units proposed.
8. A Landscape plan shall be provided.
9. Project site shall have a minimum of 5% devoted to open space and amenities. Landscaped areas adjacent to public streets shall not be counted towards the open space requirement.
10. It is recommended that an amenity be provided onsite for occupant use, such as a play area, seating, BBQ grills, etc.
11. The applicant shall comply with all Good Neighbor Policies of the Visalia Municipal Code. This shall include submittal, approval, and recordation of an Operational Management Plan to the City of Visalia. Recordation of the plan shall occur prior to Building Permit issuance.
12. It is recommended that a minimum four parking stalls be provided onsite.
13. Identify the location where trash receptacles or a trash enclosure will be located.
14. Garages will be required to be usable/functional as a part of the proposed residential use.
15. Obtain a Building Permit.
16. Meet all other codes and standards

NOTES:

1. The applicant shall contact the San Joaquin Valley Air Pollution Control District to verify whether additional permits are required through the District.
2. Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.

Sections of the Municipal Code to review:

17.19 Mixed Use Zones
17.30 Development Standards
17.32.080 Maintenance of landscaped areas
17.34 Off-street parking and loading facilities
17.36 Fences Walls and Hedges

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.



Signature: _____



**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

<input type="checkbox"/> Lupe Garcia	713-4197
<input type="checkbox"/> Keyshawn Ford	713-4268
<input type="checkbox"/> Edelma Gonzalez	713-4364
<input type="checkbox"/> Sarah MacLennan	713-4271
<input checked="" type="checkbox"/> Luqman Ragabi	713-4362

ITEM NO: 7 DATE: MARCH 27, 2024

SITE PLAN NO.: 24-057
PROJECT TITLE: 405 N WEST APARTMENT
DESCRIPTION: REQUESTING APPROVAL WITH CITY OF
VISALIA CODE ENFORCEMENT FOR
PREMATURE CONSTRUCTION WORK
COMPLETED AT THIS SITE.
APPLICANT: DENEE FIORE
PROP OWNER:
LOCATION: 405 N WEST ST
APN: NW OF WEST ST AND SCHOOL

SITE PLAN REVIEW COMMENTS

☒ REQUIREMENTS (indicated by checked boxes)

- ☐ Install curb return with ramp, with _____ radius;
☐ Install curb; ☐ gutter
☐ Drive approach size: ☐ Use radius return;
☐ Sidewalk: _____ width; ☐ parkway width at _____
☒ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
☐ Right-of-way dedication required. A title report is required for verification of ownership.
☐ Deed required prior to issuing building permit;
☒ City Encroachment Permit Required. **FOR ANY WORK NEEDED WITHIN PUBLIC RIGHT-OF-WAY**
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
☐ CalTrans Encroachment Permit required. ☐ CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;
☐ Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
☐ Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
☐ Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. ☐ Prepared by registered civil engineer or project architect. ☐ All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) ☐ directed to the City's existing storm drainage system; b) ☐ directed to a permanent on-site basin; or c) ☐ directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____ : _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
☐ Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .20%, V-gutter = 0.25%)
☐ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
☐ All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.

- ☐ Traffic indexes per city standards:
- ☐ Install street striping as required by the City Engineer.
- ☐ Install landscape curbing (typical at parking lot planters).
- ☐ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☐ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☐ Provide "R" value tests: each at
- ☐ Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide wide riparian dedication from top of bank.
- ☐ Show Valley Oak trees with drip lines and adjacent grade elevations. ☐ Protect Valley Oak trees during construction in accordance with City requirements.
- ☐ A permit is required to remove Valley Oak trees. Contact Public Works Admin at 713-4428 for a Valley Oak tree evaluation or permit to remove. ☐ A pre-construction conference is required.
- ☐ Relocate existing utility poles and/or facilities.
- ☐ Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer:
- ☒ Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- ☐ If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- ☐ If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- ☐ Comply with prior comments. ☒ Resubmit with additional information. ☒ Redesign required.

Additional Comments:

- 1. Project is located in AE flood zone, comply with FEMA and local floodplain requirements. Substantial improvements will be assessed at the time of permit review.**
- 2. A building permit is required, standard plan check and inspection fees will apply.**
- 3. Proposed project will incur development Impact fees. refer to page 3 for details.**

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: **24-057**

Date: **03/27/2024**

Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date: **08/19/2023**)

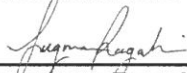
(Project type for fee rates: **MULTI-FAMILY**)

☒ Existing uses may qualify for credits on Development Impact Fees. **SFD**

FEE ITEM	FEE RATE
<input type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	\$5,025/DU X 3 = \$15,075
	CREDIT
	\$7,156/DU X 1 = \$7,156
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	\$542/DU X 3 = \$1,626
	CREDIT
	\$960/DU X 1 = \$960
<input checked="" type="checkbox"/> Treatment Plant Fee	\$953/DU X 2 = \$15,075
<input type="checkbox"/> Sewer Front Foot Fee	
<input type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input type="checkbox"/> Waterways Acquisition Fee	
<input type="checkbox"/> Public Safety Impact Fee: Police	
<input type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$614/DU X 3 = \$1,842
	CREDIT
	\$692/DU X 1 = \$692
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Luqman Ragabi

City of Visalia
Building: Site Plan
Review Comments

SFR 24057
405 N WEST APARTMENT
405 N WEST ST

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
Please refer to the applicable California Code & local ordinance for additional requirements.

- ☒ A building permit will be required. **FOR ALL IMPROVEMENTS** For information call (559) 713-4444
- ☒ Submit 1 digital set of professionally prepared plans and 1 set of calculations. (Small Tenant Improvements)
- ☐ Submit 1 digital set of plans prepared by an architect or engineer. Must comply with 2016 California Building Cod Sec. 2308 for conventional light-frame construction or submit 1 digital set of engineered calculations.
- ☐ Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- ☐ Meet State and Federal requirements for accessibility for persons with disabilities.
- ☐ A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- ☒ All accessible units required to be adaptable for persons with disabilities. **ALL GROUND FLOOR UNITS SHALL BE ADAPTABLE AND ON AN ACCESSIBLE ROUTE.**
- ☒ Maintain sound transmission control between units minimum of 50 STC. **1 HR BETWEEN UNITS (VERT. & HORIZ.)**
- ☐ Maintain fire-resistive requirements
- ☐ A demolition permit & deposit is required. For information call (559) 713-4444
- ☐ Obtain required permits from San Joaquin Valley Air Pollution Board. For information call (661) 392-5500
- ☐ Plans must be approved by the Tulare County Health Department. For information call (559) 624-8011
- ☒ Project is located in flood zone **AE** • ☐ Hazardous materials report. **MEET FEMA FLOOD REQUIREMENTS**
- ☐ Arrange for an on-site inspection. (Fee for inspection \$157.00) For information call (559) 713-4444
- ☐ School Development fees.
- ☐ Park Development fee \$ _____, per unit collected with building permits.
- ☒ Additional address may be required for each structure located on the site. For information call (559) 713-4320
- ☐ Acceptable as submitted
- ☐ No comments at this time

Additional comments: **ALL IMPROVEMENTS DONE WITHOUT THE BENEFIT OF A PERMIT SHALL BE REVIEWED TO CURRENT CODE STANDARDS. SUBMIT NEW PLANS FOR REVIEW OF TRIPLEX.**

VAL GARCIA 3/26/24
Signature

**Site Plan Comments**

Visalia Fire Department
Corbin Reed, Fire Marshal
420 N. Burke
Visalia CA 93292
559-713-4272 office
prevention.division@visalia.city

Date	March 26, 2024
Item #	7
Site Plan #	24057
APN:	093175010

- The Site Plan Review comments are issued as **general overview** of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2022 California Fire Code (CFC), 2022 California Building Codes (CBC) and City of Visalia Municipal Codes.
- All **fire detection, alarm, and extinguishing systems** in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If building has been vacant for a significant amount of time the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. 2022 CFC 901.6
- **Address numbers** must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses are served by a common driveway, the range of numbers shall be posted at the roadway/driveway. 2022 CFC 505.1
- All hardware on **exit doors, illuminated exit signs and emergency lighting** shall comply with the 2022 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- **Commercial dumpsters** with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. 2022 CFC 304.3.3
- A **Knox Box key lock system** is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. Go to knoxbox.com to order and please allow adequate time for shipping and installation. 2022 CFC 506.1

Corbin Reed
Fire Marshal



City of Visalia
Police Department
303 S. Johnson St.
Visalia, CA 93292
(559) 713-4370

Date: 03/26/24
Item: 7
Site Plan: SPR24057
Name: Austin Huerta

Site Plan Review Comments

- ☒ No Comment at this time.
- ☐ Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- ☐ Public Safety Impact Fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001.
- ☐ Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.
- ☐ Not enough information provided. Please provide additional information pertaining to:

- ☐ Territorial Reinforcement: Define property lines (private/public space).

- ☐ Access Controlled/ Restricted etc.

- ☐ lighting Concerns:

- ☐ Traffic Concerns:

- ☐ Surveillance Issues:

- ☐ Line of Sight Issues:

- ☐ Other Concerns:



SITE PLAN REVIEW DATE: 03/27/24

WASTEWATER COLLECTIONS AND PRETREATMENT DIVISION (QUALITY ASSURANCE)
SITE PLAN REVIEW COMMENTS

SITE PLAN REVIEW NO: 24057

PROJECT NAME: MULTIFAMILY RESIDENTIAL

THE PROJECT IS SUBJECT TO THE FOLLOWING REQUIREMENTS FROM WASTEWATER
PRETREATMENT DIVISION (QUALITY ASSURANCE):

☒ SUBMISSION OF WASTEWATER DISCHARGE PERMIT
APPLICATION/QUESTIONNAIRE/OTHER REGULATORY FORMS

- FORM REQUIRED RESI DEV HOUSING
- FORM REQUIRED _____
- FORM REQUIRED _____

☐ INSTALLATION OF SAND AND GREASE INTERCEPTOR

☐ INSTALLATION GREASE INTERCEPTOR

☐ OTHER _____

☐ SITE PLAN REVIEWED-NO COMMENTS

CONTACT THE WASTEWATER DEPARTMENT AT (559) 713-4466 OR
BEN.LITWACK@VISALIA.CITY, IF YOU HAVE ANY QUESTIONS.

COMMENTS

SEE ATTACHED

DATE REVIEWED: 03/26/24



RESIDENTIAL HOUSING DEVELOPMENT QUESTIONNAIRE
FOR WASTEWATER DEPARTMENT USE

- Development Name: _____
- Development Location: _____
- Contact Name: _____
- Contact Phone: _____
- Contact Email: _____
- How many homes are estimated to be built? _____
- # of Bedrooms/ Bath Info If available: _____
- When does construction plan to begin? _____
- Will housing be built in phases? _____
- What is the anticipated rate of completion? (How many houses/structures per month/year)

- Estimated month/year of project completion? _____
- Provide an estimate of wastewater characteristics (using Wastewater Engineering 3rd Edition) by Metcalf & Eddy) of the following:

FLOW _____ mgd

BOD _____ lbs/day

TSS _____ lbs/day

If you have questions regarding the completion of this form, please contact:

Jessica Sandoval
Pretreatment Coordinator
Phone: 559-713-4529
Cell: 559 309-5170
Email: Jessica.sandoval@visalia.city

Susan Currier

From: Lau, Scott@DOT <Scott.Lau@dot.ca.gov>
Sent: Tuesday, March 26, 2024 8:21 AM
To: Susan Currier
Cc: Duran, Braden@DOT; Cristobal Carrillo
Subject: Caltrans response for SPR Agenda 032724

Hi Susan,

I hope this email finds you well.

I have reviewed the Visalia SPR Agenda for March 27, 2024, and here are my findings:

1. SPR 24043-1 –Almond Joy TSM: No comments.
2. SPR 24051 – New Administration Building: **Routed for review.**
3. SPR 24052 – Multifamily Residential: No comments.
4. SPR 24054 – Single-Story Medical Office: No comments.
5. SPR 24056 – Paradise Playland: No comments.
6. **SPR 24057** – Premature Construction Work: No comments.
7. SPR 24058 – Happy Hearts Preschool: No comments.

Respectfully,

Scott Lau

Associate Transportation Planner

California Department of Transportation
District 6 Transportation Planning – Regional
1352 West Olive Ave, Fresno, CA 93728
Phone: 559.981.7341
Web: [Caltrans District 6](#)





CALIFORNIA WATER SERVICE

Visalia District 216 North Valley Oaks Drive
Visalia, CA 93292 Tel: (559) 624-1600

Site Plan Review Comments From:

California Water Service
Scott McNamara, Superintendent
216 N Valley Oaks Dr.
Visalia, CA 93292
559-624-1622 Office
smcnamara@calwater.com

Date: 03/27/2024

Item # 7

Site Plan # 24-057

Project:

Description: Premature work completed on building.

Applicant: Dennee Fiore

APN: 093-175-010

Address: 405 N West St

The following comments are applicable when checked:

☐ No New Comments

☐ **Water Mains:**
Comments:

☒ **Water Services:**

Comments: Existing service(s) at this location. If the existing service(s) is not sufficient in size to meet the customer's demand, the property owner/developer will need to request and pay for the installation of the correct size service that meets the customers demand and the abandonment of the insufficient size service. If there are additional services that may be needed, those will also be installed at the developer's expense. If the existing service(s) lands within a new drive approach, that service will be relocated at the property owner/developer's expense. If there are any existing services that will not be utilized, the property owner/developer will need to pay for the abandonment of those services. If fire sprinklers are required for your commercial building, a fire protection service will need to be installed at the property owner/developer's expense.

☒ **Fire Hydrants:**

Comments: Fire hydrants will be installed per the Visalia Fire Departments requirements. If fire hydrants are required for your project off an existing water main, Cal Water will utilize our own contractor (West Valley) for installation. This work will be paid for by the property owner/developer.

☒ **Backflow Requirements:**

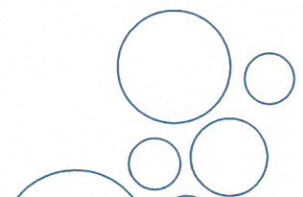
Comments: A backflow is required if any parcel is for multi-family, commercial, or has multiple services. Please contact Cross Connection Control Specialist Juan Cisneros at 559-624-1670 or visaliabackflow@calwater.com for a backflow install packet.

Additional Comments:

☒ Please contact New Business Superintendent Sedelia Sanchez at 559-624-1621 or ssanchez@calwater.com to start you project with Cal Water.

☐

☐



CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4532
COMMERCIAL BIN SERVICE

24057

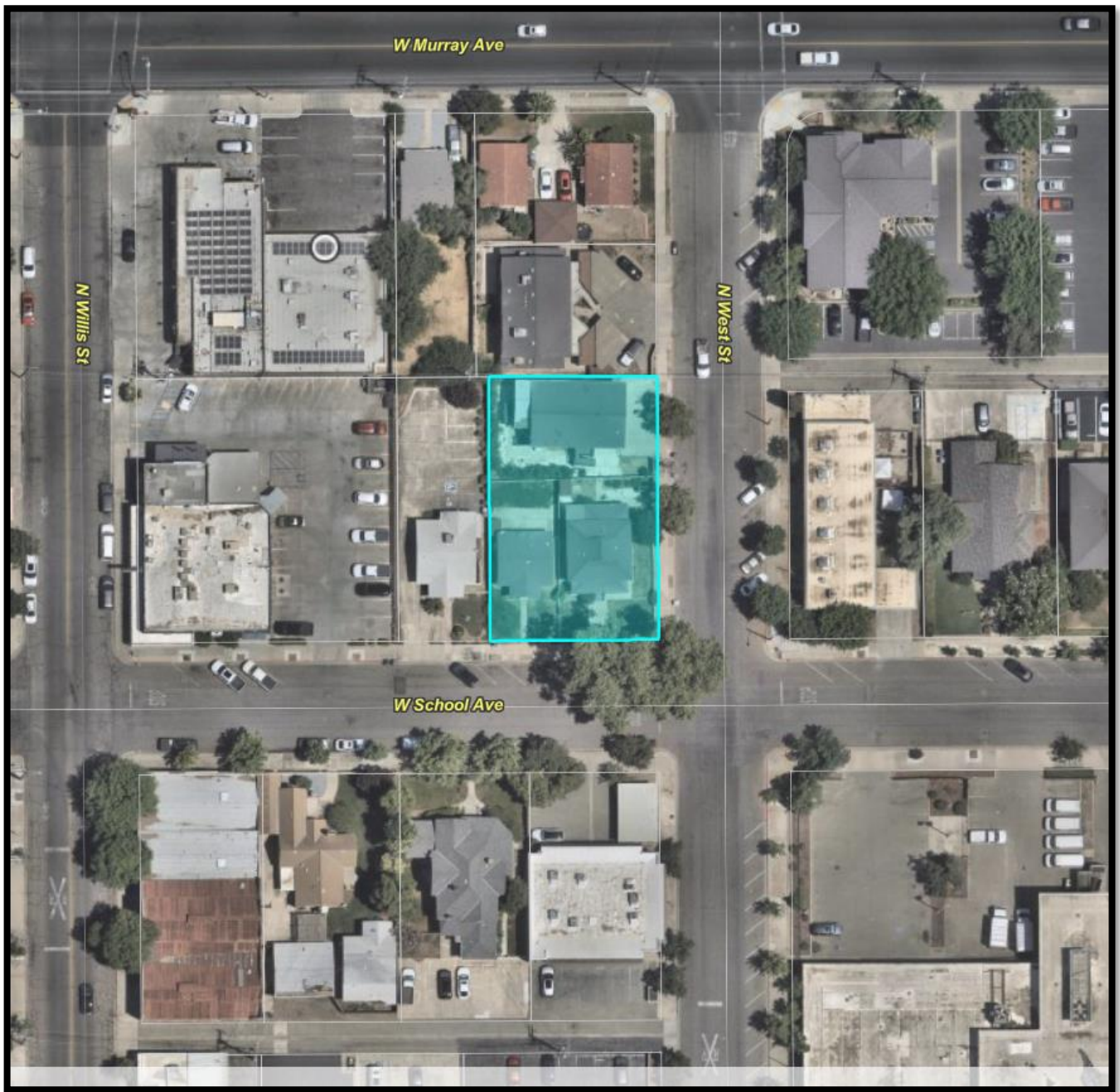
March 27, 2024

- ☐ No comments.
- ☒ See comments below
- ☐ Revisions required prior to submitting final plans. See comments below.
- ☐ Resubmittal required. See comments below.
- ☒ Customer responsible for all cardboard and other bulky recyclables to be broken down before disposing of in recycle containers
- ☐ ALL refuse enclosures must be city standard R-1 OR R-2 & R-3 OR R-4
- ☐ Customer must provide combination or keys for access to locked gates/bins
- ☐ Type of refuse service not indicated.
- ☐ Location of bin enclosure not acceptable. See comments below.
- ☐ Bin enclosure insufficient to comply with state recycling mandates. See comments for suggestions.
- ☐ Inadequate number of bins to provide sufficient service. See comments below.
- ☐ Drive approach too narrow for refuse trucks access. See comments below.
- ☐ Area not adequate for allowing refuse truck turning radius of : Commercial 50 ft. outside 36 ft. inside; Residential 35 ft. outside, 20 ft. inside.
- ☐ Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- ☐ Bin enclosure gates are required
- ☐ Hammerhead turnaround must be built per city standards.
- ☐ Cul - de - sac must be built per city standards.
- ☐ Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- ☐ Area in front of refuse enclosure must be marked off indicating no parking
- ☐ Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS) with no less than 38' clear space in front of the bin, included the front concrete pad.
- ☒ Customer will be required to roll container out to curb for service.
- ☐ Must be a concrete slab in front of enclosure as per city standards, the width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.
- ☐ Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.
- ☐ City ordinance 8.28.120-130 (effective 07/19/18) requires contractor to contract with City for removal of construction debris unless transported in equipment owned by contractor or unless contracting with a franchise permittee for removal of debris utilizing roll-off boxes.
- ☐ Solid waste collection services to include trash, recycling, and organic recycling, per the State of California's mandatory recycling laws (AB341 & AB1826). City standard (3-can) residential services to be assigned per address.
- Comment

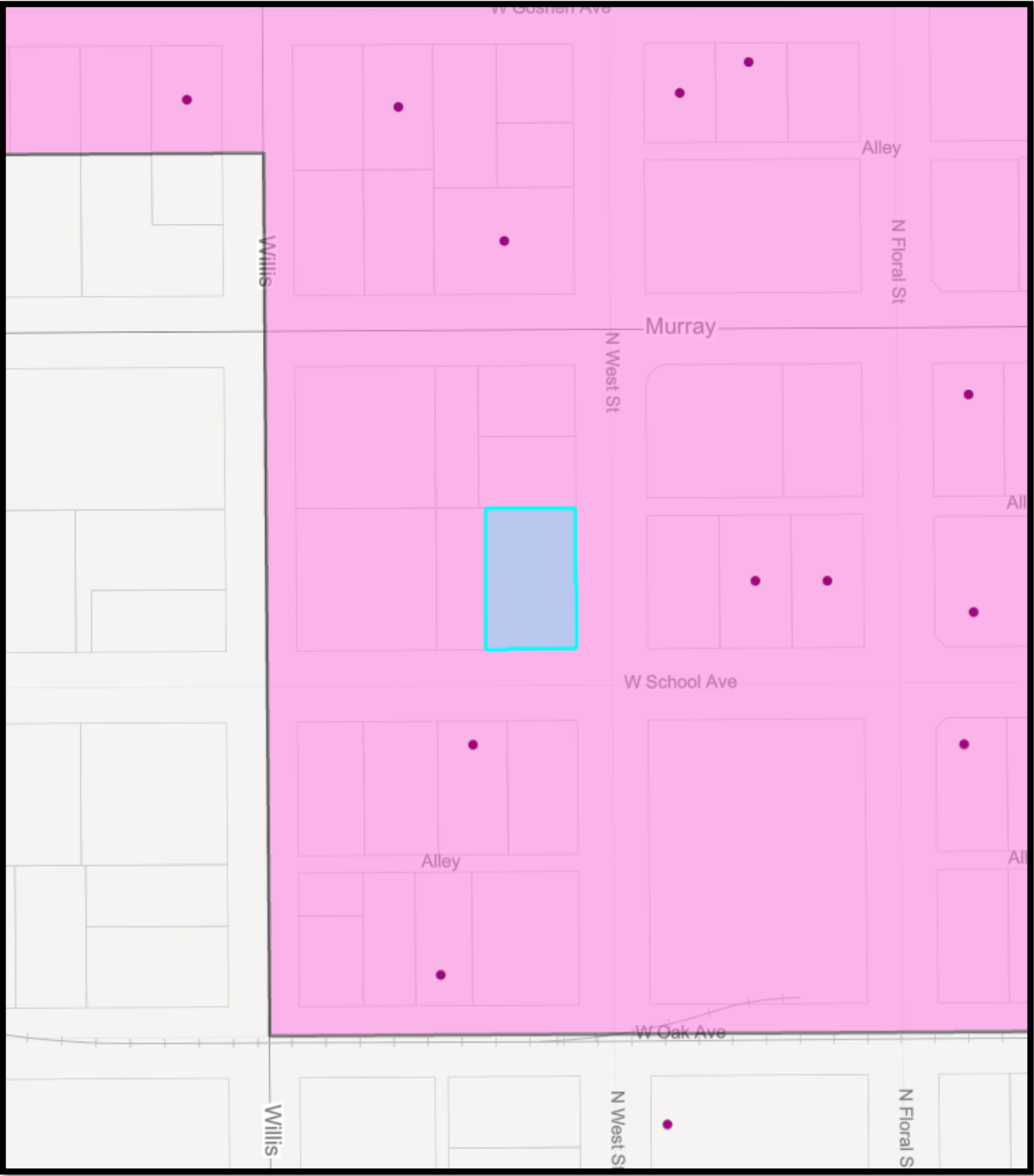
Jason Serpa, Solid Waste Manager, 559-713-4533
Edward Zuniga, Solid Waste Supervisor, 559-713-4338

Nathan Garza, Solid Waste, 559-713-4532

Aerial Map



Historic District and Local Register Map



Environmental Document # 2024-75

NOTICE OF EXEMPTION

City of Visalia
315 E. Acequia Ave.
Visalia, CA 93291

To: County Clerk
County of Tulare
County Civic Center
Visalia, CA 93291-4593

Conditional Use Permit No. 2024-46

PROJECT TITLE

405 North West Street (APN: 093-175-010)

PROJECT LOCATION

Visalia

PROJECT LOCATION - CITY

Tulare

COUNTY

A request by Maria Lomeli to establish three residential units within the D-MU (Downtown Mixed Use) Zone.

DESCRIPTION - Nature, Purpose, & Beneficiaries of Project

City of Visalia, Attn: Cristobal Carrillo, Associate Planner, 315 E. Acequia Avenue, Visalia, CA 93291, Email: cristobal.carrillo@visalia.city, (559) 713-4443

NAME OF PUBLIC AGENCY APPROVING PROJECT

Maria C. Lomeli, 1279 West Henderson Avenue, #278, Porterville, CA 93257, (559) 359-2835, claudialomeli76@gmail.com

NAME AND ADDRESS OF APPLICANT CARRYING OUT PROJECT

The Equity Group, Attn: Denee Fiore, 420 North Court Street, Visalia CA 93291, (559) 419-0641, denee@equitygroupinc.com

NAME AND ADDRESS OF AGENT CARRYING OUT PROJECT

EXEMPT STATUS: (Check one)

☐

Ministerial - Section 15268

☐

Emergency Project - Section 15269

☒

Categorical Exemption - State type and Section number: **Section 15301**

☐

Statutory Exemptions- State code number:

A request to convert an existing building into three residential units. There are no additional changes or alterations proposed to the project site.

REASON FOR PROJECT EXEMPTION

Cristobal Carrillo

CONTACT PERSON

(559) 713-4443

AREA CODE/PHONE

DATE

ENVIRONMENTAL COORDINATOR
Brandon Smith, AICP



April 9, 2024

Site Plan Review No. 2024-057:

Pursuant to Zoning Ordinance Chapter 17.28 the Site Plan Review process has found that your application complies with the general plan, municipal code, policies, and improvement standards of the city. A copy of each Departments/Divisions comments that were discussed with you at the Site Plan Review meeting are attached to this document.

Based upon Zoning Ordinance Section 17.28.070, this is your Site Plan Review determination. However, your project requires discretionary action as stated on the attached Site Plan Review comments. You may now proceed with filing discretionary applications to the Planning Division.

This is your Site Plan Review Permit; your Site Plan Review became effective **March 27, 2024**. A site plan review permit shall lapse and become null and void one year following the date of approval unless, prior to the expiration of one year, a building permit is issued by the building official, and construction is commenced and diligently pursued toward completion.

If you have any questions regarding this action, please call the Community Development Department at (559) 713-4359.

Respectfully,

A handwritten signature in blue ink, appearing to read "Paul Bernal".

Paul Bernal
Community Development Director
315 E. Acequia Ave.
Visalia, CA 93291

Attachment(s):

- Site Plan Review Comments



MEETING DATE

March 27, 2024

SITE PLAN NO.

2024-057

PARCEL MAP NO.

SUBDIVISION

LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with

☐ Planning

☐ Engineering prior to resubmittal plans for Site Plan Review.

☐ Solid Waste

☐ Parks and Recreation

☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☐ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.

☒ Your plans must be reviewed by:

☐ CITY COUNCIL

☐ REDEVELOPMENT

☒ PLANNING COMMISSION

☐ PARK/RECREATION

☒ CUP

☒ HISTORIC PRESERVATION

☐ OTHER – Lot Line Adjustment

☒ **ADDITIONAL COMMENTS:**

If you have any questions or comments, please call the Site Plan Review Hotline at (559) 713-4440
Site Plan Review Committee

SITE PLAN REVIEW COMMENTS

Cristobal Carrillo, Planning Division, (559) 713-4443

Date: March 27, 2024

SITE PLAN NO: 2024-057

PROJECT: 405 N West Apartment

DESCRIPTION: Requesting Approval With City of Visalia Code Enforcement For Premature Construction Work Completed At The Site.

APPLICANT: Dennee Fiore – Applicant

APN: 093-175-010

LOCATION: West of North West Street, approximately 90 feet north of West School Avenue.

GENERAL PLAN: Downtown Mixed Use

ZONING: D-MU (Mixed Use Downtown)

Planning Division Recommendation:

- ☒ Revise and Proceed
☐ Resubmit

Project Requirements

- Conditional Use Permit
- Historic Preservation Advisory Committee Review
- Building Permit

**Reference Site Plan
Review No. 2018-169
and Conditional Use
Permit No. 2018-20.**

PROJECT SPECIFIC INFORMATION: March 27, 2024

1. A Conditional Use Permit (CUP) shall be required to establish residential uses in the D-MU Zone.
2. The project site is located within the Historic District. Review by the Historic Preservation Advisory Committee (HPAC) of the CUP and exterior alterations request shall be required prior to review by the Visalia Planning Commission.
3. A Site Plan shall be provided. The plan shall identify which structures are to be altered. The site plan shall also be revised to depict the correct number of residential units proposed.
4. Provide an Operational Statement describing the proposed project with the Building Permit submittal. This shall include the correct number of units proposed on the project site and all exterior alterations proposed/conducted to the buildings.
5. Provide Building Elevations. Elevations shall identify all proposed alterations to any structures onsite, and materials to be used.
6. It is highly recommended that the garage doors have a classic carriage house appearance.
7. Provide Floor Plans for the buildings to be altered. Floor plans shall depict the correct number of units proposed.
8. A Landscape plan shall be provided.
9. Project site shall have a minimum of 5% devoted to open space and amenities. Landscaped areas adjacent to public streets shall not be counted towards the open space requirement.
10. It is recommended that an amenity be provided onsite for occupant use, such as a play area, seating, BBQ grills, etc.
11. The applicant shall comply with all Good Neighbor Policies of the Visalia Municipal Code. This shall include submittal, approval, and recordation of an Operational Management Plan to the City of Visalia. Recordation of the plan shall occur prior to Building Permit issuance.
12. It is recommended that a minimum four parking stalls be provided onsite.
13. Identify the location where trash receptacles or a trash enclosure will be located.
14. Garages will be required to be usable/functional as a part of the proposed residential use.
15. Obtain a Building Permit.
16. Meet all other codes and standards

NOTES:

1. The applicant shall contact the San Joaquin Valley Air Pollution Control District to verify whether additional permits are required through the District.
2. Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.

Sections of the Municipal Code to review:

17.19 Mixed Use Zones
17.30 Development Standards
17.32.080 Maintenance of landscaped areas
17.34 Off-street parking and loading facilities
17.36 Fences Walls and Hedges

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.



Signature: _____



**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

<input type="checkbox"/> Lupe Garcia	713-4197
<input type="checkbox"/> Keyshawn Ford	713-4268
<input type="checkbox"/> Edelma Gonzalez	713-4364
<input type="checkbox"/> Sarah MacLennan	713-4271
<input checked="" type="checkbox"/> Luqman Ragabi	713-4362

ITEM NO: 7 DATE: MARCH 27, 2024

SITE PLAN NO.: 24-057
PROJECT TITLE: 405 N WEST APARTMENT
DESCRIPTION: REQUESTING APPROVAL WITH CITY OF
VISALIA CODE ENFORCEMENT FOR
PREMATURE CONSTRUCTION WORK
COMPLETED AT THIS SITE.
APPLICANT: DENEE FIORE
PROP OWNER:
LOCATION: 405 N WEST ST
APN: NW OF WEST ST AND SCHOOL

SITE PLAN REVIEW COMMENTS

☒ REQUIREMENTS (indicated by checked boxes)

- ☐ Install curb return with ramp, with _____ radius;
☐ Install curb; ☐ gutter
☐ Drive approach size: ☐ Use radius return;
☐ Sidewalk: _____ width; ☐ parkway width at _____
☒ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
☐ Right-of-way dedication required. A title report is required for verification of ownership.
☐ Deed required prior to issuing building permit;
☒ City Encroachment Permit Required. **FOR ANY WORK NEEDED WITHIN PUBLIC RIGHT-OF-WAY**
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
☐ CalTrans Encroachment Permit required. ☐ CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;
☐ Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
☐ Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
☐ Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. ☐ Prepared by registered civil engineer or project architect. ☐ All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) ☐ directed to the City's existing storm drainage system; b) ☐ directed to a permanent on-site basin; or c) ☐ directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____ : _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
☐ Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .20%, V-gutter = 0.25%)
☐ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
☐ All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.

- ☐ Traffic indexes per city standards:
- ☐ Install street striping as required by the City Engineer.
- ☐ Install landscape curbing (typical at parking lot planters).
- ☐ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☐ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☐ Provide "R" value tests: each at
- ☐ Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide wide riparian dedication from top of bank.
- ☐ Show Valley Oak trees with drip lines and adjacent grade elevations. ☐ Protect Valley Oak trees during construction in accordance with City requirements.
- ☐ A permit is required to remove Valley Oak trees. Contact Public Works Admin at 713-4428 for a Valley Oak tree evaluation or permit to remove. ☐ A pre-construction conference is required.
- ☐ Relocate existing utility poles and/or facilities.
- ☐ Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer:
- ☒ Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- ☐ If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- ☐ If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- ☐ Comply with prior comments. ☒ Resubmit with additional information. ☒ Redesign required.

Additional Comments:

- 1. Project is located in AE flood zone, comply with FEMA and local floodplain requirements. Substantial improvements will be assessed at the time of permit review.**
- 2. A building permit is required, standard plan check and inspection fees will apply.**
- 3. Proposed project will incur development Impact fees. refer to page 3 for details.**

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: **24-057**

Date: **03/27/2024**

Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date: **08/19/2023**)

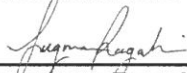
(Project type for fee rates: **MULTI-FAMILY**)

☒ Existing uses may qualify for credits on Development Impact Fees. **SFD**

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	\$5,025/DU X 3 = \$15,075
	CREDIT
	\$7,156/DU X 1 = \$7,156
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	\$542/DU X 3 = \$1,626
	CREDIT
	\$960/DU X 1 = \$960
<input checked="" type="checkbox"/> Treatment Plant Fee	\$953/DU X 2 = \$15,075
<input type="checkbox"/> Sewer Front Foot Fee	
<input type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input type="checkbox"/> Waterways Acquisition Fee	
<input type="checkbox"/> Public Safety Impact Fee: Police	
<input type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$614/DU X 3 = \$1,842
	CREDIT
	\$692/DU X 1 = \$692
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Luqman Ragabi

City of Visalia
Building: Site Plan
Review Comments

SPR 24057
405 N WEST APARTMENT
405 N WEST ST

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
Please refer to the applicable California Code & local ordinance for additional requirements.

- ☒ A building permit will be required. **FOR ALL IMPROVEMENTS** For information call (559) 713-4444
- ☒ Submit 1 digital set of professionally prepared plans and 1 set of calculations. (Small Tenant Improvements)
- ☐ Submit 1 digital set of plans prepared by an architect or engineer. Must comply with 2016 California Building Cod Sec. 2308 for conventional light-frame construction or submit 1 digital set of engineered calculations.
- ☐ Indicate abandoned wells, septic systems and excavations on construction plans.
- ☐ You are responsible to ensure compliance with the following checked items:
- ☐ Meet State and Federal requirements for accessibility for persons with disabilities.
- ☐ A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- ☒ All accessible units required to be adaptable for persons with disabilities. **ALL GROUND FLOOR UNITS SHALL BE ADAPTABLE AND ON AN ACCESSIBLE ROUTE.**
- ☒ Maintain sound transmission control between units minimum of 50 STC. **1 HR BETWEEN UNITS (VERT. & HORIZ.)**
- ☐ Maintain fire-resistive requirements
- ☐ A demolition permit & deposit is required. For information call (559) 713-4444
- ☐ Obtain required permits from San Joaquin Valley Air Pollution Board. For information call (661) 392-5500
- ☐ Plans must be approved by the Tulare County Health Department. For information call (559) 624-8011
- ☒ Project is located in flood zone **AE** • ☐ Hazardous materials report. **MEET FEMA FLOOD REQUIREMENTS**
- ☐ Arrange for an on-site inspection. (Fee for inspection \$157.00) For information call (559) 713-4444
- ☐ School Development fees.
- ☐ Park Development fee \$ _____, per unit collected with building permits.
- ☒ Additional address may be required for each structure located on the site. For information call (559) 713-4320
- ☐ Acceptable as submitted
- ☐ No comments at this time

Additional comments: **ALL IMPROVEMENTS DONE WITHOUT THE BENEFIT OF A PERMIT SHALL BE REVIEWED TO CURRENT CODE STANDARDS. SUBMIT NEW PLANS FOR REVIEW OF TRIPLEX.**

VAL GARCIA 3/26/24
Signature



Site Plan Comments

Visalia Fire Department
Corbin Reed, Fire Marshal
420 N. Burke
Visalia CA 93292
559-713-4272 office
prevention.division@visalia.city

Date	March 26, 2024
Item #	7
Site Plan #	24057
APN:	093175010

- The Site Plan Review comments are issued as **general overview** of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2022 California Fire Code (CFC), 2022 California Building Codes (CBC) and City of Visalia Municipal Codes.
- All **fire detection, alarm, and extinguishing systems** in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If building has been vacant for a significant amount of time the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. 2022 CFC 901.6
- **Address numbers** must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses are served by a common driveway, the range of numbers shall be posted at the roadway/driveway. 2022 CFC 505.1
- All hardware on **exit doors, illuminated exit signs and emergency lighting** shall comply with the 2022 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- **Commercial dumpsters** with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. 2022 CFC 304.3.3
- A **Knox Box key lock system** is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. Go to knoxbox.com to order and please allow adequate time for shipping and installation. 2022 CFC 506.1

Corbin Reed
Fire Marshal



City of Visalia
Police Department
303 S. Johnson St.
Visalia, CA 93292
(559) 713-4370

Date: 03/26/24
Item: 7
Site Plan: SPR24057
Name: Austin Huerta

Site Plan Review Comments

- ☒ No Comment at this time.
- ☐ Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- ☐ Public Safety Impact Fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001.
- ☐ Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.
- ☐ Not enough information provided. Please provide additional information pertaining to:

- ☐ Territorial Reinforcement: Define property lines (private/public space).

- ☐ Access Controlled/ Restricted etc.

- ☐ lighting Concerns:

- ☐ Traffic Concerns:

- ☐ Surveillance Issues:

- ☐ Line of Sight Issues:

- ☐ Other Concerns:



SITE PLAN REVIEW DATE: 03/27/24

WASTEWATER COLLECTIONS AND PRETREATMENT DIVISION (QUALITY ASSURANCE)
SITE PLAN REVIEW COMMENTS

SITE PLAN REVIEW NO: 24057

PROJECT NAME: MULTIFAMILY RESIDENTIAL

THE PROJECT IS SUBJECT TO THE FOLLOWING REQUIREMENTS FROM WASTEWATER
PRETREATMENT DIVISION (QUALITY ASSURANCE):

☒ SUBMISSION OF WASTEWATER DISCHARGE PERMIT
APPLICATION/QUESTIONNAIRE/OTHER REGULATORY FORMS

- FORM REQUIRED RESI DEV HOUSING
- FORM REQUIRED _____
- FORM REQUIRED _____

☐ INSTALLATION OF SAND AND GREASE INTERCEPTOR

☐ INSTALLATION GREASE INTERCEPTOR

☐ OTHER _____

☐ SITE PLAN REVIEWED-NO COMMENTS

CONTACT THE WASTEWATER DEPARTMENT AT (559) 713-4466 OR
BEN.LITWACK@VISALIA.CITY, IF YOU HAVE ANY QUESTIONS.

COMMENTS

SEE ATTACHED

DATE REVIEWED: 03/26/24



RESIDENTIAL HOUSING DEVELOPMENT QUESTIONNAIRE
FOR WASTEWATER DEPARTMENT USE

- Development Name: _____
- Development Location: _____
- Contact Name: _____
- Contact Phone: _____
- Contact Email: _____
- How many homes are estimated to be built? _____
- # of Bedrooms/ Bath Info If available: _____
- When does construction plan to begin? _____
- Will housing be built in phases? _____
- What is the anticipated rate of completion? (How many houses/structures per month/year)

- Estimated month/year of project completion? _____
- Provide an estimate of wastewater characteristics (using Wastewater Engineering 3rd Edition) by Metcalf & Eddy) of the following:

FLOW _____ mgd

BOD _____ lbs/day

TSS _____ lbs/day

If you have questions regarding the completion of this form, please contact:

Jessica Sandoval
Pretreatment Coordinator
Phone: 559-713-4529
Cell: 559 309-5170
Email: Jessica.sandoval@visalia.city

Susan Currier

From: Lau, Scott@DOT <Scott.Lau@dot.ca.gov>
Sent: Tuesday, March 26, 2024 8:21 AM
To: Susan Currier
Cc: Duran, Braden@DOT; Cristobal Carrillo
Subject: Caltrans response for SPR Agenda 032724

Hi Susan,

I hope this email finds you well.

I have reviewed the Visalia SPR Agenda for March 27, 2024, and here are my findings:

1. SPR 24043-1 –Almond Joy TSM: No comments.
2. SPR 24051 – New Administration Building: **Routed for review.**
3. SPR 24052 – Multifamily Residential: No comments.
4. SPR 24054 – Single-Story Medical Office: No comments.
5. SPR 24056 – Paradise Playland: No comments.
6. **SPR 24057** – Premature Construction Work: No comments.
7. SPR 24058 – Happy Hearts Preschool: No comments.

Respectfully,

Scott Lau

Associate Transportation Planner

California Department of Transportation
District 6 Transportation Planning – Regional
1352 West Olive Ave, Fresno, CA 93728
Phone: 559.981.7341
Web: [Caltrans District 6](#)





CALIFORNIA WATER SERVICE

Visalia District 216 North Valley Oaks Drive
Visalia, CA 93292 *Tel:* (559) 624-1600

Site Plan Review Comments From:

California Water Service
Scott McNamara, Superintendent
216 N Valley Oaks Dr.
Visalia, CA 93292
559-624-1622 Office
smcnamara@calwater.com

Date: 03/27/2024

Item # 7

Site Plan # 24-057

Project:

Description: Premature work completed on building.

Applicant: Dennee Fiore

APN: 093-175-010

Address: 405 N West St

The following comments are applicable when checked:

☐ No New Comments

☐ **Water Mains:**
Comments:

☒ **Water Services:**

Comments: Existing service(s) at this location. If the existing service(s) is not sufficient in size to meet the customer's demand, the property owner/developer will need to request and pay for the installation of the correct size service that meets the customers demand and the abandonment of the insufficient size service. If there are additional services that may be needed, those will also be installed at the developer's expense. If the existing service(s) lands within a new drive approach, that service will be relocated at the property owner/developer's expense. If there are any existing services that will not be utilized, the property owner/developer will need to pay for the abandonment of those services. If fire sprinklers are required for your commercial building, a fire protection service will need to be installed at the property owner/developer's expense.

☒ **Fire Hydrants:**

Comments: Fire hydrants will be installed per the Visalia Fire Departments requirements. If fire hydrants are required for your project off an existing water main, Cal Water will utilize our own contractor (West Valley) for installation. This work will be paid for by the property owner/developer.

☒ **Backflow Requirements:**

Comments: A backflow is required if any parcel is for multi-family, commercial, or has multiple services. Please contact Cross Connection Control Specialist Juan Cisneros at 559-624-1670 or visaliabackflow@calwater.com for a backflow install packet.

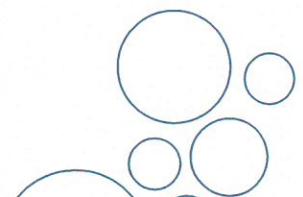
Additional Comments:

☒ Please contact New Business Superintendent Sedelia Sanchez at 559-624-1621 or ssanchez@calwater.com to start you project with Cal Water.

☐

☐

Quality. Service. Value.
calwater.com



CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4532
COMMERCIAL BIN SERVICE

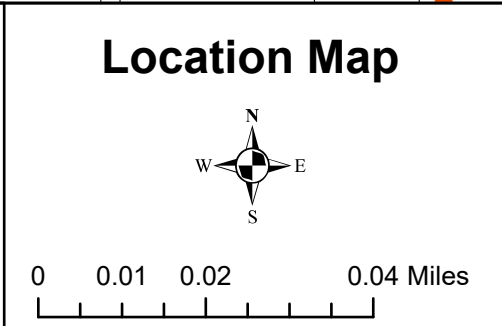
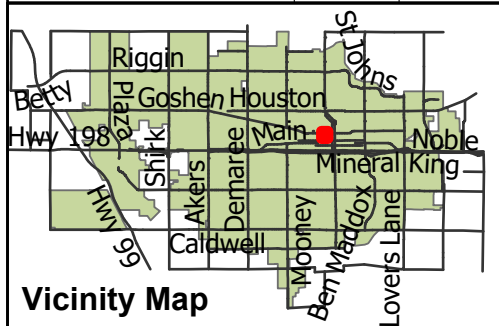
24057

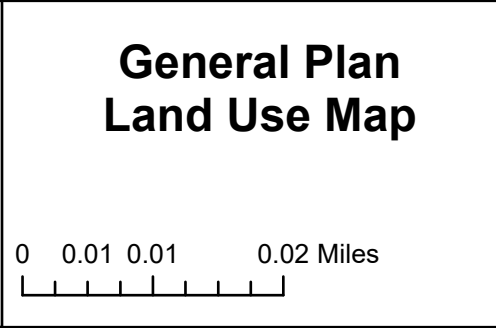
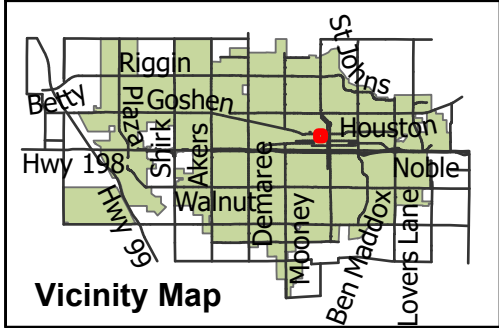
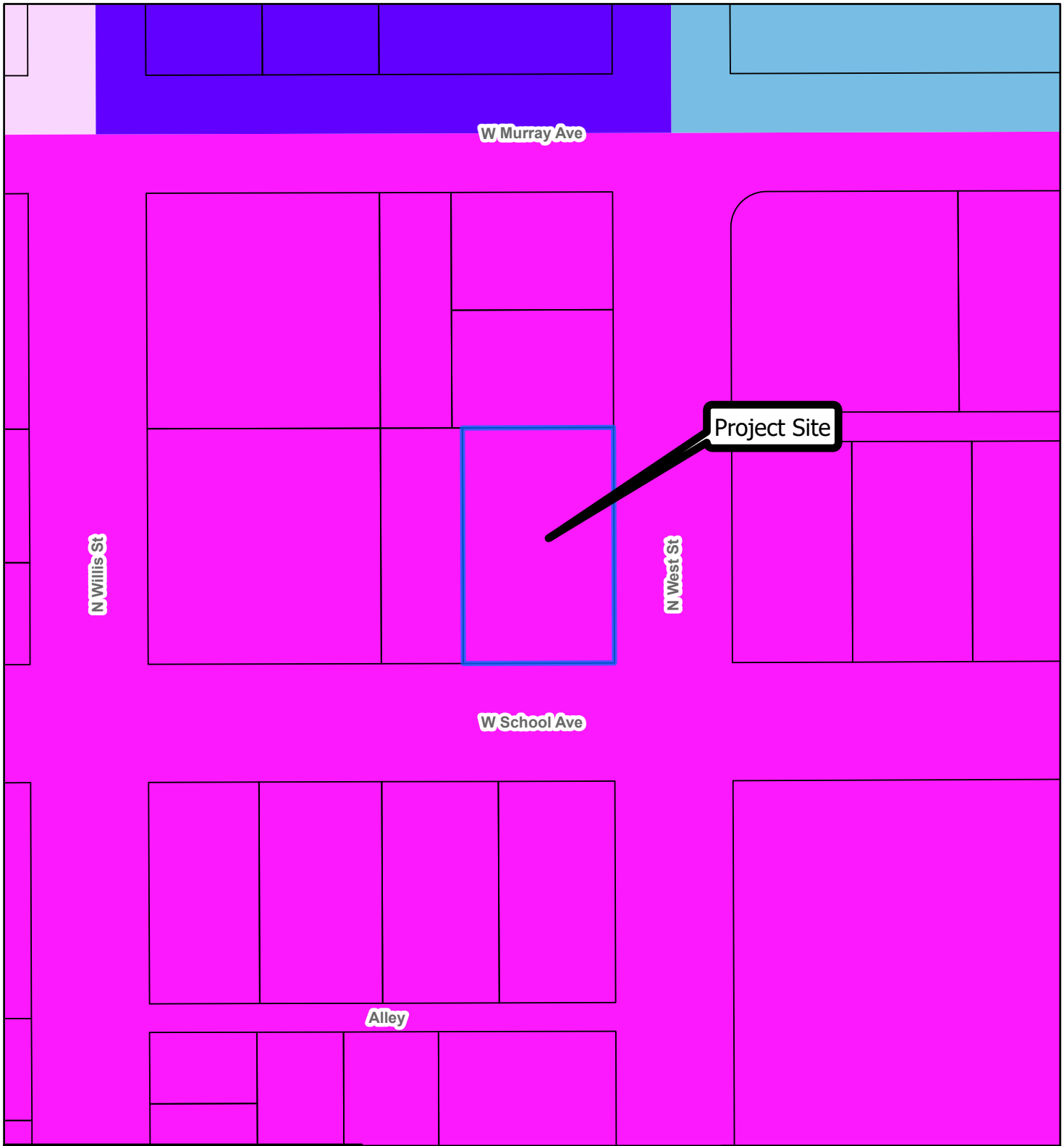
March 27, 2024

- ☐ No comments.
- ☒ See comments below
- ☐ Revisions required prior to submitting final plans. See comments below.
- ☐ Resubmittal required. See comments below.
- ☒ Customer responsible for all cardboard and other bulky recyclables to be broken down before disposing of in recycle containers
- ☐ ALL refuse enclosures must be city standard R-1 OR R-2 & R-3 OR R-4
- ☐ Customer must provide combination or keys for access to locked gates/bins
- ☐ Type of refuse service not indicated.
- ☐ Location of bin enclosure not acceptable. See comments below.
- ☐ Bin enclosure insufficient to comply with state recycling mandates. See comments for suggestions.
- ☐ Inadequate number of bins to provide sufficient service. See comments below.
- ☐ Drive approach too narrow for refuse trucks access. See comments below.
- ☐ Area not adequate for allowing refuse truck turning radius of : Commercial 50 ft. outside 36 ft. inside; Residential 35 ft. outside, 20 ft. inside.
- ☐ Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- ☐ Bin enclosure gates are required
- ☐ Hammerhead turnaround must be built per city standards.
- ☐ Cul - de - sac must be built per city standards.
- ☐ Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- ☐ Area in front of refuse enclosure must be marked off indicating no parking
- ☐ Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS) with no less than 38' clear space in front of the bin, included the front concrete pad.
- ☒ Customer will be required to roll container out to curb for service.
- ☐ Must be a concrete slab in front of enclosure as per city standards, the width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.
- ☐ Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.
- ☐ City ordinance 8.28.120-130 (effective 07/19/18) requires contractor to contract with City for removal of construction debris unless transported in equipment owned by contractor or unless contracting with a franchise permittee for removal of debris utilizing roll-off boxes.
- ☐ Solid waste collection services to include trash, recycling, and organic recycling, per the State of California's mandatory recycling laws (AB341 & AB1826). City standard (3-can) residential services to be assigned per address.
- Comment

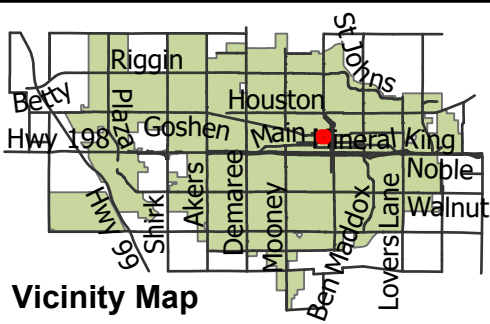
Jason Serpa, Solid Waste Manager, 559-713-4533
Edward Zuniga, Solid Waste Supervisor, 559-713-4338

Nathan Garza, Solid Waste, 559-713-4532

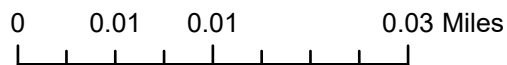


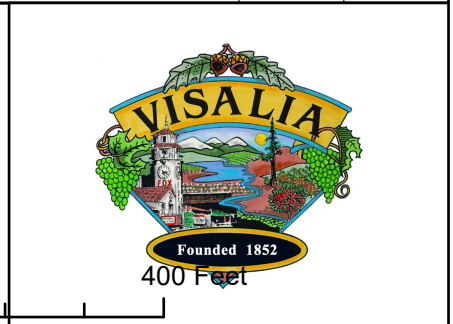
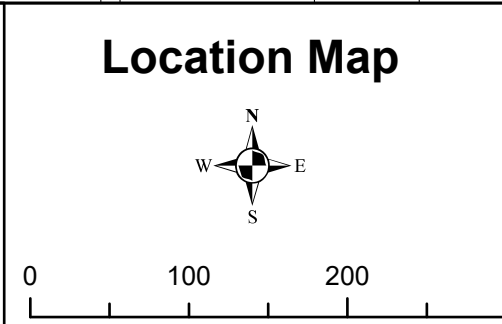
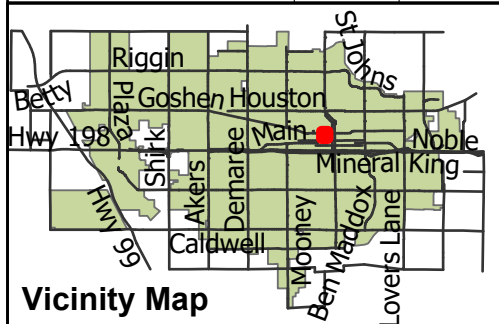






Aerial Map







REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE:

May 12, 2025

PROJECT PLANNER:

Josh Dan, Senior Planner

Phone No.: (559) 713-4003

E-mail: josh.dan@visalia.city

SUBJECT: Conditional Use Permit 2025-11: A request to construct a fast-food restaurant measuring 2,003 square feet with drive-thru and indoor dining, on a parcel measuring 0.79-acres in the Mixed-Use Commercial (C-MU) Zone. The project site is located along South Demaree Street, approximately 296-feet south of West Caldwell Avenue (Address: N/A) (APN:121-580-008).

STAFF RECOMMENDATION

Staff recommends approval of Conditional Use Permit No. 2025-11, based upon the findings and conditions in Resolution No. 2025-26. Staff's recommendation is based on the conclusion that the request is consistent with the General Plan, Zoning Ordinance and Demaree/Caldwell Specific Plan.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2025-11 based on the findings and conditions in Resolution No. 2025-26.

PROJECT DESCRIPTION

Conditional Use Permit No. 2025-11 is a request to construct a Jack in the Box (i.e. fast-food) restaurant. The building is proposed to measure 2,003 square feet with a drive-thru lane (see Exhibit "A"). The Visalia Municipal Code (VMC) Table 17.25.030 ("Zoning Matrix"), Line D7 lists drive-thru lanes not meeting the drive-thru performance standards of VMC Section 17.32.162, are conditionally permitted in the C-MU zone. The Jack in the Box will be developed within the Demaree/Caldwell Specific Plan along the south Demaree street frontage, approximately 296-feet south of West Caldwell Avenue. Access to the proposed development will be provided from the South Demaree Street via an existing vehicular access point and additionally by the interior drive aisle which can provide access to West Caldwell Avenue (see Exhibit "A").

The fast-food restaurant, as seen in Exhibit "A", is proposed to be oriented in a north-south orientation. The drive-thru lane, as designed, can accommodate up to 16 vehicles. The floor plan (see Exhibit "B") depicts the interior layout of a fast-food restaurant which includes food service areas and minimal indoor dining room for 24 patrons. The elevations provided (see Exhibit "C") identify the building materials proposed for this Jack in the Box restaurant.

BACKGROUND INFORMATION

General Plan Land Use Designation:	Commercial Neighborhood
Zoning:	C-MU (Mixed-Use Commercial)
Surrounding Zoning and Land Use:	North: C-MU (Mixed-Use Commercial) – CVS Drugstore (Demaree/Caldwell Specific Plan)
	South: C-MU (Mixed-Use Commercial) – Vacant

	(Demaree/Caldwell Specific Plan)
East:	C-MU (Mixed-Use Commercial) – Vacant (Demaree/Caldwell Specific Plan – entitled carwash CUP No. 2023-24 & TPM No. 2023-08)
West:	C-MU (Mixed-Use Commercial) – South Demaree Street – Church and Vacant Land
Environmental Review:	Categorical Exemption No. 2025-18
Special Districts:	Demaree/Caldwell Specific Plan
Site Plan:	2025-002

RELATED PLANS AND POLICIES

All related plans and policies are reprinted in the attachment to this staff report entitled “Related Plans and Policies”.

RELATED PROJECTS

The Demaree/Caldwell Specific Plan was passed and adopted by the Visalia City Council May 24, 1999, approving the development plan for a 66-acre commercial, professional office, and multi-family residential development located in the south and southeast areas of West Caldwell Avenue and South Demaree Street. Link to Demaree/Caldwell Specific Plan:

<https://www.visalia.city/civicax/filebank/blobdload.aspx?BlobID=4522>

On September 11, 2006, the Planning Commission approved Specific Plan Amendment No. 2006-02 and Conditional Use Permit No. 2006-32 amending the previously adopted 1999 Demaree/Caldwell Specific Plan which demonstrated a fast-food restaurant with drive-thru at the southeast corner of West Caldwell Avenue and South Demaree Street for a Long’s Drug store and pharmacy with drive-thru.

PROJECT EVALUATION

Staff recommends approval of the requested Conditional Use Permit based on project consistency with the General Plan, the Demaree/Caldwell Specific Plan and the Zoning Ordinance.

Land Use Compatibility

The Visalia Zoning Matrix identifies drive-thrus, not meeting the drive-thru performance standards as required per VMC Section 17.32.162, as conditionally permitted in the C-MU zone. Through the CUP process, potential impacts can be addressed thereby ensuring compatibility between the proposed use and existing surrounding uses. Staff has concluded that the proposed drive-thru will not have a negative impact on surrounding uses and complement other commercial uses within proximity to West Caldwell Avenue and South Demaree Street as well as other commercial uses within the area.

A CUP is necessary as the proposed drive-thru does not comply with subsection B(1) of the above reference section, in which the drive-thru is within 250 feet of a residence located to the south of the project site.

At the Site Plan Review meeting held on January 8, 2025, the applicant informed staff that the Jack in the Box will operate seven days a week with a 24-hour operation format, consistent with all other Jack in the Boxes in the City. The Jack in the Box site plan (see Exhibit “A”) depicts the placement of two menu boards and speaker boxes - one in each of the two drive-thru approaches - with an orientation northward toward the existing CVS. The distance from the

speaker box to the closest residential structure is ± 300 feet. Noises emitted from the speaker box of the drive-thru are required to meet the community noise standards as set forth in Chapter 8.36 (Noise Ordinance) of the Municipal Code. The requirement to comply with the City's Noise Ordinance is included as Condition No. 6 of the Conditions of Approval for the project.

On September 11, 2006, the Planning Commission approved Specific Plan Amendment No. 2006-02 and Conditional Use Permit No. 2006-32 amending the previously adopted 1999 Demaree/Caldwell Specific Plan which demonstrated a fast-food restaurant with drive-thru at the southeast corner of West Caldwell Avenue and South Demaree Street for a Long's Drug store and pharmacy with drive-thru. Condition of Approval No. 5 of CUP No. 2006-32 required that a seven-foot-high block wall be constructed along the southern property line of Subarea "B". The wall has been constructed to meet this condition.

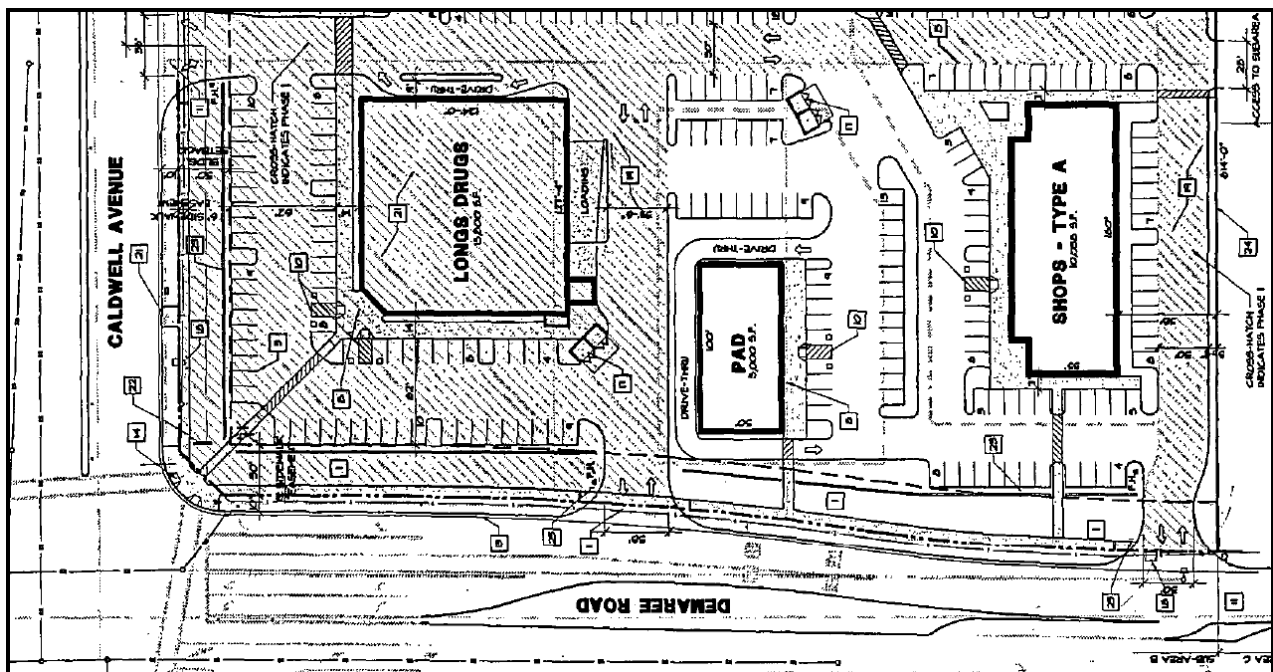
Compliance with Demaree/Caldwell Specific Plan

The Demaree/Caldwell Specific Plan was approved in 1999 and set forth the development plan for a 66-acre area within the south and southeast portions of the Caldwell / Demaree intersection. The project site is located within Subarea "B" of the specific plan, which was identified to provide community-level retail commercial uses. Furthermore, the specific plan identifies in the Subarea "B" policies, B-2 that proposed uses are to conform to the equivalent zone district (or equivalent, or comparable zone, should the City's Zoning Ordinance be modified. The Zoning Ordinance was updated in 2017 and the area zoning was changed to C-MU (Mixed-Use Commercial), which as stated above, conditionally permits drive-thru uses not meeting performance standards.

Link to Demaree/Caldwell Specific Plan:

<https://www.visalia.city/civicax/filebank/blobdload.aspx?BlobID=4522>

The 2006 amendments restructured the Subarea "B" layout, demonstrating a fast-food drive-thru use on the subject site (see Exhibit "H" – SP Amendment report and the image below). The applicant's request is consistent with the approved amendment in use, location, and layout. However, because the drive-thru use for this site was never fully analyzed in relation to the other drive-thru uses as part of the 2006 approval, and due to the lack of analysis and information provided, staff concluded a CUP amendment was warranted.



Acoustical Analysis

An Acoustical Analysis was prepared for the proposed project [ref.: Acoustical Analysis, Jack In The Box Restaurant S. Demaree Street & Caldwell Avenue. WJV Acoustics, March 31, 2025]. The purpose of the study is to determine if noise levels associated with the proposed Drive-thru will comply with the City's applicable noise level standards upon the existing single-family residential use to the south. The analysis concluded that noise levels associated with the proposed drive-thru operations would not be expected to exceed the City's exterior noise level standards.

Public Frontage Improvements

The site is located along South Demaree Street, a Minor Arterial roadway as defined in the City's Circulation Element. Currently, there are two existing vehicular drive approaches along the Demaree frontage to the site. To date, the only improvement along the project site frontage is curb and gutter. The proposed development will extend sidewalk and parkway landscaping across their project street frontage. Furthermore, the applicant has shown on their site plan (Exhibit "A") that the project will comply with the requirement to average 30-feet of landscaping setback along South Demaree Street.

Parking

Zoning Ordinance Section 17.34.020.F.10 prescribes parking as one space per 150 square feet of floor area for restaurant uses. Based on this parking requirement, the Jack in the Box restaurant exceeds the parking ratio required for this commercial pad, providing 31 total stalls when only 14 would be required.

Environmental Review

This project is considered Categorically Exempt under Section 15332 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA) for new in-fill (Categorical Exemption No. 2025-18).

RECOMMENDED FINDINGS

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The project site has adequate ingress and egress and parking for the proposed fast-food restaurant.
 - b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The project site has adequate ingress and egress and parking for the proposed fast-food restaurant.
3. That the project is considered Categorically Exempt under Section 15332 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2025-18).

RECOMMENDED CONDITIONS OF APPROVAL

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2025-002.
2. That the site be developed in substantial compliance with the Site Plan in Exhibit "A".
3. That the building elevations be developed in substantial compliance with the elevations shown in Exhibit "C".
4. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. In addition, landscape and irrigation plans shall comply with the State Model Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
5. That the noise emitted from the drive-thru teller speaker box shall meet all community noise standards as identified in the City's Noise Ordinance Chapter 8.36.
6. That all signs shall require a separate building permit.
7. That all other federal, state and city codes, ordinances and laws be met.

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal with applicable fees shall be in writing and shall be filed with the City Clerk at 220 North Santa Fe Street, Visalia California. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

Attachments:

- Related Plans and Policies
- Resolution
- Exhibit "A" – Site Plan
- Exhibit "B" – Floor Plan
- Exhibit "C" – Building Elevations
- Exhibit "D" – Acoustical Analysis
- Exhibit "E" – Traffic Memo - Queuing
- Exhibit "F" – Photometric Plan
- Exhibit "G" – Preliminary Landscape Plan
- Exhibit "H" – SP Amendment 2006 & CUP No. 2006-32
- Site Plan Review Comments – No. 2025-002
- General Plan Land Use Map
- Zoning Map
- Aerial Photo
- Location Map

Related Plans & Policies
Conditional Use Permits
(Chapter 17.38)

17.38.010 Purposes and powers

In certain zones conditional uses are permitted subject to the granting of a conditional use permit. Because of their unusual characteristics, conditional uses require special consideration so that they may be located properly with respect to the objectives of the zoning ordinance and with respect to their effects on surrounding properties. In order to achieve these purposes and thus give the zone use regulations the flexibility necessary to achieve the objectives of this title, the planning commission is empowered to grant or deny applications for conditional use permits and to impose reasonable conditions upon the granting of such permits. (Prior code § 7525)

17.38.020 Application procedures

- A. Application for a conditional use permit shall be made to the planning commission on a form prescribed by the commission which shall include the following data:
 - 1. Name and address of the applicant;
 - 2. Statement that the applicant is the owner of the property or is the authorized agent of the owner;
 - 3. Address and legal description of the property;
 - 4. The application shall be accompanied by such sketches or drawings as may be necessary by the planning division to clearly show the applicant's proposal;
 - 5. The purposes of the conditional use permit and the general description of the use proposed;
 - 6. Additional information as required by the historic preservation advisory committee.
- B. The application shall be accompanied by a fee set by resolution of the city council sufficient to cover the cost of handling the application. (Prior code § 7526)

17.38.030 Lapse of conditional use permit

A conditional use permit shall lapse and shall become void twenty-four (24) months after the date on which it became effective, unless the conditions of the permit allowed a shorter or greater time limit, or unless prior to the expiration of twenty-four (24) months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site which was the subject of the permit. A permit may be renewed for an additional period of one year; provided, that prior to the expiration of twenty-four (24) months from the date the permit originally became effective, an application for renewal is filed with the planning commission. The commission may grant or deny an application for renewal of a conditional use permit. In the case of a planned residential development, the recording of a final map and improvements thereto shall be deemed the same as a building permit in relation to this section. (Ord. 2001-13 § 4 (part), 2001: prior code § 7527)

17.38.040 Revocation

Upon violation of any applicable provision of this title, or, if granted subject to a condition or conditions, upon failure to comply with the condition or conditions, a conditional use permit shall be suspended automatically. The planning commission shall hold a public hearing within sixty (60) days, in accordance with the procedure prescribed in Section 17.38.080, and if not satisfied that the regulation, general provision or condition is being complied with, may revoke the permit or take such action as may be necessary to insure compliance with the regulation, general provision or condition. Appeals of the decision of the planning commission may be made to the city council as provided in Section 17.38.120. (Prior code § 7528)

17.38.050 New application

Following the denial of a conditional use permit application or the revocation of a conditional use permit, no application for a conditional use permit for the same or substantially the same conditional use on the same or substantially the same site shall be filed within one year from the date of denial or revocation of the permit unless such denial was a denial without prejudice by the planning commission or city council. (Prior code § 7530)

17.38.060 Conditional use permit to run with the land

A conditional use permit granted pursuant to the provisions of this chapter shall run with the land and shall continue to be valid upon a change of ownership of the site or structure which was the subject of the permit application subject to the provisions of Section 17.38.065. (Prior code § 7531)

17.38.065 Abandonment of conditional use permit

If the use for which a conditional use permit was approved is discontinued for a period of one hundred eighty (180) days, the use shall be considered abandoned and any future use of the site as a conditional use will require the approval of a new conditional use permit.

17.38.070 Temporary uses or structures

- A. Conditional use permits for temporary uses or structures may be processed as administrative matters by the city planner and/or planning division staff. However, the city planner may, at his/her discretion, refer such application to the planning commission for consideration.
- B. The city planner and/or planning division staff is authorized to review applications and to issue such temporary permits, subject to the following conditions:
 1. Conditional use permits granted pursuant to this section shall be for a fixed period not to exceed thirty (30) days for each temporary use not occupying a structure, including promotional enterprises, or six months for all other uses or structures.
 2. Ingress and egress shall be limited to that designated by the planning division. Appropriate directional signing, barricades, fences or landscaping shall be provided where required. A security officer may be required for promotional events.
 3. Off-street parking facilities shall be provided on the site of each temporary use as prescribed in Section 17.34.020.
 4. Upon termination of the temporary permit, or abandonment of the site, the applicant shall remove all materials and equipment and restore the premises to their original condition.
 5. Opening and closing times for promotional enterprises shall coincide with the hours of operation of the sponsoring commercial establishment. Reasonable time limits for other uses may be set by the city planner and planning division staff.
 6. Applicants for a temporary conditional use permit shall have all applicable licenses and permits prior to issuance of a conditional use permit.
 7. Signing for temporary uses shall be subject to the approval of the city planner.
 8. Notwithstanding underlying zoning, temporary conditional use permits may be granted for fruit and vegetable stands on properties primarily within undeveloped agricultural areas. In reviewing applications for such stands, issues of traffic safety and land use compatibility shall be evaluated and mitigation measures and conditions may be imposed to ensure that the stands are built and are operated consistent with appropriate construction standards, vehicular access and off-street parking. All fruits and vegetables sold at such stands shall be grown by the owner/operator or purchased by said party directly from a grower/farmer.
- C. The applicant may appeal an administrative decision to the planning commission. (Ord. 9605 § 30 (part), 1996: prior code § 7532)

17.38.080 Public hearing--Notice

- A. The planning commission shall hold at least one public hearing on each application for a conditional use permit.
- B. Notice of the public hearing shall be given not less than ten days nor more than thirty (30) days prior to the date of the hearing by mailing a notice of the time and place of the hearing to property owners within three hundred (300) feet of the boundaries of the area occupied or to be occupied by the use which is the subject of the hearing, and by publication in a newspaper of general circulation within the city. (Prior code § 7533)

17.38.090 Investigation and report

The planning staff shall make an investigation of the application and shall prepare a report thereon which shall be submitted to the planning commission. (Prior code § 7534)

17.38.100 Public hearing--Procedure

At the public hearing the planning commission shall review the application and the statement and drawing submitted therewith and shall receive pertinent evidence concerning the proposed use and the proposed conditions under which it would be operated or maintained, particularly with respect to the findings prescribed in Section 17.38.110. The planning commission may continue a public hearing from time to time as it deems necessary. (Prior code § 7535)

17.38.110 Action by planning commission

- A. The planning commission may grant an application for a conditional use permit as requested or in modified form, if, on the basis of the application and the evidence submitted, the commission makes the following findings:
 - 1. That the proposed location of the conditional use is in accordance with the objectives of the zoning ordinance and the purposes of the zone in which the site is located;
 - 2. That the proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity.
- B. A conditional use permit may be revocable, may be granted for a limited time period, or may be granted subject to such conditions as the commission may prescribe. The commission may grant conditional approval for a permit subject to the effective date of a change of zone or other ordinance amendment.
- C. The commission may deny an application for a conditional use permit. (Prior code § 7536)

17.38.120 Appeal to city council

The decision of the City planning commission on a conditional use permit shall be subject to the appeal provisions of Section 17.02.145. (Prior code § 7537) (Ord. 2006-18 § 6, 2007)

17.38.130 Effective date of conditional use permit

A conditional use permit shall become effective immediately when granted or affirmed by the council, or upon the sixth working day following the granting of the conditional use permit by the planning commission if no appeal has been filed. (Prior code § 7539).

RESOLUTION NO. 2025-26

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2025-11, A REQUEST TO CONSTRUCT A FAST-FOOD RESTAURANT MEASURING 2,003 SQUARE FEET WITH DRIVE-THRU AND INDOOR DINING, ON A PARCEL MEASURING 0.79-ACRES IN THE MIXED-USE COMMERCIAL (C-MU) ZONE. THE PROJECT SITE IS LOCATED ALONG SOUTH DEMAREE STREET, APPROXIMATELY 296-FEET SOUTH OF WEST CALDWELL AVENUE (ADDRESS: N/A) (APN:121-580-008)

WHEREAS, Conditional Use Permit No. 2025-11 is A request to construct a fast-food restaurant measuring 2,003 square feet with drive-thru and indoor dining, on a parcel measuring 0.79-acres in the Mixed-Use Commercial (C-MU) Zone. The project site is located along South Demaree Street, approximately 296-feet south of West Caldwell Avenue (Address: N/A) (APN:121-580-008); and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice, did hold a public hearing before said Commission on May 12, 2025; and

WHEREAS, the Planning Commission of the City of Visalia finds the Conditional Use Permit to be in accordance with Section 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, the Planning Commission finds the project to be Categorically Exempt consistent with the California Environmental Quality Act (CEQA) and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT RESOLVED that the project is exempt from further environmental review pursuant to CEQA Section 15332.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The project site has adequate ingress and egress and parking for the proposed fast-food restaurant.
 - b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The project site has adequate ingress and egress and parking for the proposed fast-food restaurant.

3. That the project is considered Categorically Exempt under Section 15332 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2025-18).

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2025-002.
2. That the site be developed in substantial compliance with the Site Plan in Exhibit "A".
3. That the building elevations be developed in substantial compliance with the elevations shown in Exhibit "C".
4. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. In addition, landscape and irrigation plans shall comply with the State Model Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
5. That the noise emitted from the drive-thru teller speaker box shall meet all community noise standards as identified in the City's Noise Ordinance Chapter 8.36.
6. That all signs shall require a separate building permit.
7. That all other federal, state and city codes, ordinances and laws be met.

SITE LEGEND

- PROPERTY BOUNDARY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- PROPOSED ASPHALT PAVING
- PROPOSED CONCRETE PAVING
- EXISTING SIDEWALK
- PROPOSED SIDEWALK
- EXISTING CURB AND GUTTER
- PROPOSED CURB AND GUTTER
- CURB RAMPS
- EXISTING STREET LIGHT
- PROPOSED SITE LIGHTING
- BUILDING ENTRANCE
- WHEEL STOP
- ACCESSIBLE PARKING SPACES
- BIKE RACK
- PROPOSED TRANSFORMER LOCATION
- PROPOSED WATER METER LOCATION
- PROPOSED GREASE INTERCEPTOR LOCATION
- NUMBER OF PARKING STALLS
- LIMITS OF WORK
- VEHICLE STACKING

GENERAL CONDITIONS

GENERAL CONTRACTOR SHALL NOTE THAT ALL WORK TO BE DONE SUCH AS EXCAVATIONS, TRENCHES, CAISSONS, WALLS, ETC., AS INDICATED ON DRAWINGS, IS SHOWN WITHOUT KNOWLEDGE OF UNDERGROUND UTILITIES ON THIS PARTICULAR SITE. THE ARCHITECT AND OWNER ASSUME NO RESPONSIBILITY FOR DETERMINING THEIR LOCATION, SIZE, DEPTH, OR HAZARD.

GENERAL NOTES

- A. REFER TO THE ACCESSIBILITY AND DIMENSIONING PLAN, SHEET SD1.1, FOR HORIZONTAL CONTROL INFORMATION AND JOINT DETAILING.
- B. GENERAL CONTRACTOR SHALL PROVIDE, AS REQUESTED BY CONSTRUCTION MANAGER, 'COMING SOONISH' SIGNAGE, SEE DETAIL 9/SD2.1.
- C. ALL SITE CONCRETE PAVING SHALL BE SPRAYED WITH A CURING COMPOUND AS SOON AS CONCRETE IS SET UP.
- D. GENERAL CONTRACTOR SHALL PROVIDE 12"x12" LANDSCAPE PAVERS AT WINDOWS WHEN LANDSCAPE IS PROVIDED AROUND WINDOW AREAS.

PARKING REQUIREMENTS

THE MUNICIPAL PARKING REQUIREMENTS FOR THIS PARCEL ARE:
BUILDING FOOTPRINT SQUARE FOOTAGE = 2,003 SQFT
MIN. 1 SPACE PER 100 SQFT 2,003/100 = 20 SPACES

DESIGNER NOTE: JIB PREFERENCE IS TO PROVIDE A MINIMUM 1 SPACE PER 100 SQFT BUILDING. COORDINATE WITH JIB CONSTRUCTION MANAGER FOR CONFIRMATION.

TOTAL PARKING REQUIRED

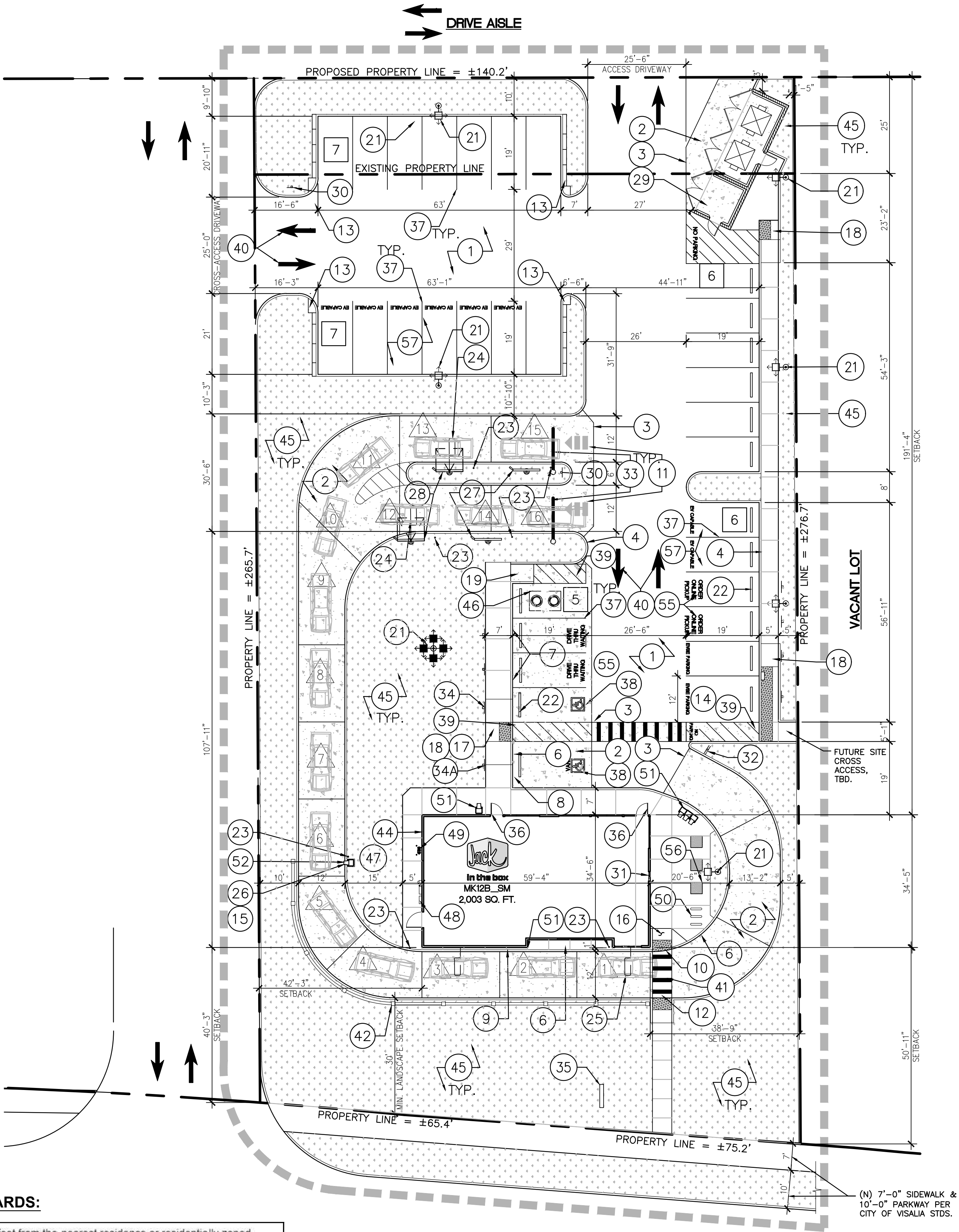
ACCESSIBLE PARKING PROVIDED: 2 SPACES MINIMUM
STANDARD PARKING PROVIDED: 29 SPACES PARKING
PROVIDED: 31 SPACES TOTAL

TOTAL 36.7% LANDSCAPE:

SITE AREA : 38,025 SQ.FT.
LANDSCAPE: 13,974 SQ.F.T

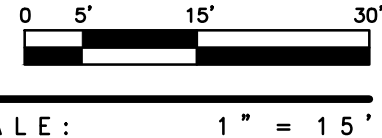
DRIVE THRU PERFORMANCE STANDARDS:

1. Separation from residences. The drive-thru lane shall be no less than two hundred fifty (250) feet from the nearest residence or residentially zoned property.
2. Stacking. The drive-thru lane shall contain no less than ten (10) vehicle stacking, measured from pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicle spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
3. Circulation. No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive-thru shall not take ingress or egress from a local residential road.
4. Noise. No component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60 dB between the hours of 7.00 p.m. and 6.00 a.m. daily.
5. Screening. The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.
6. Menu boards and signage. Shall be oriented or screened to avoid direct visibility from adjacent public streets. (Ord. 2017-01 (part), 2017: Ord. 2014-07 § 3, 2014)



S. DEMAREE STREET
90' PUBLIC RIGHT-OF-WAY

SITE PLAN



KEYNOTES

1. INSTALL ASPHALT PAVING; REFER TO THE 'PAVING SECTION THICKNESS' TABLE THIS SHEET AND GEOTECHNICAL RECOMMENDATIONS UNDER SEPARATE COVER.
2. INSTALL CONCRETE PAVING; REFER TO THE 'PAVING SECTION THICKNESS' TABLE THIS SHEET AND GEOTECHNICAL RECOMMENDATIONS UNDER SEPARATE COVER.
3. PROVIDE THICKENED CONCRETE EDGE AT PAVING INTERFACE; SEE DETAIL 8/SD2.0.
4. CONSTRUCT CONCRETE CURB AND GUTTER; SEE DETAIL 1/SD2.0.
5. CONSTRUCT CONCRETE CURB AT ASPHALT PAVING; SEE DETAIL 2/SD2.0.
6. CONSTRUCT CONCRETE CURB AT CONCRETE PAVING; SEE DETAIL 3/SD2.0.
7. CONSTRUCT MONOLITHIC CONCRETE CURB AND SIDEWALK; SEE DETAIL 4/SD2.0.
8. CONSTRUCT MONOLITHIC CONCRETE CURB AND PAVING; SEE DETAIL 5/SD2.0.
9. CONSTRUCT CONCRETE CURB AT BUILDING SIDE OF DRIVE-THRU LANE; SEE DETAIL 6/SD2.0.
10. CONSTRUCT CONCRETE CURB & GUTTER AT BOTTOM RAMP; SEE DETAIL 19/SD2.0.
11. DRIVE-THRU ARROW, PAINTED IMPULSIVE PURPLE, SEE DETAIL 20/SD2.2.
12. INSTALL CONCRETE CROSS PAN (DRIVE APPROACH) TO MUNICIPALITY STANDARDS.
13. INSTALL 4-INCH (MIN) THICK CONCRETE SIDEWALK WITH LIGHT BROOM FINISH. SEE DETAIL 2/SD2.1.
14. PROVIDE ACCESSIBLE EV PARKING SPACE. EV PARKING REQUIREMENTS PER CURRENT CITY CODE.
15. DOWEL TO EXISTING CONCRETE PAVING WITH 18-INCH NO. 4 BARS AT 18-INCHES O.C.; MINIMUM OF TWO BARS - REDUCE SPACING AS NECESSARY.
16. CONSTRUCT FLARE SIDED (WINGED) CURB RAMP; SEE DETAIL 13/SD2.0.
17. CONSTRUCT IN-LINE CURB RAMP; SEE DETAIL 15/SD2.0.
18. PROVIDE RAMP AND LANDINGS AT HANDICAP ACCESSIBLE PARKING SPACES; SEE DETAIL 15/SD2.0.
19. CONSTRUCT DELIVERY RAMP; SEE DETAIL 17/SD2.0.
20. PROVIDE PUBLIC RIGHT-OF-WAY ACCESS RAMPS PER THE MUNICIPALITY STANDARDS.
21. INSTALL LIGHT POLE BASE; SEE DETAIL 16/SD2.1. REFER TO SHEET ES1.1 FOR POLE AND FIXTURE REQUIREMENTS.
22. INSTALL CONCRETE WHEEL STOP TYPICAL AT HANDICAP SPACES, STANDARD SPACES FRONTING 6-FOOT OR LESSER WIDTH SIDEWALKS, AND IN FRONT OF LIGHT POLES; SEE DETAIL 9/SD2.0. SEE KEYNOTE 55.
23. INSTALL BARRIER POSTS AT PREVIEW BOARD, MENU BOARD, AND PERIPHERAL TO BUILDING CORNERS; SEE DETAIL 4/SD2.1. CONTRACTOR TO PROVIDE SLEEVES THROUGH CURB AND GUTTER WHERE APPROPRIATE. SEE A1.0 FOR DIMENSIONING CONTROL & BARRIERS ADJACENT TO BUILDING.
24. INSTALL DETECTOR LOOP FOR ORDER BOARD; SEE DETAIL 8/SD2.4.
25. INSTALL DETECTOR LOOP AT PICK UP WINDOW; SEE DETAIL 2/SD2.4.
26. PROVIDE TRASH RECEPTACLE ON SIDEWALK AT FRONT OF CAR 4. PREFERRED LOCATION IS AT CAR 4; CENTERED ON WINDOW. LOCATE AS REQUIRED FOR SITE CONDITIONS.
27. INSTALL PREVIEW BOARD; SEE DETAIL 10/SD2.4.
28. INSTALL ORDER/SPEAKER BOARD AND WEATHER PROTECTION CANOPY; SEE DETAIL 8/SD2.4.
29. CONSTRUCT MASONRY BLOCK TRASH ENCLOSURE; SEE DETAIL 16/SD2.2.
30. SITE 'ENTRY' SIGN UNDER SEPARATE COVER. GENERAL CONTRACTOR REFER TO SHEET ES1.0 FOR CONDUIT AND WIRING REQUIREMENTS.
31. FOOD PICK-UP WINDOW, SEE FLOOR PLAN.
32. SITE 'THANK YOU/DO NOT ENTER' SIGN UNDER SEPARATE COVER. GENERAL CONTRACTOR REFER TO SHEET ES1.0 FOR CONDUIT AND WIRING REQUIREMENTS.
33. INSTALL SINGLE BAR HEIGHT CLEARANCE (9'-0") AND WARNING POLE SIGN; SEE DETAIL 6 AND 9/SD2.4.
□ INSTALL DRIVE-THRU ENTRY PORTAL; SEE DETAIL 1/SD2.4.
34. INSTALL ACCESSIBLE PARKING SIGN. SEE DETAIL 8/SD2.1.
- 34A. INSTALL ACCESSIBLE PARKING SIGN WITH 'VAN' PLACARD. SEE DETAIL 8/SD2.1.
35. PYLON / MONUMENT SIGN; GENERAL CONTRACTOR TO PROVIDE CONDUIT AND WIRING PER SHEET ES1.0. INSTALLATION BY SIGN CONTRACTOR.
36. APPLY INTERNATIONAL SYMBOL OF ACCESSIBILITY DECAL ON DOOR.
37. PAINT 4-INCH SOLID WHITE PARKING STRIPES TO MUNICIPALITY STANDARDS. SEE KEYNOTE 55.
38. PAINT ACCESSIBLE PARKING SYMBOLS, TEXT, AND DIAGONALS; SEE DETAIL 6/SD2.4.
39. PAINT 4-INCH SOLID WHITE STRIPES AT 8-INCHES O.C. AND 45-DEGREES TO TRAFFIC DIRECTION, BORDER WITH 6-INCH SOLID WHITE STRIPE. TYPICAL LOADING SPACES AS SHOWN.
40. PAINT TRAFFIC DIRECTIONAL ARROWS; SOLID WHITE AND TYPICAL AS SHOWN.
41. PAINT 18-INCH WIDE SOLID WHITE CROSS WALK STRIPES AT 5-FEET (MAX) O.C.
42. 36" HT STUCCO CLAD CMU WALL W/ 42"HT. STUCCO CLAD COLUMNS, TYP. SEE DETAIL 17/SD2.1.
43. INSTALL PEDESTRIAN GUARD RAILING; SEE DETAIL 3/SD2.1.
44. PROPOSED WATER METER, CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER. REFER TO SHEET PS1.0.
45. PROVIDE SCHEDULE 40 PVC SLEEVES AT 2X DIAMETER OF PROPOSED IRRIGATION PIPING; TYPICAL OF ALL LANDSCAPE AREAS. REFER TO SHEET I1.1.
46. PROPOSED GREASE INTERCEPTOR, CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER. PREFERRED TO BE LOCATED IN PARKING SPOT. REFER TO SHEET SP1.0.
47. PROPOSED TRANSFORMER LOCATION, CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER. BOLLARDS INSTALLED PER PROVIDER STANDARDS. REFER TO SHEETS PS1.0 AND ES1.0.
48. PROPOSED ELECTRIC METER AND C/T CABINET MOUNTING, CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER. REFER TO SHEET SP1.0.
49. PROPOSED GAS METER LOCATION, CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER. BOLLARDS INSTALLED PER PROVIDER STANDARDS. REFER TO SHEETS PS1.0 AND P3.0.
50. INSTALL U-BAR BIKE RACK(S); SEE DETAIL 5/SD2.1.
51. OF-WR1 TRASH, ORGANIC WASTE & RECYCLING RECEPTACLES NOT SHOWN, SEE ENLARGED PLAN ON SD2.3 FOR LOCATION (IF APPLICABLE).
52. OF-WR2 TRASH RECEPTACLE ON SIDEWALK AT FRONT OF CAR 4.
53. POSSIBLE CROSS ACCESS DRIVE IF REQUIRED.
54. LOCATION OF EXTERIOR WATER METER AND BACKFLOW PREVENTER IF REQUIRED. SEE DETAIL 18/SD2.2.
55. DEDICATED WAITING STALL PARKING SIGN, SEE DETAIL 8/SD2.1. PAINT PARKING STALL STRIPING IMPULSIVE PURPLE FOR ONLINE ORDERING/DELIVERY PARKING SPOTS. PAINT WHEEL STOPS AT THE DEDICATED WAITING SPACES IMPULSIVE PURPLE.
56. OUTDOOR WAITING SEATING CONCRETE BOX.
57. TYPICAL EV PARKING SPACE. EV PARKING REQUIREMENTS PER CURRENT CITY CODE.

NOTE:

SITE PLAN COMPLIES WITH THE DEMAREE-CALDWELL SPECIFIC PLAN.

Jack
in the box®
9357 SPECTRUM CENTER BLVD.
SAN DIEGO, CA 92123
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DATES

RELEASE: JULY 07, 2022

P.M. UPDATES: _____

SUBMITTAL DATE: _____

1: _____

2: _____

3: _____

BID: _____

CONSTRUCTION: _____

REVISIONS

- △ _____
- △ _____
- △ _____
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- △ _____
- △ _____
- △ _____

PM
DESIGN
Architectural
Solutions Group

6930 DESTINY DRIVE
SUITE 100
ROCKLIN, CA 95677
CONTACT: GREG BORCHARDT
PHONE: 916.303.4512
EMAIL: gborchardt@pmdginc.com

KEN MCKRACKEN, ARCHITECT

SITE INFORMATION

MK TYPE: MK12B_SM

JIB #: 5221

ADDRESS:
S. DEMAREE ST.&
W. CALDWELL AVE.
VISALIA, CA 93277

DRAWN BY: JJP

PROJECT #: MFT24001.0

SCALE: AS NOTED

SITE PLAN

SD1.0

1. EXTERIOR DOWNSPOUTS PER X/A9.X, SEE SITE PLAN FOR CONTINUATION.
2. GAS METER, SEE PLUMBING DRAWINGS.
3. LINE OF FOOD SERVICE EQUIPMENT, SEE KITCHEN DRAWINGS.
4. WATER HEATER; SEE PLUMB FOR MORE INFORMATION.
5. BARRIER POST, TYP., SEE "SD" SHEETS & CIVIL DRAWINGS.
6. ALIGN BULKHEAD ABOVE ROOF ACCESS HATCH OPENING. FACE EXPOSED SIDES W/ F.R.P. OVER 5/8" GYPSUM BOARD. CENTER ROOF LADDER IN OPENING, SEE ROOF PLAN & DETAIL 6/A9.3.
7. LINE OF BULKHEAD/HEADER ABOVE, SEE 13/A9.5 (BULKHEAD ONLY).
8. ELECTRICAL METER, SEE EXTERIOR ELEVATIONS & ELECTRICAL DRAWINGS.
9. CO2 TANK FILL BOX, SEE DETAIL 5/A9.3.
10. ELECTRICAL PANELS, SEE ELECTRICAL DRAWINGS.
11. GREASE TANK/ BULK OIL FILL BOX, SEE DETAIL 4/A9.3, KITCHEN & PLUMBING DRAWINGS.
12. INTEGRAL COUNTER BY WINDOW MANUFACTURER, SEE A8.1.
13. LADDER TO ROOF, SEE DETAILS 1/A9.3, 2/A9.3, & 6/A9.3.






14. IRRIGATION CONTROLS IN TAMPERPROOF ENCLOSURE, SEE LANDSCAPE AND ELECTRICAL DRAWINGS.
15. WOP SINK, SEE DETAIL 16/A9.0, CONSTRUCT WALLS TO UNDERSIDE OF PORF DECK ABOVE.
16. SELF -SERVICE BEVERAGE COUNTER, SEE ID3.1 & KITCHEN SHEETS
17. LINE OF CANOPY. REFER TO A2.0, A5.0 & A6.0 SERIES SHEETS FOR EXTENTS.
18. LINE OF KITCHEN HOOD ABOVE, SEE RCP, KITCHEN, & HOOD SHEETS.
19. GC SUPPLIED AND INSTALLED CARPENTRY/MILLWORK IN BOH OFFICE AREA. REFER TO K2.2 FOR OFFICE ELEVATIONS.
20. EXTENTS OF STEEL STUD FRAMING (NON-COMBUSTIBLE CONSTRUCTION) DO NOT INSTALL R.F.P. AT THIS LOCATION. SEE GENERAL NOTES ON THIS SHEET & STRUCTURAL DRAWINGS.
21. LANDING TO SLOPE MAX. 2% AWAY FROM BUILDING. REFER TO DETAIL ON PLAN, CIVIL & SITE DESIGN DRAWINGS.
22. WALL-MOUNTED ANSUL PLUM LOCATION INSTALLED @ 48" MAX TO TOP OF CONTROLS, SEE K1.0 FOR DETAILS.
23. WALL-MOUNTED WINDOW BELL (GC-E015).

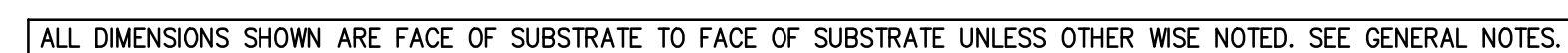
24. SERVICE COUNTER REFER TO ID3.0 FOR MORE INFORMATION.
25. LINE OF STAINLESS STEEL SHELF ABOVE; PROVIDE 2X FLAT BLOCKING FOR SECURE ATTACHMENT, SEE KITCHEN DRAWINGS FOR SIZE & MOUNTING HEIGHTS.
26. TELEPHONE CABINET & UPS, SEE INTERIOR ELEVATIONS & ELECTRICAL DWGS.
27. NOT USED.
28. 2X6 FLAT BLOCKING AT EMPLOYEE DESK, SEE K2 SERIES FOR MOUNTING HEIGHT.
29. NOT USED.
30. NOT USED.
31. EQUIPMENT SHELF BOX HEADER TO BE (2) 12" 18GA STUDS/ W/ 20GA 5-1/2" TRACT T&B. REFER TO K2.0 FOR MOUNTING HEIGHT

- A. ALL DIMENSIONS SHOWN ARE FACE OF SUBSTRATE TO FACE OF SUBSTRATE, UNLESS OTHERWISE NOTED. DRIVE-THRU WINDOWS AND FOOD PICK-UP LOCKER ARE DIMENSIONED FROM CENTERLINE TO FACE OF SUBSTRATE.
- B. PROVIDE 2x SOLID BLOCKING IN WOOD FRAMED WALLS AS REQUIRED FOR REINFORCEMENT OF ALL GRAB BARS, RESTROOM FIXTURES, PLUMBING LINES, WALL BUMPERS, KITCHEN SHELVING, AND MILLWORK ATTACHMENT, ETC. SEE SHEET A7 SERIES FOR RESTROOMS AND SHEETS K2.0, K2.1, AND K2.2 FOR KITCHEN, EMPLOYEE, AND WORKSTATION AREAS.
- C. NON-COMBUSTIBLE BLOCKING MUST BE USED BEHIND KITCHEN EQUIPMENT, I.E. HOODS, TACO STATION WHERE METAL STUD FRAMING IS DESIGNATED ON FLOOR PLAN.
- D. FOR EXTERIOR WALL TREATMENT, SEE EXTERIOR ELEVATIONS.
- E. GENERAL CONTRACTOR SHALL COORDINATE THEIR WORK WITH THE WORK OF THE FOOD SERVICE CONTRACTOR.
- F. FOR ITEMS FURNISHED BY OTHERS & INSTALLED BY GENERAL CONTRACTOR SEE KITCHEN AND INTERIOR DRAWINGS.
- G. CONSULT CONSTRUCTION MANAGER FOR REQUIREMENTS FOR AIR CURTAINS AT DRIVE-THRU AND CASHIER WINDOWS AND AT DINING ENTRANCES. PROVIDE DOOR SWITCHES AS NECESSARY. REFER TO KITCHEN, MECHANICAL, & ELECTRICAL DWGS.
- H. ALL DOORS TO BE 4" FROM ADJACENT WALL (2"+2" FRAME), U.O.N.

(NOT ALL WALL TYPES MAY BE USED.)

- ALL EXTERIOR WALLS SHALL BE 2x6 WOOD STUDS AT 16" O.C. WITH MIN. R-21 BATT INSULATION & 5/8" WATER RESISTANT GYPSUM BOARD ON THE INTERIOR FACE, U.O.N. BELOW. SEE STRUCTURAL DRAWINGS FOR EXTERIOR SHEATHING.
- ALL INTERIOR WALLS SHALL BE 2x4 WOOD STUDS AT 24" O.C. WITH 5/8" WATER RESISTANT GYPSUM BOARD ON BOTH SIDES, U.O.N. BELOW.
- FOR TYPICAL SUBSTRATE AT WALL BASE, SEE DETAILS 12, 14, 18, & 19/49.2.

-  WOOD STUD WALL CONSTRUCTION, SEE NOTES ABOVE.
-  PARTIAL WOOD STUD CONSTRUCTION, SEE INTERIOR ELEVATIONS FOR HEIGHT.
-  WOOD STUD WALL CONSTRUCTION WITH R-11 SOUND BATT INSULATION.
-  5/8" GYPSUM BOARD INTERIOR SIDE ONLY OVER 5-1/2" METAL STUD WALL CONSTRUCTION TO EXTEND 18" MIN. PAST EACH SIDE OF HOOD, SEE STRUCTURAL DWGS. DO NOT INSTALL F.R.P. BEHIND HOODS; STAINLESS STEEL PANELS TO BE USED, SEE KITCHEN DWGS FOR FINISH. PROVIDE NON-COMBUSTIBLE BLOCKING IN WALL. BATT INSULATION IN NON-COMBUSTIBLE WALL AREA SHALL BE UN-FACED NON-COMBUSTIBLE INSULATION.
-  FREEZER AND COOLER PANEL WALLS.
- 1 2x4 STUD WALL @ 16" O.C. FACED WITH 5/8" PLYWOOD EACH SIDE IN LIEU OF 5/8" WATER RESISTANT GYPSUM BOARD.
- 1A 2x4 STUD WALL @ 16" O.C. FACED WITH 5/8" PLYWOOD (TAG SIDE) IN LIEU OF 5/8" WATER RESISTANT GYPSUM BOARD. WATER RESISTANT PLYWOOD TO BE USED @ WET AREAS.
- 2 2x6 STUD WALL @ 16" O.C. FACED WITH 5/8" WATER RESISTANT GYPSUM BOARD EACH SIDE.
- 2A 2x6 STUD WALL @ 16" O.C. FACED WITH 5/8" PLYWOOD (TAG SIDE) IN LIEU OF 5/8" WATER RESISTANT GYPSUM BOARD.



SCALE: 1 / 4" = 1' - 0"



①

in the box[®]

357 SPECTRUM CENTER BLVD.
SAN DIEGO, CA 92123

These designs, drawings and specifications are the property of Jack in the Box Inc. and shall not be copied or reproduced without its previous written consent.

RELEASE: JULY 07, 2022

M. UPDATES:

SUBMITTAL DATE:

 \vdots

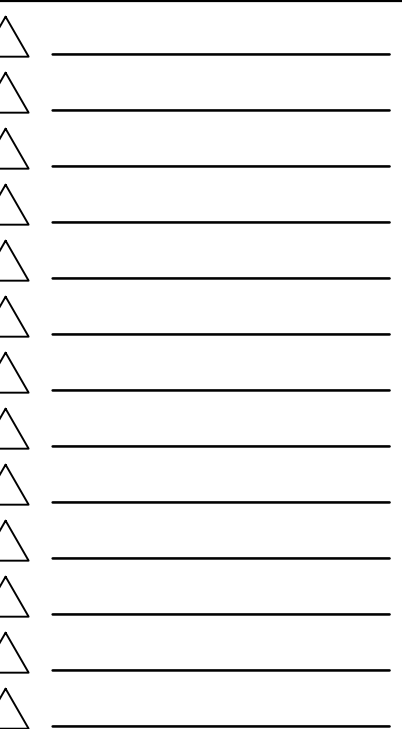
2:

3.

D:

CONSTRUCTION:

REVISIONS



6930 DESTINY DRIVE
SUITE 100
ROCKLIN, CA 95677
CONTACT: GREG BORCHARDT
PHONE: 916.303.4512
MAIL: gborchardt@pmdginc.com

N MCKRACKEN, ARCHITECT

SITE INFORMATION

K TYPE: MK12B SM

B #: 5221

ADDRESS:
1. DEMAREE ST.&
2. CALDWELL AVE.
3. SALIA, CA 93277

RAWN BY: JIP

PROJECT #: MET24001.0

CAIF: AS NOTED

FLOOR PLAN

A1.0

ACOUSTICAL ANALYSIS

**JACK IN THE BOX RESTAURANT
S. DEMAREE STREET & W. CALDWELL AVENUE
VISALIA, CALIFORNIA**

WJVA Project No. 25-08

PREPARED FOR

**VTP ENTERPRISES
1660 FARMERSVILLE BOULEVARD
FARMERSVILLE, CALIFORNIA 93223**

PREPARED BY

**WJV ACOUSTICS, INC.
VISALIA, CALIFORNIA**



MARCH 31, 2025

INTRODUCTION

The applicant proposes a 2,003 square foot building for a freestanding Jack in the Box restaurant with a small patio and drive-thru along Demaree Street, south of Caldwell Avenue. The desired operating hours for walk-in service will be 6:00 am to 10:00 pm daily and the drive-thru service will operate 24 hours per day. Primary vehicle access to the project site will be available from Demaree Street via a shared access drive with the CVS Pharmacy to the north.

This report is based upon the project site plan prepared by PM Design Architectural Solutions Group dated July 7, 2022, noise measurements obtained by WJV Acoustics, Inc. (WJVA) at the project site, reference noise measurements conducted by WJVA and project-related information provided to WJVA by the project applicant concerning the proposed project activities and hours of operation. Revisions to the site plan or other project-related information available to WJVA at the time the analysis was prepared may require a reevaluation of the findings and/or recommendations of the report. The Project Site Plan is provided as Figure 1.

Appendix A provides definitions of the acoustical terminology used in this report. Unless otherwise stated, all sound levels reported in this analysis are A-weighted sound pressure levels in decibels (dB). A-weighting de-emphasizes the very low and very high frequencies of sound in a manner similar to the human ear. Most community noise standards utilize A-weighted sound levels, as they correlate well with public reaction to noise. Appendix B provides typical A-weighted sound levels for common noise sources.

CRITERIA FOR ACCEPTABLE NOISE EXPOSURE

The City of Visalia Noise Element of the General Plan (noise element) establishes noise level criteria in terms of the Day-Night Average Level (L_{dn}) metric, for transportation noise sources. The L_{dn} is the time-weighted energy average noise level for a 24-hour day, with a 10 dB penalty added to noise levels occurring during the nighttime hours (10:00 p.m.-7:00 a.m.). The L_{dn} represents cumulative exposure to noise over an extended period of time and is therefore calculated based upon *annual average* conditions.

The exterior noise level standard of the noise element is 65 dB L_{dn} for outdoor activity areas of residential uses. Outdoor activity areas generally include backyards of single-family residences and individual patios or decks and common outdoor activity areas of multi-family developments. The intent of the exterior noise level requirement is to provide an acceptable noise environment for outdoor activities and recreation.

The noise element also requires that interior noise levels attributable to exterior noise sources not exceed 45 dB L_{dn} . The intent of the interior noise level standard is to provide an acceptable noise environment for indoor communication and sleep.

Additionally, the noise element establishes hourly acoustical performance standards for non-transportation (stationary) noise sources. The standards are set in terms of the L_{eq} (hourly equivalent) and L_{max} (maximum) noise levels. The standards, provided in Table I, are made more restrictive during the nighttime hours of 10:00 p.m. to 7:00 a.m.

<p style="text-align: center;">TABLE I</p> <p style="text-align: center;">NON-TRANSPORTATION NOISE LEVEL STANDARDS, dBA</p> <p style="text-align: center;">CITY OF VISALIA</p>			
Daytime (7 a.m.-10 p.m.)		Nighttime (10 p.m.-7 a.m.)	
L_{eq}	L_{max}	L_{eq}	L_{max}
50	70	45	65

Source: City of Visalia Noise Element of General Plan

Section 8.36 of the City's Municipal Code (noise ordinance) applies to noise sources that are not pre-empted from local control by existing state or federal regulations. Commercial activities are not pre-empted noise sources and are therefore subject to the provisions of the noise ordinance.

The noise ordinance addresses the statistical distribution of noise over time and allows for progressively shorter periods of exposure to levels of increasing loudness. Table II summarizes the exterior noise level standards of the ordinance. Note that the ordinance is to be applied during any one-hour time period of the day, and that the standards are 5 dB more restrictive between the hours of 7:00 p.m. and 6:00 a.m.

TABLE II
EXTERIOR NOISE LEVEL STANDARDS, dBA
CITY OF VISALIA NOISE ORDINANCE

Category	Cumulative # Min/Hr. (L _n)	Daytime (6am-7pm)	Nighttime (7pm-6am)
1	30 (L ₅₀)	50	45
2	15 (L ₂₅)	55	50
3	5 (L _{8.3})	60	55
4	1 (L _{1.7})	65	60
5	0 (L _{max})	70	65

Note: L_n is an abbreviation for the percentage of time that a certain noise level is exceeded during a specified time period (in this case, one hour). For example, an L₅₀ value of 50 dBA may not be exceeded during the hours of 6 am-7pm.

Source: City of Visalia Municipal Code

The City's noise ordinance also establishes interior residential noise level standards that would apply to the project. The interior noise level standards are established in allowable exceedance limits over differing amounts of time, within residential land uses. Similar to the applicable exterior standards, the interior standards become 5 dB more restrictive during nighttime hours. The applicable interior noise level standards are provided in Table III.

TABLE III
INTERIOR NOISE LEVEL STANDARDS, dBA
CITY OF VISALIA NOISE ORDINANCE

Category	Cumulative # Min/Hr.	Daytime (6am-7pm)	Nighttime (7pm-6am)
1	5	45	35
2	1	50	40
3	0	55	45

Source: City of Visalia Municipal Code

The City's noise ordinance also states *"In the event the measured ambient noise level without the alleged offensive source in operation exceeds an applicable noise level standard in any category above, the applicable standard or standards shall be adjusted so as to equal the ambient noise level"*.

PROJECT SITE NOISE EXPOSURE

The project is the proposed development of a Jack in the Box restaurant with a small patio and drive-thru. The project site is located along the east side of S. Demaree Street, approximately 400 feet south of W. Caldwell Avenue, in Visalia, California. The project site currently consists of undeveloped land. The project site is bordered to the north by retail land uses (CVS Pharmacy and store) to the east by undeveloped land, to the south by residential land uses, and to the west by commercial/retail land uses as well as a church.

The closest sensitive receptor (residential land use) to the project site is located approximately 250 feet to the south (3750 S. Demaree Street). Additionally, the church located at 3747 S. Demaree Street is located approximately 300 feet to the southwest of the project site.

A project site inspection and ambient noise monitoring were conducted on March 26 & 27, 2025, to evaluate the acoustical characteristics of the site and quantify existing ambient noise levels within the project area. The existing noise environment is dominated by traffic noise associated with vehicles traveling along S. Demaree Street and W. Caldwell Avenue. Additional sources of noise observed at the project site include noise associated with nearby commercial and residential land uses, as well as occasional aircraft overflights.

Ambient noise monitoring equipment consisted of a Larson-Davis Laboratories Model LDL 820 sound level analyzer equipped with a Bruel & Kjaer (B&K) Type 4176 ½" microphone. The monitor was calibrated with a B&K Type 4230 acoustical calibrator to ensure the accuracy of the measurements. The equipment complies with applicable specifications of the American National Standards Institute (ANSI) for Type 1 (precision) sound level meters.

WJVA collected 15-minute ambient noise level measurements near the existing residential land uses adjacent to the project site to the south. The location of the short-term ambient noise measurement site (ST-1) was selected as it represents existing ambient noise levels in the vicinity of the residentially zoned property line adjacent to the project site, the closest noise-sensitive land use to the project site.

Four (4) individual noise measurements were collected at the ambient noise measurement site to document existing (without project) ambient noise levels during early morning, midday, late evening, and late night/early morning conditions. The findings of the noise measurements are provided in Table IV. The project vicinity and ambient noise monitoring site location are provided as Figure 2. Figure 3 provides a photograph of the short-term ambient noise measurement site ST-1.

<p style="text-align: center;">TABLE IV</p> <p style="text-align: center;">SUMMARY OF SHORT-TERM AMBIENT NOISE MEASUREMENT DATA</p> <p style="text-align: center;">DEMAREE & CALDWELL JACK IN THE BOX</p> <p style="text-align: center;">MARCH 26 & 27, 2025</p>							
Site	Time	A-Weighted Decibels, dBA					
		L _{eq}	L ₅₀	L ₂₅	L ₈	L ₂	L _{max}
ST-1	3:20 a.m.	53.3	43.5	48.6	56.8	64.8	69.0
ST-1	7:45 a.m.	61.4	59.2	61.6	66.0	67.4	71.8
ST-1	12:45 p.m.	60.1	57.8	61.0	64.8	66.8	67.8
ST-1	7:30 p.m.	58.4	53.7	56.9	62.5	65.0	69.6

Source: WJV Acoustics, Inc.

From Table IV it can be determined that existing ambient (without project) noise levels at monitoring site ST-1 were in the range of approximately 53 to 61 dB L_{eq} with maximum noise levels in the range of 68 to 72 dB. The noise levels described in Table IV exceed the City's exterior noise level standards for stationary noise sources in most statistical categories during each of the noise measurement periods, with the exception of the 3:20 a.m. measurement period. Noise levels measured during the 3:20 a.m. measurement period exceeded applicable noise standards in four of the six statistical categories. Such existing ambient noise levels warrant an adjustment (increase) in the noise level standards described above (consistent with VMC Sec. 8.36.040.B.).

PROJECT RELATED NOISE LEVELS

The project is a proposed Jack In The Box Restaurant, including drive-thru operations. The proposed operating hours for walk-in service will be 6:00 am to 10:00 pm daily and the drive-thru service will operate 24 hours per day.

Noise levels associated with the restaurant would generally be limited to parking lot activities and the drive-thru food service operations. The drive-thru would incorporate an amplified order/speaker board for customers to place their orders.

Drive-Thru Operations:

The proposed project would include drive-thru operations, with proposed 24-hour operations. Drive-thru patrons would enter the project site via S. Demaree Street. Patrons would enter the drive-thru queue at the north side of the restaurant, where they would place orders via the amplified order/speaker board, before proceeding along the northern property line, turning westward, then southward, and collecting their orders at the western portion of the project site.

In order to assess potential project-related noise levels associated with drive-thru operations, WJVA conducted reference noise levels measured at an existing Jack In the Box drive-thru restaurant located on at 958 S. Mooney Boulevard in Tulare, California. The Tulare location is also owned by the project applicant, and utilizes the same order/speaker board equipment as that which is proposed at the project site location.

Measurements were conducted during the nighttime hours of March 27, 2025, between 10:00 p.m. and 10:45 p.m. using the previously-described noise monitoring equipment. The nighttime measurement time was selected in an effort to minimize extraneous traffic noise sources during the reference noise level measurements.

The microphone used by customers to order food and the loudspeaker used by employees to confirm orders are both integrated into a order/speaker board that is located a few feet from the drive-thru lane at the approximate height of a typical car window. Vehicles would enter the drive-thru lane from the east where the order is placed at the order/speaker board, and then turn to the south along the west side of the restaurant to pay and collect the order.

Reference noise measurements were obtained at a distance of approximately 25 feet from the order/speaker board containing the microphone/loudspeaker system at an angle of about 90° toward the rear of the vehicle being served (this represents the angle direction of the residential land use at the proposed Demaree and Caldwell location) as well as at a distance of approximately 25 feet directly facing the order/speaker board. Figure 4 provides the locations of the two reference noise measurement sites. Figure 5 provides photographs of the two reference noise measurement sites.

Each ordering cycle was observed to take between approximately 15-60 seconds including vehicle movements. A typical ordering cycle included 5-10 seconds of loudspeaker use with

typical maximum noise levels in the range of 45-50 dBA at the 25 foot-reference location east of the order/speaker board as well as at the measurement location facing directly toward the order/speaker board. Additionally, WJVA conducted noise level measurements at a setback distance of ten (10) feet directly in front of the order/speaker board while a store employee was communicating through the speaker system, while no vehicle was in the lane shielding the noise source. Unshielded maximum noise levels measured at the 10-foot setback distance were measured to be approximately 60-65 dB.

As described above, the proposed hours of operation for the drive-thru would generally be 24 hours per day. This means that the nighttime standards of the City's Noise Ordinance (Municipal Code) and the City's General Plan are applicable to the project.

During the nighttime reference noise measurement period, cars intermittently accessed the drive-thru, with extended periods without any customers. However, it can be reasonably assumed that during busier times that the drive-thru would likely have a consistent queue of customers accessing the drive-thru lane. As noted above, a typical drive-through ordering cycle was observed to last about 15-60 seconds during peak levels of activity. This translates into about 60+ cycles per hour. Assuming that loudspeaker use would average 5-10 seconds per cycle, also noted above, loudspeaker use would total 5-10 minutes per hour (or 8.3-16.7% of the time) during peak levels of activity. This corresponds to the L_{8.3} and L₂₅ statistical categories of the city's noise ordinance.

The closest sensitive receptor (residential land use) to the project site is located approximately 250 feet to the south (3750 S. Demaree Street). Additionally, the church located at 3747 S. Demaree Street is located approximately 300 feet to the southwest of the project site. Potential project-related noise exposure at these locations was calculated based upon the above-described reference noise measurement data and the normal rate of sound attenuation over distance for a "point" noise source (6 dB/doubling of distance). Based upon the above-described reference noise level measurements and the distance between the noise source (order/speaker board) and the sensitive receptor locations (single-family residence and church), project-related noise levels were calculated to be as follows:

- **Residence at 3750 S. Demaree Street: 25-30 dB**
- **Church at 3747 S. Demaree Street: 23-28 dB**

The noise levels described above are based upon the maximum measured reference noise levels at the Tulare Jack In the Box location. These noise levels do not exceed any City of Visalia statistical categories established in the noise ordinance or the General Plan. These calculated noise levels do not take into account any localized acoustic shielding that would occur from vehicles in the drive-thru queue or from the restaurant building itself, and therefore represent a worst-case assessment of project-related noise levels. Additionally, these noise levels do not exceed existing ambient noise levels measured at ambient noise measurement site ST-1. While an adjustment to the City's noise standards was determined warranted based upon existing elevated ambient noise levels in the project vicinity, project-related noise levels would not

exceed the unadjusted noise level standards provided above in Table I and Table II.

Parking Lot Activities:

Noise due to traffic in parking lots is typically limited by low speeds and is not usually considered to be significant. Human activity in parking lots that can produce noise includes voices, stereo systems and the opening and closing of car doors and trunk lids. Such activities can occur at any time. The noise levels associated with these activities cannot be precisely defined due to variables such as the number of parking movements, time of day and other factors. It is typical for a passing car in a parking lot to produce a maximum noise level of 60-65 dBA at a distance of 50 feet, which is comparable to the level of a raised voice.

For this project, parking would be generally be located along the southern and eastern portions of the project site. The closest proposed parking areas would be located approximately 200 feet from the closest sensitive receptor location. At this distance, maximum (L_{max}) parking lot vehicle movements would be expected to be approximately 48 to 53 dB L_{max} . Such levels do not exceed applicable noise level standards or existing ambient noise levels.

CONCLUSIONS AND RECOMMENDATIONS

Noise levels associated with the proposed Jack In The Box drive-thru restaurant would not be expected to exceed any City of Visalia noise level standards. Existing (without project) ambient noise levels in the vicinity of the project site are considered to be relatively high, due to the project site's proximity to two major arterial roadways (Demaree Street and Caldwell Avenue). As such, project-related noise levels would not be expected to exceed existing ambient noise levels at any sensitive receptor location near the project site.

The conclusions and recommendations of this acoustical analysis are based upon the best information known to WJV Acoustics Inc. (WJVA) at the time the analysis was prepared concerning the proposed site plan, project equipment and proposed hours of operation. Any significant changes in these factors will require a reevaluation of the findings of this report. Additionally, any significant future changes in noise regulations or other factors beyond WJVA's control may result in long-term noise results different from those described by this analysis.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read 'Walter J. Van Groningen', with a stylized flourish at the end.

Walter J. Van Groningen
President

WJV:wjv

FIGURE 1: PROJECT SITE PLAN

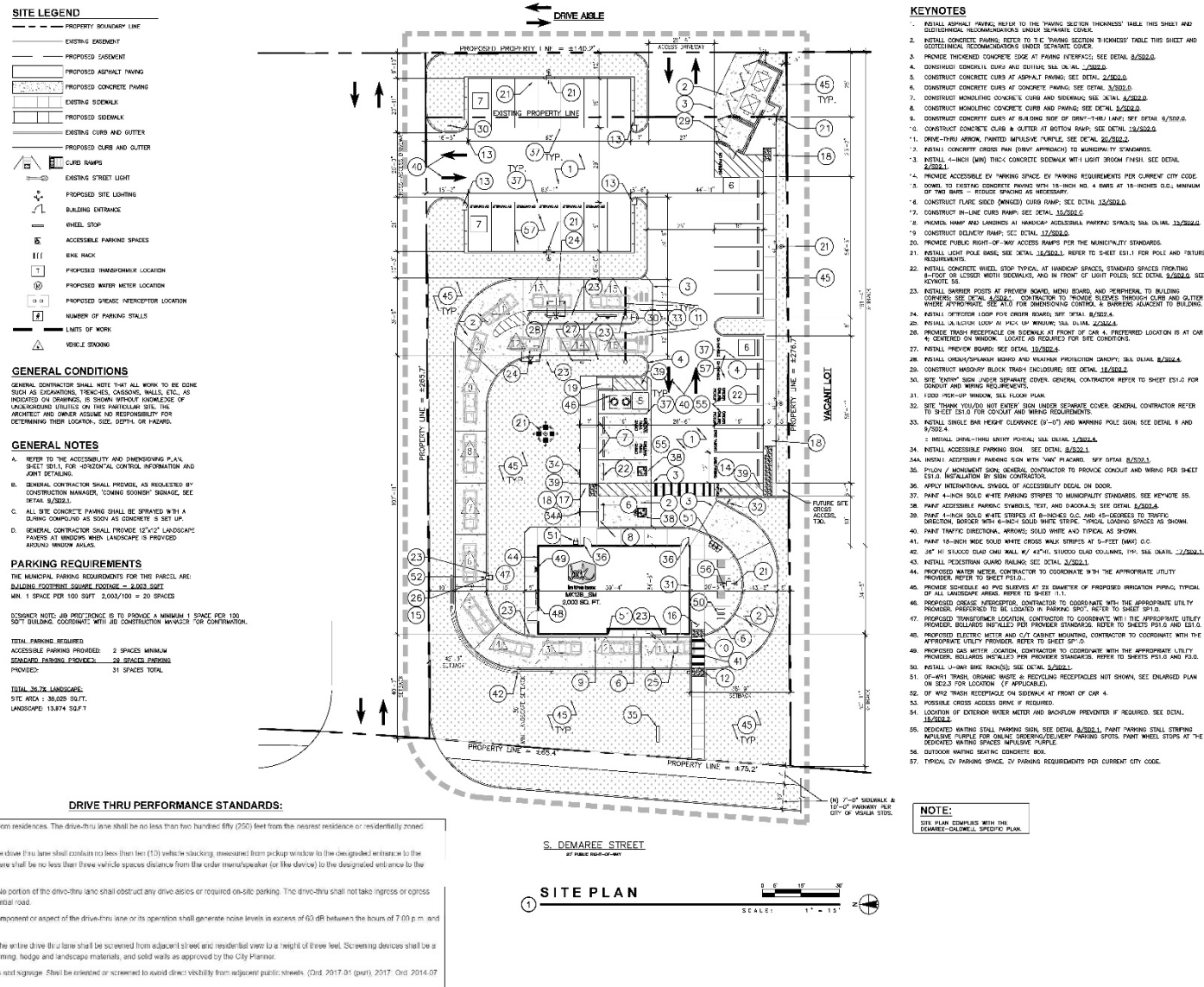


FIGURE 2: PROJECT SITE VICINITY AND NOISE MONITORING SITE LOCATION



FIGURE 3: AMBIENT NOISE MONITORING SITE ST-1



FIGURE 4: REFERENCE NOISE MONITORING SITE LOCATIONS



FIGURE 5: REFERENCE NOISE MONITORING SITES



APPENDIX A

ACOUSTICAL TERMINOLOGY

AMBIENT NOISE LEVEL:	The composite of noise from all sources near and far. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location.
CNEL:	Community Noise Equivalent Level. The average equivalent sound level during a 24-hour day, obtained after addition of approximately five decibels to sound levels in the evening from 7:00 p.m. to 10:00 p.m. and ten decibels to sound levels in the night before 7:00 a.m. and after 10:00 p.m.
DECIBEL, dB:	A unit for describing the amplitude of sound, equal to 20 times the logarithm to the base 10 of the ratio of the pressure of the sound measured to the reference pressure, which is 20 micropascals (20 micronewtons per square meter).
DNL/L_{dn}:	Day/Night Average Sound Level. The average equivalent sound level during a 24-hour day, obtained after addition of ten decibels to sound levels in the night after 10:00 p.m. and before 7:00 a.m.
L_{eq}:	Equivalent Sound Level. The sound level containing the same total energy as a time varying signal over a given sample period. L _{eq} is typically computed over 1, 8 and 24-hour sample periods.
NOTE:	The CNEL and DNL represent daily levels of noise exposure averaged on an annual basis, while L _{eq} represents the average noise exposure for a shorter time period, typically one hour.
L_{max}:	The maximum noise level recorded during a noise event.
L_n:	The sound level exceeded "n" percent of the time during a sample interval (L ₉₀ , L ₅₀ , L ₁₀ , etc.). For example, L ₁₀ equals the level exceeded 10 percent of the time.

A-2

ACOUSTICAL TERMINOLOGY

NOISE EXPOSURE

CONTOURS:

Lines drawn about a noise source indicating constant levels of noise exposure. CNEL and DNL contours are frequently utilized to describe community exposure to noise.

NOISE LEVEL

REDUCTION (NLR):

The noise reduction between indoor and outdoor environments or between two rooms that is the numerical difference, in decibels, of the average sound pressure levels in those areas or rooms. A measurement of Noise level reduction@ combines the effect of the transmission loss performance of the structure plus the effect of acoustic absorption present in the receiving room.

SEL or SENEL:

Sound Exposure Level or Single Event Noise Exposure Level. The level of noise accumulated during a single noise event, such as an aircraft overflight, with reference to a duration of one second. More specifically, it is the time-integrated A-weighted squared sound pressure for a stated time interval or event, based on a reference pressure of 20 micropascals and a reference duration of one second.

SOUND LEVEL:

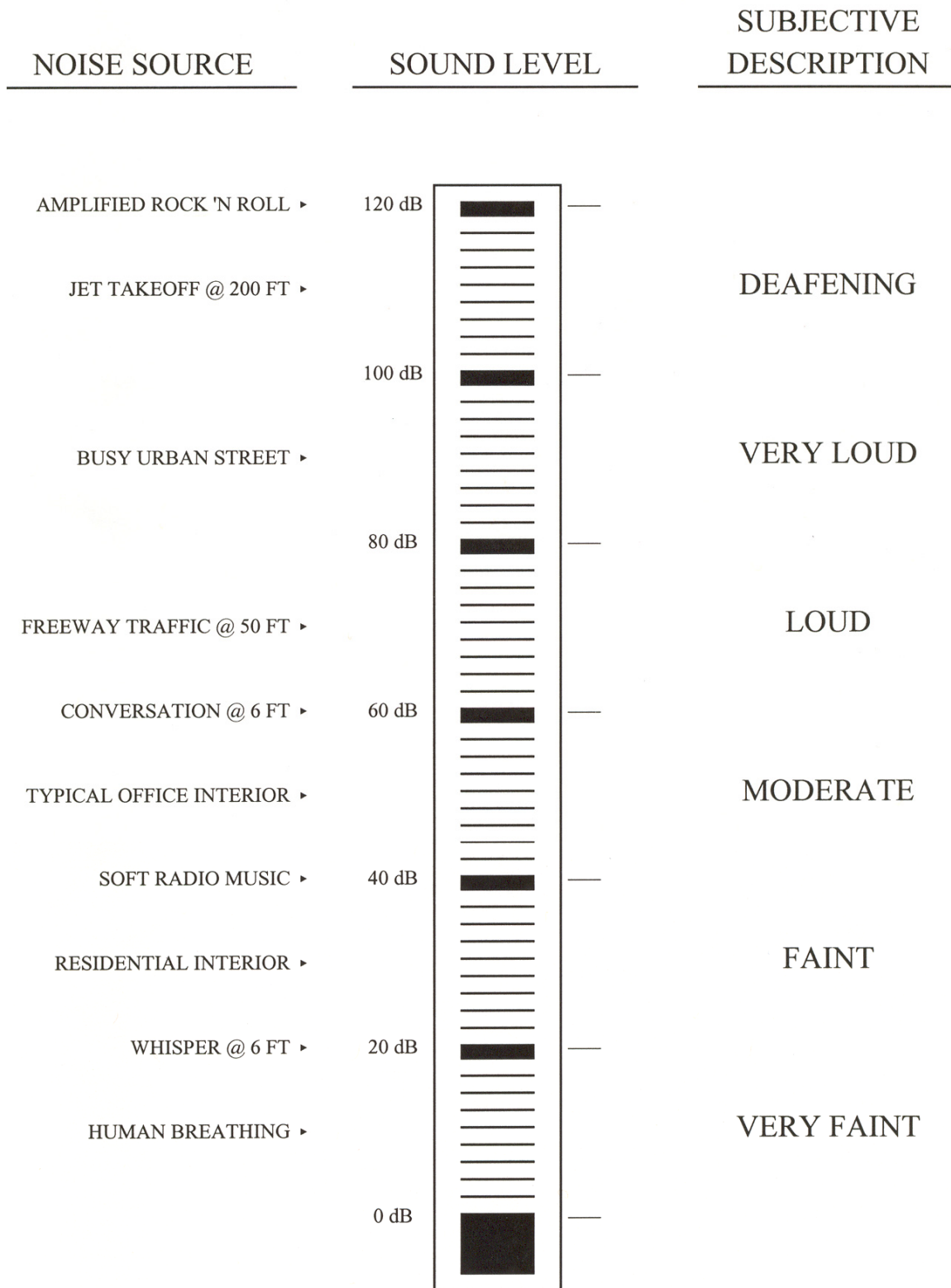
The sound pressure level in decibels as measured on a sound level meter using the A-weighting filter network. The A-weighting filter de-emphasizes the very low and very high frequency components of the sound in a manner similar to the response of the human ear and gives good correlation with subjective reactions to noise.

SOUND TRANSMISSION

CLASS (STC):

The single-number rating of sound transmission loss for a construction element (window, door, etc.) over a frequency range where speech intelligibility largely occurs.

APPENDIX B EXAMPLES OF SOUND LEVELS



TRAFFIC MEMORANDUM

DATE: April 2, 2025

TO: Paul Bernal, City Planner
City of Visalia

FROM: Lisa Wallis-Dutra, PE, TE, PTOE, RSP₁
4Creeks, Inc.

PROJECT: Proposed Jack in the Box located at the southeast corner of Caldwell Avenue and Demaree Street in Visalia, California

SUBJECT: Traffic Memorandum for the Subject Project

This traffic memorandum documents the evaluation of the project trip generation, drive-through queuing capacity, and vehicle miles traveled (VMT) for the proposed Jack in the Box restaurant located at the southeast corner of W. Caldwell Avenue and S. Demaree Street in Visalia, California.

PROJECT DESCRIPTION

The project site is generally located on the southeast corner of W. Caldwell Avenue and S. Demaree Street in the City of Visalia. A location map is provided as Figure 1. The applicant is proposing a 2,003 square-foot building for a freestanding Jack in the Box restaurant with a small patio and drive-through. The projected operating hours for the walk-in service will be 6 AM to 10 PM daily and the drive-through service will operate 24 hours a day. Vehicle access to the project site will be available from S. Demaree Street. The project site plan is shown in Figure 2.

EXISTING CONDITIONS

The City of Visalia developed a Specific Plan for this area of the city. Outlined in the Demaree/Caldwell Specific Plan, Appendix A, the project will be included in what is delineated as Subarea B. The specific plan's analysis indicated that 93,800 SF of gross leasable commercial buildings can be developed within Subarea B based on twenty-five percent building coverage. Using this square footage, it was estimated that 41.8 trips per 1000 SF of building area would be anticipated for Subarea B resulting in an Average Daily Traffic (ADT) volume of 3,920 vehicles per day.

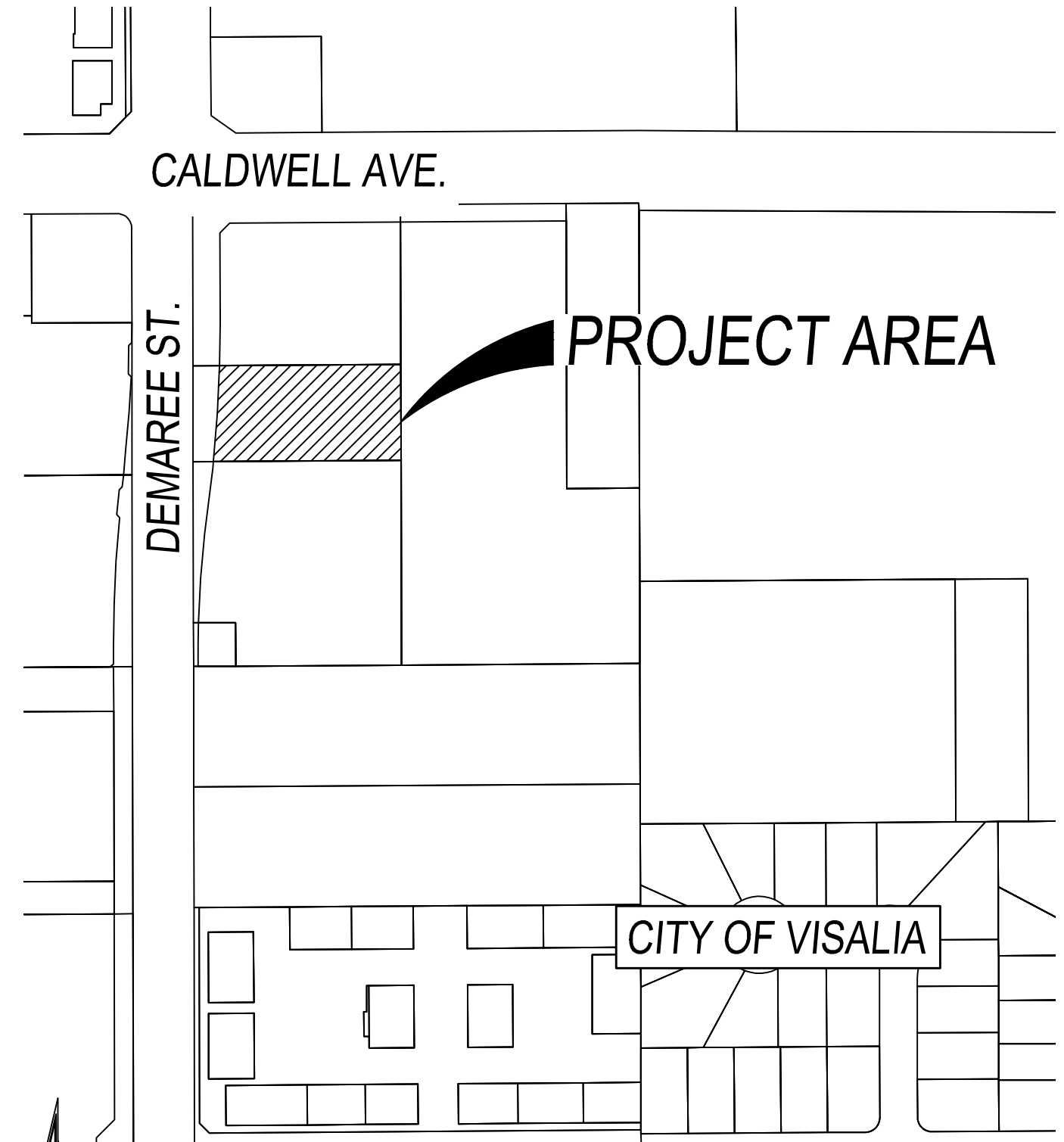


FIGURE 1 - LOCATION MAP

PREPARED BY:

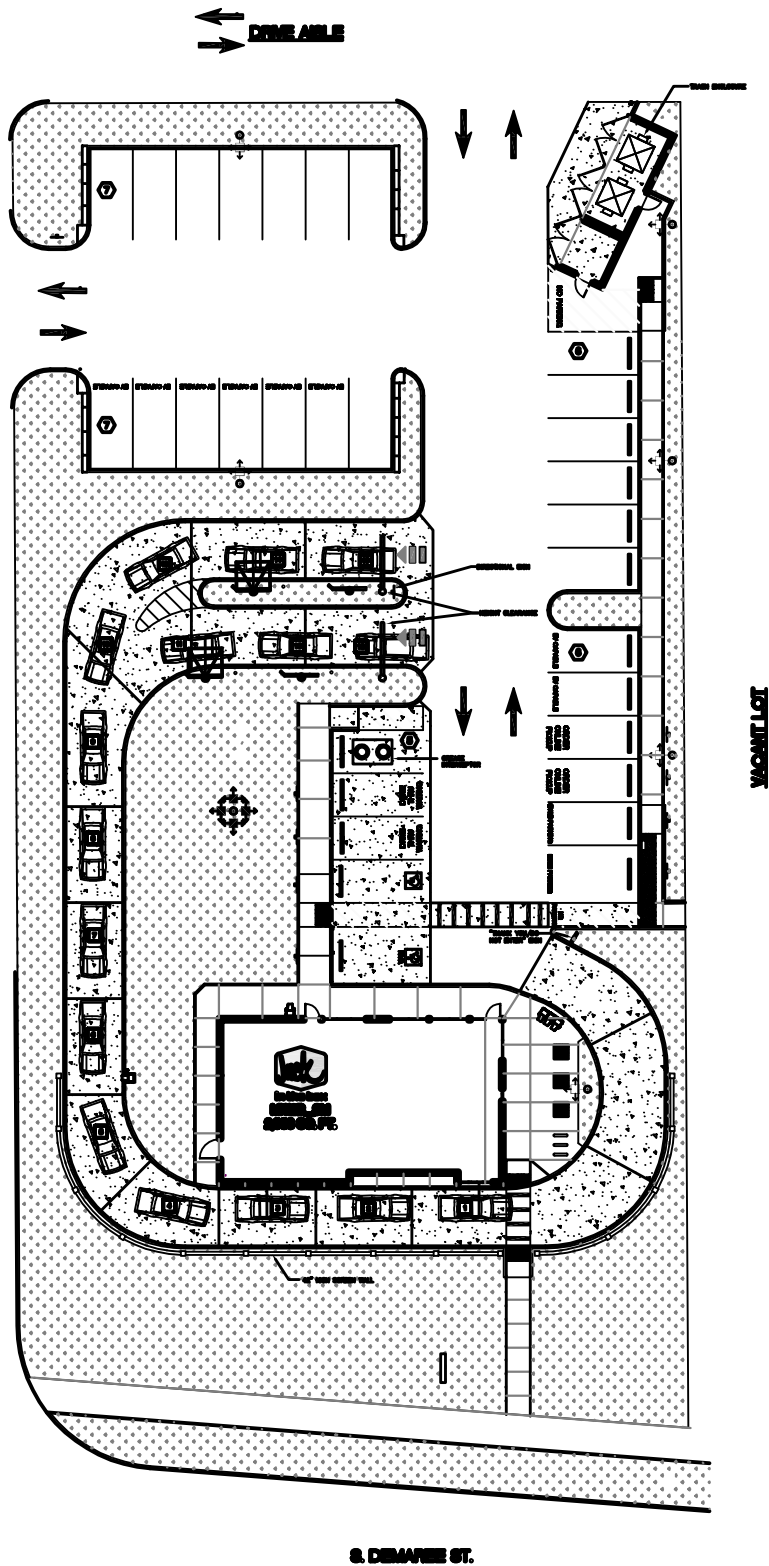


4CREEKS

324 S. SANTA FE, STE. A
VISALIA, CA 93292
TEL: 559.802.3052
FAX: 559.802.3215
www.4-creeks.com



FIGURE 2 - SITE PLAN



PREPARED BY:



4CREEKS

324 S. SANTA FE, STE. A
VISALIA, CA 93292
TEL: 559.802.3052
FAX: 559.802.3215
www.4-creeks.com

Currently, the only parcel that has been developed within Subarea B is in the northwest corner. It was originally a Longs Drug Store and has since been made a CVS Pharmacy. As part of the Conditional Use Permit (CUP) for the Longs Drug Store, Appendix B, an access analysis for a second driveway along Caldwell Avenue was performed (see Exhibit "G" included in the CUP, Appendix B. The trip generation for Subarea B was performed based on the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 7th Edition* for Land Use Code 280 (Shopping Center). For Subarea B, this evaluation used a total building square footage of 77,400 SF and a trip rate of 74.28 trips per 1,000 SF of building area, resulting in an ADT of 5,749 vehicles per day.

TRIP GENERATION

Data provided in the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition*, was used to estimate the number of trips anticipated for the proposed Jack in the Box restaurant. Pass-by reduction factors were assumed for the proposed use and are based on the *ITE Trip Generation Handbook, 3rd Edition*. Trip generation estimates for the project are shown in Table 1. After pass-by reductions are applied, the Jack in the Box project is estimated to generate 858 new daily trips, with 45 new AM peak hour trips (23 inbound, 22 outbound), and 33 new PM peak hour trips (17 inbound, 16 outbound).

TABLE 1 - SUMMARY OF PROJECT TRIP GENERATION

Land Use	ITE Code	Unit	Trip Generation Rates ¹						
			Daily	AM Peak Hours			PM Peak Hours		
				IN	OUT	Total	IN	OUT	Total
Fast Food Restaurant w/Drive-Thru	934	KSF	467.48	22.75	21.86	44.61	17.18	15.85	33.03
Land Use	Quantity	Unit	Trip Generation Estimates						
			Daily	AM Peak Hours			PM Peak Hours		
				IN	OUT	Total	IN	OUT	Total
Fast Food Restaurant w/ Drive-Thru	2.00	KSF	935	46	43	89	34	32	66
Pass-by Trips (49% AM, 50% PM) ^{2,3}			-77	-23	-21	-44	-17	-16	-33
Total Proposed Project Trips			858	23	22	45	17	16	33
¹ Source: <i>Trip Generation Manual, 11th Edition</i> , Institute of Transportation Engineers. ² Source: <i>Trip Generation Handbook, 3rd Edition</i> , Institute of Transportation Engineers. ³ Note: Daily pass-by trips shown is equal to AM and PM pass-by trips.									

DRIVE-THROUGH QUEUING ANALYSIS

A drive-through queuing analysis was completed for the project to evaluate the adequacy of the capacity of the drive-through lanes. As shown on the project site plan in Figure 2, the opening to the drive-through lanes is located at the center of the project site and follows along the north side of the site and wraps along the west side of the building. This location will operate a double drive-through that converges into one lane as it approaches the building. There will also be two parking spaces used for drive-through waiting areas.

Assuming 20 feet per vehicle, lane one provides queuing for approximately 12 vehicles and lane two provides queuing for an additional four vehicles. In total, the site provides a total queuing capacity for 16 vehicles.

Drive-through Queuing Calculations

From the previously shown data, this site should anticipate an average of 45 trips during its AM peak hour. The following percentages have been used to determine the percentage of customers that would be dining in or using the drive-through. Approximately 24% of the building area is the dining area while 76% is non-dining area. Of the 45 total trips, 34 or 76% will most likely utilize the drive-through.

For the queuing calculations, an arrival rate in vehicles per minute is needed. Converting the 34 peak hour vehicles utilizing the drive-through to vehicles per minute, a value of 0.5667 vehicles per minute is obtained. The service rate or the number of vehicles the service window can serve per minute is also needed. A service rate of 90 seconds has been utilized, which has been converted to 0.667 vehicles per minute. The queuing capacity of the drive-through was analyzed using formulas published in the *ITE Transportation Planning Handbook, 4th Edition* and can be found in Table 2.

TABLE 2 – DRIVE-THROUGH QUEUING ANALYSIS

Input Values		
Variable	Description	Value
A	average number of vehicle arrivals per Minute	0.5667
S	service rate, number of vehicles per minute	0.667
I	traffic intensity, utilization factor = A/S	0.85
Q	queue capacity (vehicles)	16
Formulas		
Average Length of Queue (Vehicles)	$Avg\ Q = I^2 / 1-I$	4.82
Probability of Q Number of Vehicles in Queue	$P(Q) = (I)^Q(1-I)$	1.11%
Probability of Queue Exceeding Q Vehicles	$\sum_{Q=0}^{Q=a} P(Q) \geq 0.95$	6.31%
Source: <i>Transportation Planning Handbook, 4thth Edition, Institute of Transportation Engineers</i>		

VMT

The California legislature approved Senate Bill (SB) 743 in September 2013 requiring changes to the California Environmental Quality Act (CEQA). SB 743 required the Governor's Office of Planning and Research (OPR) to develop alternative metrics to the use of vehicular "level of service" (LOS) for analyzing transportation projects. In December 2018, OPR published the Technical Advisory on Evaluating Transportation Impacts in CEQA (Technical Advisory). The Technical Advisory recommended Vehicle Miles Traveled (VMT) replace LOS as the primary measure of transportation impacts. Per the Technical Advisory, local agencies were required to update their transportation procedures by July 1, 2020, or the state guidelines would go into effect for their agency.

The Technical Advisory indicates that adding retail opportunities into the urban fabric and thereby improving retail destination proximity, local-serving retail development tends to shorten trips and reduce VMT. Per the City of Visalia's Procedures for Traffic Impact Analysis (TIA) updated March 2021, retail developments including stores less than 50,000 square feet are considered local serving. The proposed 2,003 square-foot fast-foot restaurant with drive-through is less than 50,000 square feet and is therefore considered local serving and is not anticipated to lead to longer trips and would reduce or maintain area VMT. As such, the project may be presumed to create a less than significant transportation impact.

CONCLUSION

With pass-by reductions applied, the Jack in the Box restaurant is estimated to generate 858 new daily trips, with 45 new AM peak hour trips (23 inbound, 22 outbound), and 33 new PM peak hour trips (17 inbound, 16 outbound). This location will operate a double drive-through that converges into one lane as it approaches the building. There will also be two parking spaces near the drive-through exit designated as waiting spaces, for customers whose orders are not ready when they arrive at the pick-up window. The total drive-through capacity is 16 vehicles. The queueing analysis indicates that during a peak event, the average length of queue will be approximately 4.82 vehicles and the probability the queue will exceed the 16 vehicle capacity is 6.31%.

Jack in the Box is not considered a statistical outlier or a “high demand generating” brand (i.e. In-N-Out), so there is no indication that it will expect drive-through queues more than the findings of this queueing analysis. Additionally, the layout of the drive-through lanes and drive approach entrance locations can sufficiently accommodate vehicle queues on-site. Queue lengths are not anticipated to overflow onto any adjacent major street.

Lastly, the project is considered local serving, not expected to lead to longer trips and would reduce or maintain area VMT. The project may be presumed to create a less than significant transportation impact.

Appendix A
Demaree/Caldwell Specific Plan

DEMAREE/CALDWELL SPECIFIC PLAN

PREPARED FOR AND ADOPTED BY:

CITY OF VISALIA

PLAN SPONSORS

DR. JOHN WONG
DONALD W. BUHL
ROBERT C. BUHL
RONALD WACHEL
BRANTON T. AND HELEN T. SWIFT
WINIFRED H. VAN GINKEL
MICHAEL KREPS AND SUSAN ZACHARY
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RAY E. AND JUANITA M. FOX
MERL AND MAXINE BROCK
MICHAEL AND JAN BROCK
MARVIN BROCK
HAROLD DEAN CLEMENTS
FRANK E. AND C. SUE KLEINHANS
JAMES L. AND CHRIS D. PETTY

BY
QUAD KNOPF, INC.

FEBRUARY, 1999

95020

RESOLUTION NO. 99-44

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF VISALIA, APPROVING THE DEMAREE/CALDWELL SPECIFIC PLAN (SPA NO. 99-05), FOR PROPERTY LOCATED ON THE SOUTH SIDE OF CALDWELL AVENUE, ON BOTH SIDES OF DEMAREE STREET; QUAD/KNOPF ENGINEERING, AGENT

WHEREAS, an application was filed by various property owners represented by Quad/Knopf Engineering for approval of a Specific Plan on property located on the south side of Caldwell Avenue, on both sides of Demaree Street, APN: 119-070-43; 119-340-01, 02, 03, 04, 05, 06, 07, 10, 11 and 12; 126-030-14, 15; 126-030-33, 34, 35 and 36; 126-480-31, 32; 126-490-01, 02, and 03, City of Visalia, and

WHEREAS, the Planning Commission of the City of Visalia, after Twenty-one (21) days published notice held a public hearing before said Commission on April 26, 1999; and recommended approval; and

WHEREAS, the City Council of the City of Visalia, after ten (10) days published notice held a public hearing before said Council on May 24, 1999; and

WHEREAS, the City Council of the City of Visalia finds the Specific Plan Amendment to be in accordance with Section 12.04.010 of the Ordinance Code of the City of Visalia based on evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, an Initial Study was prepared which disclosed that no significant environmental impacts would result from this project, and no mitigation measures would be required.

NOW, THEREFORE, BE IT RESOLVED that a Negative Declaration was prepared consistent with the California Environmental Quality Act and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the City Council of the City of Visalia approve the proposed Specific Plan based on the following specific findings and based on the evidence presented:

1. That the proposed Specific Plan is consistent with the adopted City of Visalia General Plan.
2. That the proposed Specific Plan has addressed the major infrastructure needs to position the property for development.
3. That the proposed Negative Declaration has addressed the environmental concerns within the project and fulfills the requirements of the California Environmental Quality Act.
4. That the proposed Specific Plan and the conditions under which it will be implemented will not be detrimental to the public's health, safety or welfare.

BE IT FURTHER RESOLVED that the City Council of the City of Visalia approves the Demaree/Caldwell Specific Plan described herein, in accordance with the terms of this resolution under the provisions of Section 12.04.010 of the Ordinance Code of the City of Visalia.

PASSED AND ADOPTED: May 24, 1999

STEVEN M. SALOMON, CITY CLERK

STATE OF CALIFORNIA)
COUNTY OF TULARE) ss.
CITY OF VISALIA)

I, Steven M. Salomon, City Clerk of the City of Visalia, certify the foregoing is the full and true Resolution No. 99-44 passed and adopted by the Council of the City of Visalia at a regular meeting held on May 24, 1999.

DATED: May 28, 1999

STEVEN M. SALOMON, CITY CLERK

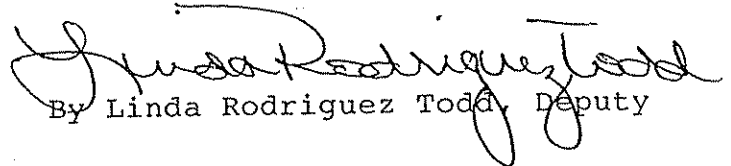

By Linda Rodriguez Todd, Deputy

Table of Contents

PREFACE

CHAPTER ONE – INTRODUCTION

Purpose and Scope of Plan	1
Legal Basis for Plan	3
Historical Background	4
Plan Formulation Process	5
Content and Format of Plan Document.....	6

CHAPTER TWO - EXISTING CONDITIONS IN PLAN AREA

Definition of Plan Subareas.....	7
Current Plan Area Land Uses	10
Existing Traffic Circulation System - Plan Area and Vicinity	10
Existing Plan Area Infrastructure.....	13
Identified Issues of Significance in Determining Plan Approaches	13
Land Use	14
Circulation	14
Urban Design and Aesthetics	14
Administration and Implementation	15

CHAPTER THREE - DEVELOPMENT PLAN FOR THE DEMAREE / CALDWELL AREA

Plan Goals.....	16
Land Use Element Objectives	17
Land Use Element Implementation Policies.....	18
Land Use and Circulation Plan	19
Plan Policies.....	23
General.....	24
Public Facilities and Services.....	29
Traffic Circulation System.....	29
Domestic Water Supply	30
Wastewater Disposal.....	30
Storm Drainage	30
Parks, Open Space and Landscaping.....	32
Schools.....	32
Public Safety Services	33
Public Utilities	33
Project Design.....	33

CHAPTER FOUR - PLAN IMPLEMENTATION AND PHASING

Phasing of Development and Improvements.....37
Circulation System Improvements.....37
Water, Sewer and Storm Drainage Improvements39
Project Phasing and Development.....40
Procedures for Project Approval, Plan Amendments and Modifications40
Relationship to City General Plan and Zoning.....42

List of Figures

FIGURE NO. PAGE	TITLE
1	Plan Area Boundary2
2	Location Map8
3	Plan Area Subareas.....9
4	Existing Land Uses.....11
5	Traffic Circulation System.....12
6	Conceptual Development Plan.....20
7	Typical On-Site Circulation Plan and Driveway Access31

List of Tables

TABLE NO. PAGE	TITLE
3-1	Summary of Permitted Land Uses and Development Intensities.....21
3-2	Trip Generation Calculation – Driveway Volumes22
4-1	Traffic Passby, Mixed Use, Calculation Results38
4-2	Incremental Traffic, ADT38

PREFACE

This report represents the policy document and plan diagram for the *Demaree/Caldwell Specific Plan*, a sixty-six acre commercial, professional office, and multiple residential site located in the southwest portion of the City of Visalia. The *Demaree/Caldwell Specific Plan* is intended to be an implementation tool of the City's *General Plan* and includes development regulations to preserve and enhance the quality and character of the community. It contains development standards, phasing regulations, and a coordinated development plan for 26 individually owned parcels of property. The property owners sponsored preparation of the plan.

The Plan was prepared in response to the *General Plan Land Use Element* requirement that a specific plan be prepared for community centers in the City. The Specific Plan is based on the land use plan and land use designations that were approved by the City Council on April 1, 1996. Once adopted by the City, the Specific Plan will govern the development of the properties and reduce the need for subsequent conditional use permits, traffic elements and discretionary approvals.

INTRODUCTION

1

INTRODUCTION

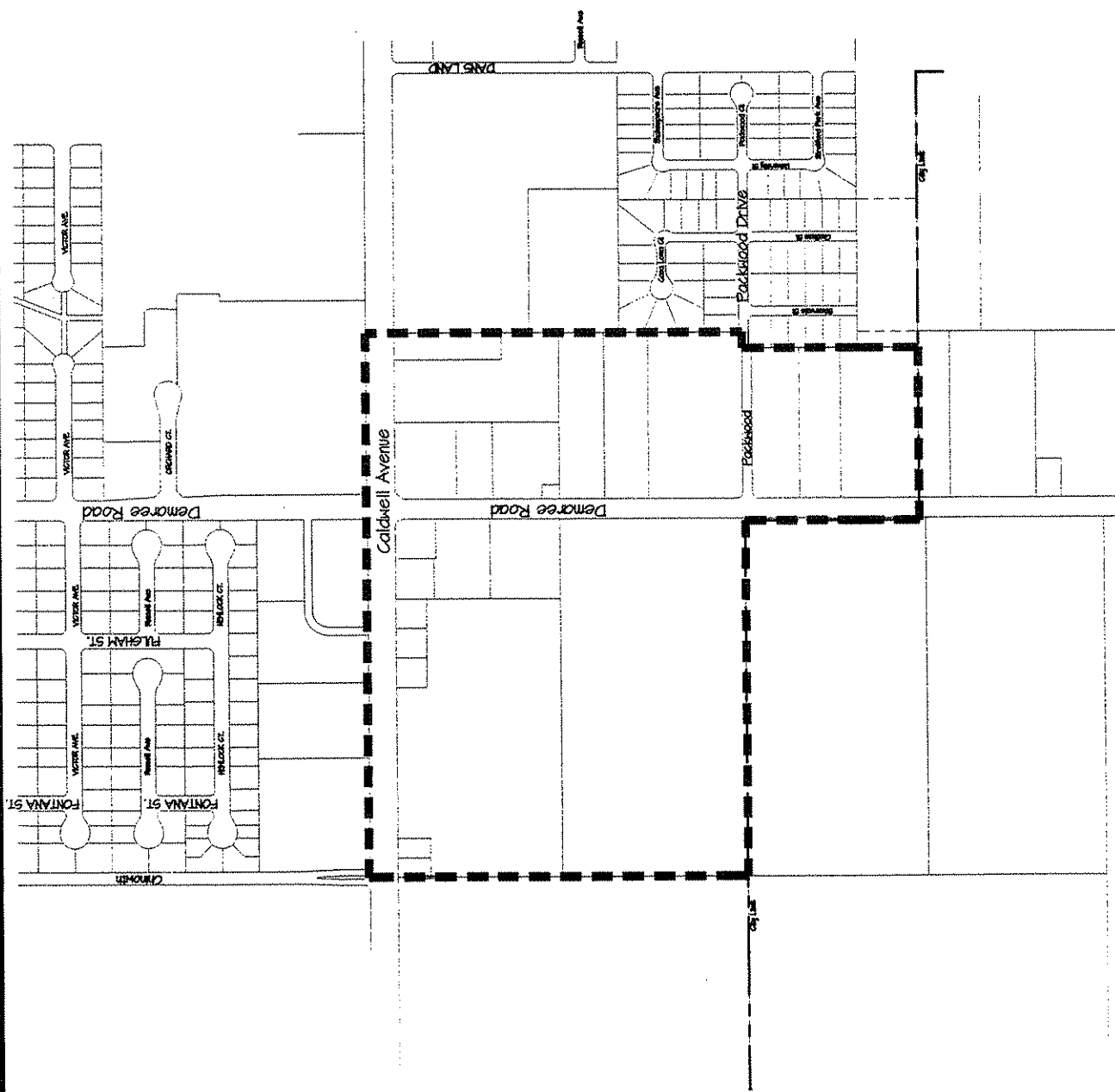
This document and accompanying plan diagram represent the *Demaree/Caldwell Specific Plan*, prepared for and adopted by the City of Visalia to guide the development, land uses and supportive services for a portion of the Southwest Community Center. The plan has been prepared by Quad Knopf, a professional planning consulting firm with offices in Visalia, Fresno, Bakersfield and Sacramento. The project was funded by the property owners in the specific Plan Area. The plan reflects a composite of property owner land use proposals, standards and policies derived from City planning documents and programs, and regulations intended to ensure that development in the Specific Plan area complies with the City of Visalia *General Plan*.

Purpose and Scope of Plan

The purpose of the *Demaree/Caldwell Specific Plan* is to establish the policy framework and guidelines for the long-term evolution and development of land uses and supportive infrastructure and services for the Plan Area, and to identify the type, nature and phasing of commercial, office and residential development in the City's Southwest Community Center. Caldwell Avenue, a major east-west arterial traffic route through the community, comprises the northerly boundary of the Plan Area. Demaree Road bisects the Plan Area, and this proximity substantially influences the potential development and long-term use of the Plan Area. All of the Plan Area is within the corporate limits of the City of Visalia. Accordingly, preparation and adoption of this specific plan has been undertaken to govern the long-term development of the Plan Area in accordance with the City's *General Plan* land use policies.

The Plan Area Boundary for this plan is illustrated by Figure 1. In total, the Plan Area includes approximately 66 gross acres. Selection of the Plan Area was based upon the following criteria:

- ▶ The adopted Visalia *General Plan* which designates 51 acres of "Community Center Commercial" land use for the Demaree/Caldwell intersection Plan Area; the actual distribution and configuration of this development was deferred to the preparation of this specific plan;
- ▶ The distribution of land ownership and parcelization of the Plan Area creates limits regarding the configuration of prospective development of the site;



DEMAREE/CALDWELL SPECIFIC PLAN
PLAN AREA BOUNDARY

FIGURE 1



QUAD

- ▶ The need for redistribution of 27 acres of regional retail uses for Mooney Boulevard to Community Commercial Centers as described in the 1995 *Circulation Element Update*; and,
- ▶ The opportunity to develop Demaree Road as a scenic gateway into the community as designated by the *Land Use Element* policy 1.1.11.

The "planning period", the time frame the plan is intended to address, is twenty-three years, to the year 2020. This time frame is consistent with the *Land Use Element* of the *General Plan*. At the same time, it is acknowledged that the full development of the Plan Area is likely to occur within a much shorter time frame since it is within the City's existing Urban Development Boundary. However, the City has expressed its intent that the *Demaree/Caldwell Specific Plan* establish standards and policies which will be applicable to the Caldwell and Demaree corridors, the development of which may extend over the longer time frame.

Legal Basis for Plan

As provided for in Sections 65450 through 65457 of the *California Government Code*, and described in the *State General Plan Guidelines* promulgated by the State Office of Planning and Research (OPR), a specific plan is a tool for the "systematic implementation" of the general plan of a local jurisdiction. Following the prescribed requirements of the referenced sections of the *Government Code*, a specific plan is to be composed of text and diagrams which:

- ▶ Depict the distribution, location and extent of land uses within the area covered by the plan;
- ▶ Describe the proposed distribution, location, extent, and intensity of major components of public and private infrastructure (e.g. sewage, water, drainage, solid waste disposal, energy, transportation, and related systems and facilities);
- ▶ Establish standards and criteria for the progress of development of the Plan Area and for the conservation, development, and utilization of natural resources, where applicable; and,
- ▶ Spell out an implementation program to carry out the goals and policies of the plan, including regulations, programs, public works projects, and potential financing strategies and measures.

A specific plan may also include any other subjects which are, in the determination of the City, necessary to facilitate implementation and administration of the General Plan as it affects the specific Plan Area. Specific plans must be consistent with the City's applicable General Plan elements. Another required component of a specific plan is a statement documenting the precise relationship of the plan to the City's *General Plan*.

Specific plans are a comparatively common tool among California communities for implementing general plan policies and programs, particularly in instances where the planning environment is unusually sensitive or where planning issues are more complex. In the City of Visalia, seven prior specific plans have been adopted: the *Northeast Area Specific Plan*, governing the long-term development of about 1,700 acres in the northeast quadrant of the community; the *Modoc Specific Plan*, addressing development of just over 300 acres in north-central Visalia (no longer in effect); the *West Visalia Specific Plan*, encompassing approximately 3,850 acres along the Highway 198 corridor in the westerly extreme of the City; the *Togni Specific Plan* which guides the development of the northeast and northwest areas of Caldwell Avenue and Demaree Road; the *Caldwell 51 Specific Plan* which provides development guidance of 55 acres along Caldwell Avenue east of Mooney Boulevard; the *Togni PUD* and, the *Country Club Estates Specific Plan*. In these specific plans, planning and development policies and standards were prescribed at a significantly greater level of detail than in the City's overall general plan, and were accompanied by a variety of programs and measures to accomplish their implementation. As reflected by the examples of these prior plans, the specific plan concept is utilized by the City as both a regulatory and a policy approach to local planning issues.

Historical Background

With the adoption of the *Land Use Element* to the General Plan in 1991, the Visalia City Council created the designation of "Community Center" to 123 acres of land in the Demaree/Caldwell area. This designation was meant to provide for an integrated mix of commercial, office, residential and public institutional land uses for this quadrant of the City. This Community Center Specific Plan is meant to serve the needs of the community in the southwest area of Visalia.

Land Use Element 3.5.8 states: "*Develop Community Centers for community-scale shopping with a wide range of commercial goods and services. Uses in the Community Centers shall be community-, neighborhood-, or convenience-level draw only. No uses which are primarily of regional draw or uses which would compete with Core Area uses shall be permitted.*"

Community Centers shall be developed as part of a Specific Plan for each of these areas. Each such Specific Plan shall designate the layout of improvements and land uses, development phasing and architectural standards. Specific phases or land uses which are found to be competitive with regional retail or Core Area land uses shall not be permitted or be designated for implementation in a time period which would conflict with other commercial and office development goals.

General guidelines for development shall be 20-30 acres of community-level retail and ancillary facilities, and up to 10 acres of Garden offices for each quadrant of the community served. Supporting facilities shall include up to 20 acres of multi-family residential development and a minimum of 20-30 acres for institutional facilities (churches, senior

residential) facilities, to be integrated into Community Center commercial area with public art and open space. The precise distribution of uses shall be determined at the time of development of a specific plan for the Community Center.

In 1995, a coalition of property owners in the Plan Area retained Quad Knopf to facilitate the implementation of a Specific Plan to address design issues and to insure a balance in land use distribution and requirements as specified by the *Land Use Element*. Quad Knopf has meet with city staff on numerous occasions to address question and concerns needed to formulate this detailed plan

On April 1, 1996 the City Council adopted General Plan Amendment No. 9513 and Change of Zone No. 9514, amending the allocation of distributed land designated as Community Center by the *General Plan*. The City Council expressed a great deal of confidence in the property owners' ability to implement a comprehensively planned center with a high degree of landscaping along Demaree and Caldwell Road, and to develop signage and architectural guidelines to ensure design compatibility. This amendment was adopted based in part on the expectation that, when developed, Demaree Road and Caldwell would be scenic entrances to Visalia. A higher density of landscaping and use of greater setbacks than currently prescribed by the current *Zoning Ordinance* would facilitate this.

On Monday, April 21, 1998 the City Council, after a public hearing, re-introduced Ordinance 96-03 establishing the zoning for the properties in the Plan Area. This step was taken because of the delay between first introduction of the ordinance and complete agreement thereto by all affected property owners.

Plan Formulation Process

Preparation and adoption of the *Demaree/Caldwell Specific Plan* has involved a process which combines technical analysis and study with a strong commitment to integrate the plan with other policy efforts in progress at the City staff level. The plan has evolved through a process of extensive dialogue among staff, the property owners and their representatives, and the City's Planning Commission and Council. The final concepts included among the plan's land use proposals, design and development standards, are a reflection of the concerns of all of these parties. Actual buildout of the Plan Area, in accordance with the principles and policies set out in the plan, will conform to the standards of the plan, as well as to those prescribed by the City's underlying General Plan, zoning, and related policies and standards.

The Specific Plan is intended to achieve the following:

1. Common design guidelines for properties in the specific Plan Area;
2. A mixture of residential, commercial and office uses;
3. A diverse mixture of commercial services and retail goods;

4. Design guidelines which provide for the establishment of an attractive gateway to Visalia;
5. An infrastructure phasing and financing program that equitably allocates development cost responsibilities to the City and to individual property owners; and,
6. Reduction of subsequent traffic studies, conditional use permits or other discretionary approvals which would duplicate this Plan.

Based upon an initial study of the project's potential effects upon the environmental setting of the Plan Area and community at-large, the City has determined that although there are impacts which were not addressed in the *Land Use Element Environmental Impact Report*, such effects are not considered to be significant. Accordingly, a negative declaration was prepared and adopted for changes that occurred subsequent to certification of the *Land Use Element Environmental Impact Report*.

Content and Format of Plan Document

The content and format of this plan document correspond to the requirements of the California Government Code and the *State General Plan Guidelines* for the preparation and adoption of specific plans and Chapter 12.04 of the Visalia Ordinance Code. Subsequent chapters of this document include a summary description of existing conditions in the Plan Area; a definition of the goals and policies for the Plan Area land use, circulation, design and infrastructure development plan for the area; development standards and design guidelines for the various elements of the plan; and implementation measures and programs to accomplish the goals and policies of the plan.

Accompanying the plan text is the required plan diagram, which outlines and delineates the distribution and intensity of Plan Area land uses, major circulation system components, and other important public facilities and infrastructure.

EXISTING CONDITIONS IN PLAN AREA

2

EXISTING CONDITIONS IN PLAN AREA

The overall Plan Area for the *Demaree/Caldwell Specific Plan* encompasses approximately 66 gross acres (62.31 net acres). The area lies near the southerly limits of urban development in southwest Visalia, in a vicinity currently characterized by a mix of both urban and agricultural land uses. The Plan Area is bounded by Caldwell Avenue to the north, an extension of Chinowth Road to the west, Demaree Road dividing the center and Packwood Drive (and its extension) to the south. Figure 2, on the following page, depicts the location of the Plan Area in relationship to the rest of the community.

Definition of Plan Subareas

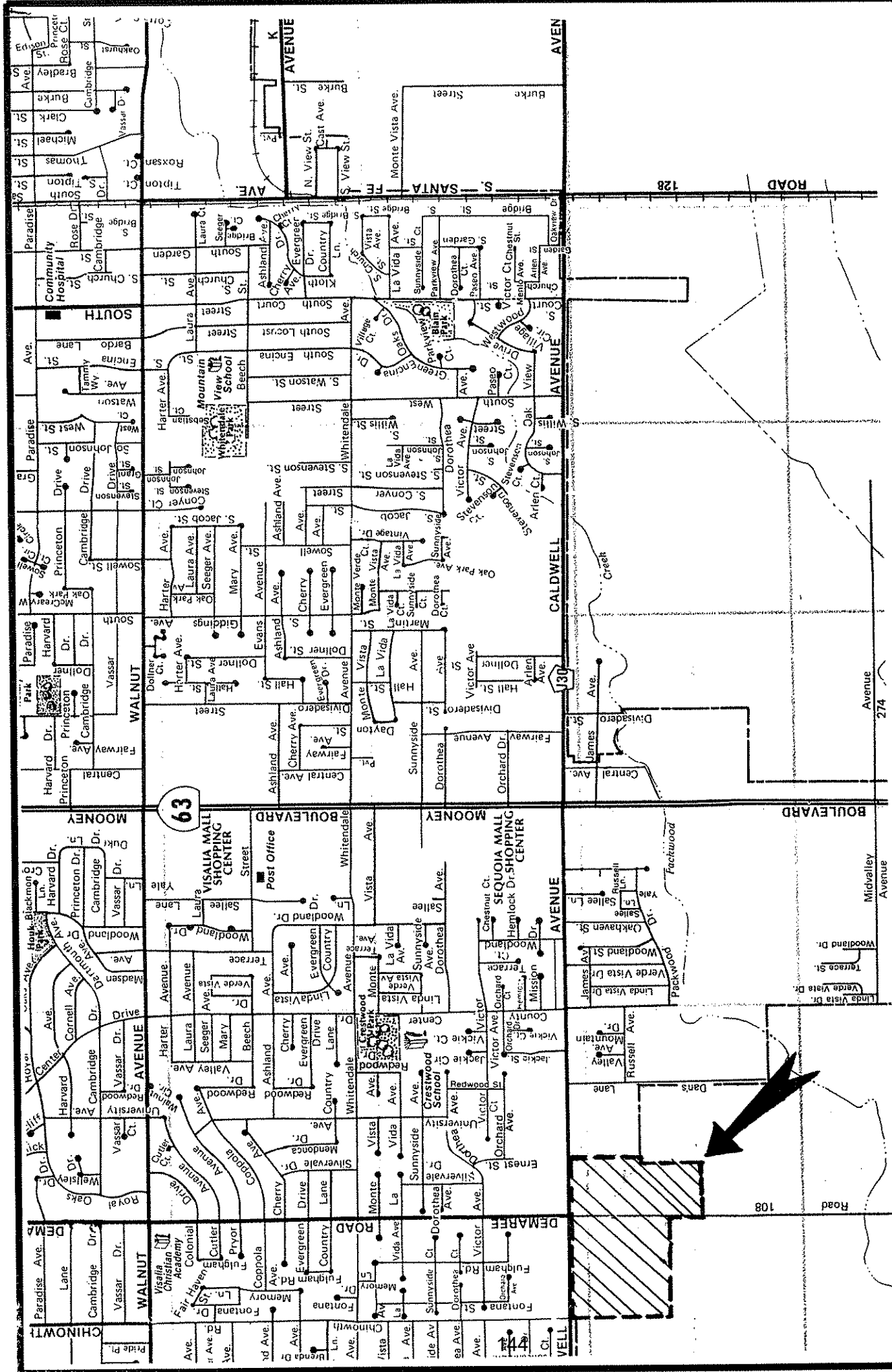
The Plan Area has been divided into five subareas to facilitate the analysis of the specific planning issues particularly applicable to each. Figure 3 illustrates the distribution of these subareas. Designation of each subarea was based upon two factors: (1) existing topographic and land use features; (2) relationship to the issues associated with proposed land uses and development for each; and, (3) separation of the Plan Area by existing roadways. Brief descriptions of the characteristics of each subarea are set out in the following paragraphs.

Subarea A

This plan subarea is comprised of approximately 28.73 acres located on the south side of Caldwell Avenue with Demaree Road bounding the east and the Chinowth Road extension to the west of this Plan Area. At this present time, a convenience store and office complex are located at the northwest corner of this subarea. Adjacent to the commercial development is a small apartment complex comprising 4 units as well as four single-family residences located along Caldwell Avenue. An existing church lies approximately at the mid-point of the subarea along Demaree Road south of Caldwell Avenue frontage. The balance of the area is open agricultural land use and is planted in row crops.

Subarea B

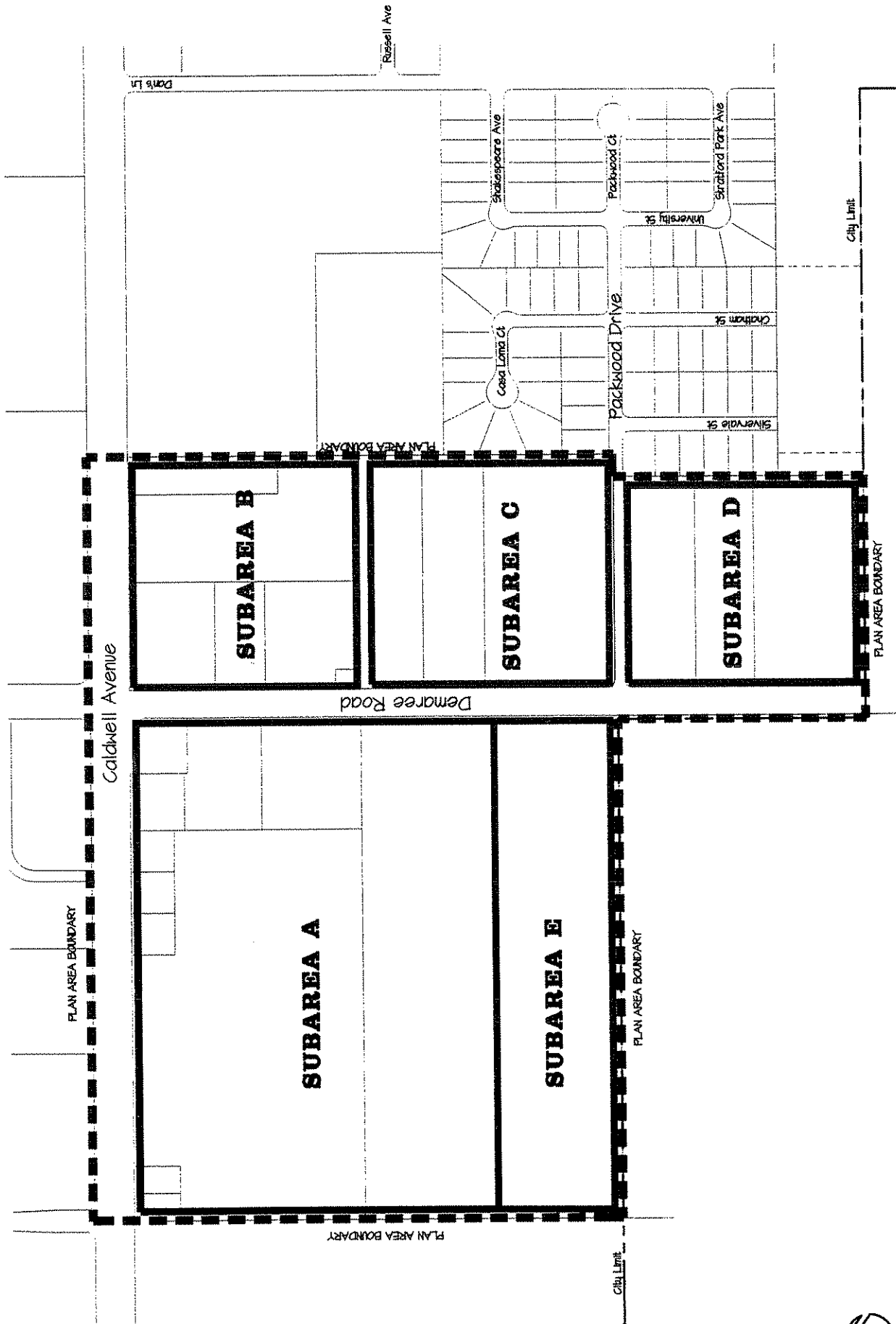
This subarea totals approximately 8.28 acres of land located on the south side of Caldwell Avenue with Demaree Road bounding the west side. At the present time, most of this ground lies fallow, with the exception of a single-family residence located toward the northeast portion of the subarea.



DEMAREE/CALDWELL SPECIFIC PLAN

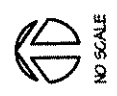
LOCATION MAP

QUAD



DEMAREE/CALDWELL SPECIFIC PLAN
PLAN AREA SUBAREAS

FIGURE 3



NO SCALE

QUAD

Subarea C

Subarea C contains approximately 8.97 acres of land. This area is bounded by Demaree Road to the west and Packwood Drive to the south. Two single family dwellings are located on the northerly half of the subarea.

Subarea D

Subarea D consist of approximately 7.67 acres of land. This subarea is bounded by Demaree Road to the west and Packwood Drive to the north. The land is presently vacant.

Subarea E

This subarea is bounded by Demaree Road to the east, an extension of Packwood Drive to the south and an extension of Chinowth Road to the west. Subarea E consist of approximately 8.66 acres of land. The present use of the this land is field crops.

This separation into subareas is for the purpose of analytic or descriptive convenience only. Planning uses are interrelated and the plan should be considered a whole.

Current Plan Area Land Uses

Figure 4 depicts the extent and character of existing land uses in the Plan Area and surrounding vicinity. As this figure illustrates, the Plan Area itself is devoted to a mix of uses including quasi-public, convenience, commercial, residential, and agricultural production at this time. Surrounding uses include a mixture of quasi-public and commercial development to the north; residential development and agriculture to the west, residential development to the east; and agriculture to the south.

Existing Traffic Circulation System - Plan Area and Vicinity

The currently planned traffic circulation system serving the Plan Area and surrounding vicinity is illustrated in Figure 5. The principal existing components of this system are Demaree Road and Caldwell Avenue, both designated as arterials and slated to carry very substantial traffic volumes through this portion of Visalia. At present, Caldwell Avenue has an average daily traffic (ADT) volume of approximately 18,000 and Demaree has a traffic volume of 12,400 ADT. By the year 2020, it is projected that this stretch of Caldwell Avenue may carry between 35,475 and 38,169 ADT and Demaree Road 18,595 to 19,020 ADT according to the Countywide traffic model with 2020 land uses. Caldwell and Demaree are to be improved with landscaped medians and appropriate left turn "pockets", intersection signalization, and related improvements intended to render these streets capable of carrying the projected traffic volumes. The Chinowth Road extension is the westerly boundary of the Plan Area. The southerly extension of Chinowth is not yet an adopted component; however, it is proposed as part of the current *Circulation Element* update.



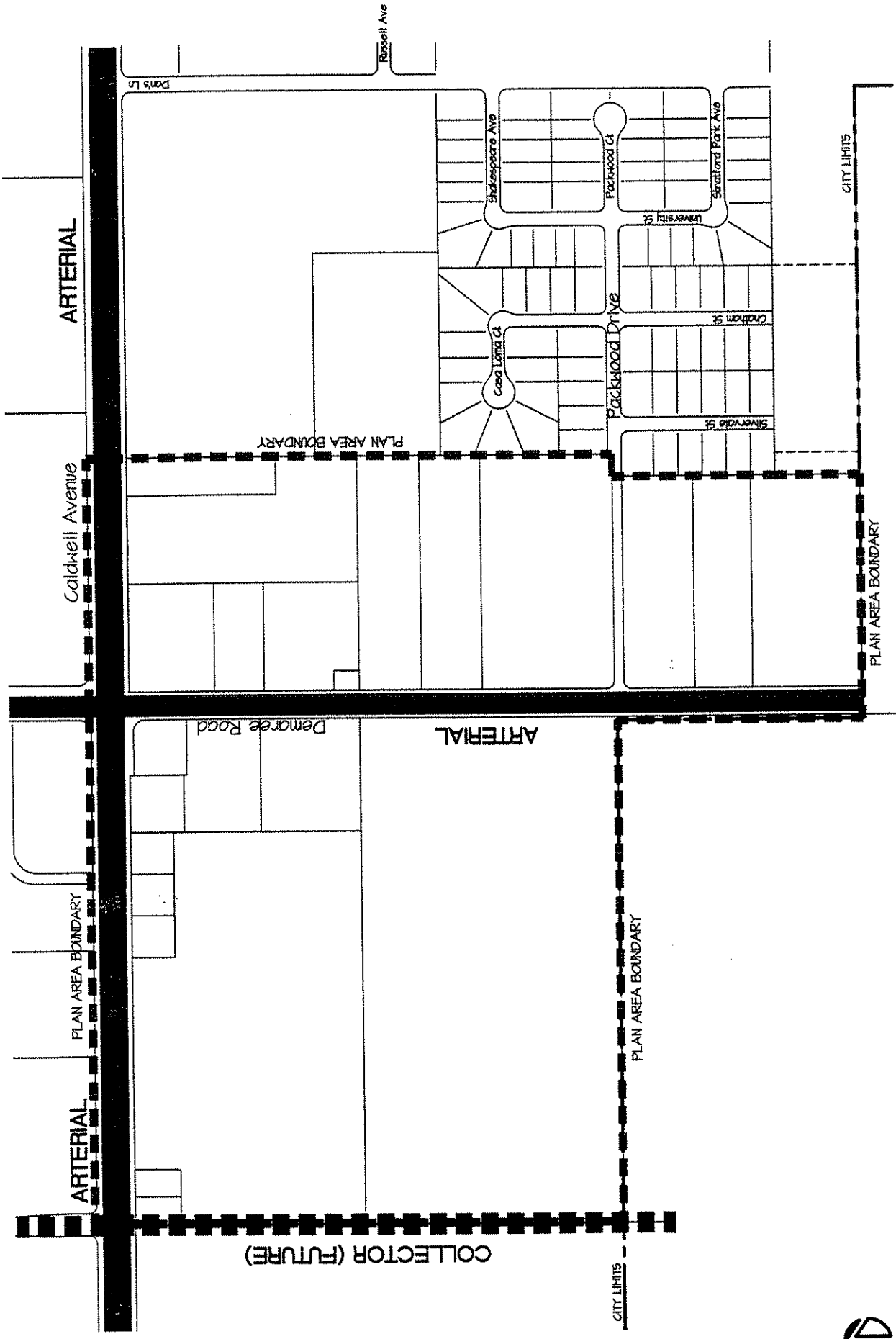
FIGURE 5

DEMAREE/CALDWELL SPECIFIC PLAN
TRAFFIC CIRCULATION SYSTEM

5110 W. COWLESS AVE.
P.O. BOX 3079
VISALIA, CA 93278
TEL: (800) 733-0440
FAX: (209) 733-1021
EMAIL: quad@quadknopf.com



Quad Knopf



Existing Plan Area Infrastructure

Water supply to the Plan Area and surrounding vicinity is provided by the California Water Service Company. An eight-inch main presently exists in Caldwell Avenue, adjacent to the Plan Area. Based upon contacts with officials of the California Water Service Company, the supply of domestic water to the Plan Area is adequate to serve urban development.

Sewer service to the Plan Area is available from the City of Visalia. There is currently a twenty-four-inch trunk sewer main in Caldwell Avenue, adjacent to the site, with adequate capacity to accommodate anticipated flows from Plan Area development. The City's wastewater treatment and disposal facilities, located west of Highway 99 approximately five miles from the Plan Area, also have adequate current capacity to accept waste flows from plan-related development.

Because of its predominantly undeveloped nature, the Plan Area is not served by any improved storm drainage facilities at this time. With the exception of the convenience store and office complex natural storm runoff is currently either absorbed or evaporated on-site. Subsequent to development, storm drainage improvements will be necessary to handle the increased runoff typically associated with the overcovering of more permeable soils with hardscape surfaces. It is expected that these improvements will be a combination of on-site privately-maintained curbs, gutters and catch basins and an offsite basin and disposal facility on Packwood Creek. The City has adopted a storm drain master plan, which projects that collection, on-site detention, discharge to Packwood Creek, and ultimate disposal in downstream retention basins, will accommodate Plan Area development-generated storm runoff.

Electrical service to the vicinity of the Plan Area is provided by the Southern California Edison Company; natural gas service is supplied by the Southern California Gas Company; telephone service is provided by the Pacific Telephone Company, in combination with affiliated or supportive long-distance services. All of these utilities currently have major lines in Caldwell Avenue and indicate that extension of service to plan-related development would be routine.

Identified Issues of Significance in Determining Plan Approaches

An early step in the process of developing the *Demaree/Caldwell Specific Plan* was the identification of issues which might potentially be influential or determinant in the establishment of the ultimate nature and content of the plan. A number of meetings with City staff were conducted to secure input to the applicants regarding concerns which would potentially affect the planning process. In the following paragraphs of this section, the issues perceived by property owners, the City staff, the Planning Commission, the City Council, and others as being important to shaping the plan are restated. Each issue and/or constraint is listed in the context of the general aspect of the Plan Area environment or planning process to which it most appropriately applies.

Land Use

- ▶ The *Land Use Element* did not spell out a specific implementation strategy for the Community Center areas. Instead, specific plans were to be developed to designate “the layout of improvements and land uses, development phasing and architectural standards.”
- ▶ The northeast corner of Demaree and Caldwell, an original component of the Southwest Community Center is currently developed to a large, single-use tenant. To ensure that the Community Commercial area south of Caldwell provides for convenience, neighborhood and community convenience uses, a mixture of sizes is provided.
- ▶ The intersection of Demaree and Caldwell provides connection to the west, east, north and south. Commercial development at this regional “node” will reduce the need for commercial development elsewhere in the community.
- ▶ The Plan Area is comprised of properties ranging in size from .07 acres to 19.36 acres. Development plans must provide sufficient flexibility to permit individual property owners to proceed with development.
- ▶ Development of commercial and office uses in the Plan Area is to be accomplished in a manner such that potential impacts upon anticipated future residential land uses adjacent to the east and south are minimized.

Circulation

- ▶ The Plan Area will require access from both Caldwell Avenue and Demaree with a controlled signalized intersection at Demaree and Caldwell Avenue, and at Demaree and Packwood.
- ▶ Left turn pockets and “worms” are proposed to provide accessibility to Plan Area parcels while maintaining traffic flows.
- ▶ Consistent with the City's designation of Caldwell Avenue and Demaree Road as arterial traffic routes, driveway access to these streets from abutting private properties is to be limited consistent with *Circulation Element* standards.

Urban Design and Aesthetics

- ▶ The landscape and aesthetic treatment of the Caldwell Avenue and Demaree Road frontages in the Plan Area is important.
- ▶ General design standards will be required for all on-site improvements, to ensure attractive development of the Plan Area, and mitigation of light and glare and

noise impacts on surrounding land uses, and design consistency from project to project. However, adequate flexibility is reserved for individual property design.

Administration and Implementation

- ▶ Flexibility in the administration of the plan will be essential. The plan will, therefore, specify development capacities, phasing criteria and performance standards. Further, the Specific Plan will identify infrastructure financing responsibilities and phasing requirements for improvements.

DEVELOPMENT PLAN FOR THE DEMAREE/CALDWELL AREA

3

DEVELOPMENT PLAN FOR THE DEMAREE / CALDWELL AREA

This chapter outlines and describes the elements of the proposed development plan for the objectives and policies of this specific plan. There are five separate elements of this development plan: plan goals, the land use and circulation plan, plan policies, public facilities and services and project design.

Plan Goals

The *Demaree/Caldwell Specific Plan* has been developed to facilitate the attainment of the goals and objectives of the General Plan of the City of Visalia. In furtherance of this purpose, specific goals and policies have been formulated which apply to the Plan Area and are an integral part of the plan.

A *goal* is a "direction-setter". It is an ideal future end, condition or state related to the public health, safety or general welfare, toward which planning or implementation measures are directed. A goal is a general expression of community values and, therefore, is abstract in nature. Consequently, a goal is typically not quantifiable, time-oriented or suggestive of specific actions for its achievement.

An *objective* is a quantifiable or measurable target that represents an incremental step toward achievement of a specified goal. Objectives can be stated in quantitative terms or as measurable milestones to measure progress towards a goal.

A *policy* is a specific statement that guides decision-making. It indicates a clear commitment of the local legislative body. A policy is best stated when it is clear and unambiguous. Policy statements form the foundation of this specific plan in guiding future development patterns and intensities in the Plan Area.

Policies applicable to the development and use of the Plan Area are set out in a subsequent section of this chapter, corresponding to each subarea of the overall Plan Area. The following, more general, statements represent the *goals* of the City of Visalia in preparing, adopting and implementing the *Demaree/Caldwell Specific Plan*:

1. Establishment of land use and development patterns for the *Demaree/Caldwell* area which are compatible with the capacity of the land and supportive infrastructure to accommodate them and which are sensitive to the environmental setting into which they are introduced.

2. Provision of additional opportunities for the local development of large-scale retail commercial outlets at accessible and appropriate sites.
3. Creation of office development opportunities.
4. Provision of an opportunity to develop professional offices serving the southerly portion of the community, taking advantage of the Plan Area's Demaree Road exposure to develop a high quality scenic corridor.
5. Assurance that the quality and character of development to take place in the Plan Area is equivalent or superior to that of the development which has occurred already in similar areas elsewhere in the community.
6. Provision for an orderly and efficient transition between commercial and residential land uses.
7. Provision for equitable and appropriate economic returns from lands located within the Plan Area.
8. Establishment and maintenance of balance between the interests of private property ownership and the general benefit and welfare of the community at-large.
9. Establishment of standards for the development of landscaping, street furnishings, setbacks, and quality and character of land uses.
10. Protection of the quality of life enjoyed by residents in the vicinity of the Plan Area from unreasonable alteration or disruption.
11. Balance between the need to minimize potentially adverse effects upon the community and regional environments associated with economic and population growth and the demand for such growth and its positive impacts upon the public welfare.

The plan should also promote the following *Land Use Element* objectives and implementing policies.

Land Use Element Objectives

- 3.5A *Maintain Visalia's role as the regional retailing center for Tulare and Kings Counties.*
- 3.5B *Ensure the continued viability of Visalia's existing commercial areas.*

- 3.5C *Promote comprehensively planned, concentric commercial areas to meet the needs of Visalia residents and its market area.*
- 3.5D *Create and maintain a commercial land use classification system (including location and development criteria) which is responsive to the needs of shoppers, maximizing accessibility and minimizing trip length.*
- 3.5E *Designate appropriate and sufficient commercial land for Visalia's needs to the year 2020 with appropriate phasing.*

Land Use Element Implementing Policies

- 3.5.1 *Ensure that future commercial development is concentrated in shopping districts and nodes to discourage expansion of new strip commercial development.*
- 3.5.2 *Ensure that commercial development in residential areas serves the needs of the area and includes site development standards which minimize negative impacts on abutting properties.*
- 3.5.4 *Designate land areas in 10-year increments for future commercial and office development. Commercial and office areas outside of the Urban Development Boundary shall be designated for commercial or office "reserve". These areas are to be zoned for agriculture and may be rezoned for commercial use upon the following findings by the Planning Commission and City Council:*
 - a. *Property is necessary to meet the needs of the shopping public.*
 - b. *Property is adequately served or will be adequately served by public facilities including streets, sewerage, police and fire protection, water supply, and other facilities.*
 - c. *Properties located within the previous boundary are developed or do not provide the likelihood of being developed in the time frame appropriate to meet the needs of the community.*
 - d. *Properties are determined to provide a significant social and economic benefit to the community.*
- 3.5.8 *Develop Community Centers for community-scale shopping with a wide range of commercial goods and services. Uses in the Community Centers shall be of community-, neighborhood-, or convenience-level draw only. No uses which are primarily of a regional draw or uses which would compete with Core Area uses shall be permitted. Locations shall be limited to arterial intersections which have connections to freeway access and adequate north-south and east-west circulation. General locations for community centers are as follows:*

- a. *Northeast, northwest or southeast corner of Riggin and Highway 63.*
- b. *Demaree and Caldwell.*
- c. *Lovers Lane between the Parkway and Caldwell. (Reserve)*
- d. *Northeast corner of Demaree and Riggin. (Reserve)*

Community Centers shall be developed as part of a Specific Plan for each of these areas. Each such Specific Plan shall designate the layout of improvements and land uses, development phasing and architectural standards. Specific phases or land uses which are found to be competitive with regional retail or Core Area land uses shall not be permitted or be designated for implementation in a time period which would conflict with other commercial and office development goals.

General guidelines for development shall be 20-30 acres of community-level retail and ancillary facilities, and up to ten acres of Garden offices for each quadrant of the community served. Supporting facilities shall include up to twenty acres of multi-family residential development and a minimum of 20-30 acres for institutional facilities (churches, senior residential) facilities, to be integrated into Community Center commercial area with public art and open space. The precise distribution of uses shall be determined at the time of development of a specific plan for the Community Center.

A new zone shall be created to facilitate the development of the Community Center commercial area to ensure compatibility with the adjacent neighborhood and to ensure that the center does not conflict with regional retail or core area development objectives.

Intersections at the Community Centers shall be developed with high landscaping, setback, and architectural standards to minimize negative impacts on the surrounding neighborhood.

Land Use and Circulation Plan

An underlying, fundamental, issue in the analysis of land use alternatives and patterns for the Plan Area was the necessary configuration of the proposed community commercial site in order for it to be optimally developable for large-scale outlets. Associated with this issue was the concern for access to the site in such a manner that provided for even traffic flow with minimum disruption to through traffic on Caldwell Avenue or Demaree Road, while still affording convenient ingress and egress to the development on-site.

The land use and circulation plan for the Plan Area is included in this document (Figure 6). The plan prescribes permitted land uses for each segment of the overall Plan Area, reflects

permitted development densities and intensities, identifies the anticipated circulation system to support development of the area as provided for by the plan, and delineates other significant features as called for by State planning law. The plan diagram is supplemented by detailed design and development standards, set out in a subsequent section of this document. In addition, it is the intent of this Specific Plan to encourage project design(s) which encourage pedestrian inter-access between the various land uses accommodated by the Plan.

Table 3-1 summarizes the land uses allowed by the plan for each of the overall subareas. It will be the approach of the plan to prescribe development "entitlements" for each subarea, corresponding to the specified land uses and development intensities set out in this document and on the plan diagram. Simply, for each subarea, subject to design and site plan approvals by the City and conformance with other design and development standards adopted and established by the City in addition to this plan, a developer can build any of the types of uses allowed by the City zoning ordinance.

TABLE 3-1

**Summary of Permitted Land Uses
And Development Intensities**

Subarea Designation	Net Acreage of Subarea	Permitted Land Uses	Potential Development Yield¹
A	28.73	Community-Level Retail Commercial	312,500 sq. ft.
B	8.28	Community-Level Retail Commercial	93,800 sq. ft.
C	8.97	Office Garden	97,700 sq. ft.
D	7.67	Office Garden	83,500 sq. ft.
E	8.66	High Density Residential	147 units

¹ Community Commercial and Office Garden, 25% building coverage of net acreage; High Density Residential, 17 units per acre.
Demaree/Caldwell Specific Plan
Quad Knopf, Inc.

Trip generation from each of the respective subareas was calculated as follows:

TABLE 3-2

**Average Daily Traffic (ADT)
Trip Generation Calculation – Driveway Volumes**

Subarea	Land Use			ADT, Driveway Volumes
	Community Commercial	Garden Office	High Density Residential	
A	13,062			13,062
B	3,920			3,920
C		1,191		1,191
D		1,018		1,018
E			911	911
Totals	16,982	2,209	911	20,102

Community Commercial: 41.8 trips per 1000 sq. ft. bldg. x 312,500 sq. ft. (A) and x 93,800 sq. ft. (B) (p.1234, ITE Trip Generation, Fifth Edition)

Garden Office: 12.2 trips per 1000 sq. ft. bldg. X 97,700 sq. ft. (C) and 83,500 sq. ft. (D) (p. 952, ITE Trip Generation, Fifth Edition)

High Density Residential: 6.2 trips per unit x 147 units (p. 311, ITE Trip Generation, 5th Edition)

NOTE: Calculations based on formulas, not Tables, and on combined community commercial and combined office garden square footages.

The above traffic generation calculations result in projected driveway volumes. No reduction has been made in this Table for “passby” trips that will utilize the planned facilities, or for overall reduction in traffic generation associated with the integration of the planned land uses. Incremental roadway traffic volumes will be lower. Section 4 will calculate such incremental roadway volumes for purposes of determining appropriate traffic impact fees.

Subarea A

Subarea A is designated by the plan for community-level retail development. Subsequent to required right-of-way dedication for Demaree Road and Caldwell Avenue, a developable area of 28.73 acres would remain. Design analysis indicates that 312,500 square feet of gross leasable commercial building area, accompanied by the required parking, landscaping, on-site circulation, and other site improvements can be developed on the segment of the Plan Area at twenty-five percent coverage. Access points are to be along Caldwell Avenue, and along Demaree Road, with the final locations of such access points to be determined during site design. Access on the west side of the development would be provided from a southerly extension of Chinowth Road. Building orientation would be toward both Demaree Road and Caldwell Avenue. Improvements to Demaree Road, Caldwell Avenue and Chinowth Road frontages of this subarea would be required, and would be completed, at the time of development.

Subarea B

Subarea B is also designated by the plan for community-level retail development. Subsequent to the required dedication along the Demaree Road and Caldwell Avenue frontages, 8.28 acres of developable area would remain in this subarea. Design analysis indicates that 93,800 square feet of gross leasable commercial building accompanied by the required parking, landscaping, on-site circulation, and other site improvements can be developed in this subarea, based upon twenty-five percent building coverage. As with subarea A, the required improvements on the Demaree Road and Caldwell Avenue frontages would be completed at the time of development. Access to the subarea B would be facilitated by the use of two entrance/exit driveway, one each along Caldwell Avenue and Demaree Road. A left turn "worm" from Caldwell would provide direct access for westbound traffic.

Subarea C

Development of Subarea C under this plan would be comprised of 97,700 gross square feet of garden office space at an assumed 25% building coverage of net land acreage. Access to the development would be comprised of one entrance/exit driveway along Demaree Road and one entrance/exit driveway along Packwood Avenue. Improvements to adjacent portions of Demaree Road and Packwood Drive would be required in conjunction with the development of the subarea.

Subarea D

This area, as designated by the plan, would be comprised of 83,500 gross square feet of garden office uses at an assumed 25% building coverage of net land acreage. Access to the this area, like Subarea C, would be one entrance/exit on Demaree road and along Packwood Drive. All required improvements along the adjacent Demaree Road and Packwood Drive frontages would be required to be constructed at the time this area is developed.

Subarea E

The development of Subarea E under the *Demaree/Caldwell-Specific Plan* would provide up to 147 units of high density residential housing assuming 17 units per net acre. Access to this segment of the overall Plan Area would be from a westerly extension of Packwood Drive. The development of this portion of the Plan Area will be consistent with Subarea D in terms of the landscaped corridor along Demaree Road.

Plan Policies

The policies set out in the following paragraphs are intended to govern the overall scope and character of prospective future development in the Plan Area. These policy statements form the basis for determining whether or not a specific development proposal conforms to the specific plan. They are established in order to provide clear guidelines to the City and

potential developers alike for the formulation, review and approval of projects. It should be emphasized that these policies augment applicable policy statements set out in the City's *General Plan*. Where policies and standards prescribed by this plan are more restrictive or specific than those in the *General Plan* or *Zoning Ordinance*, however, the text of this plan shall prevail.

General

The following policies are applicable to the Plan Area in its entirety:

- G-1 The Plan (Figure 6) accompanying this text shall be regarded as prescribing the distribution of land uses for the Plan Area. The locations and patterns of arterial and collector streets shall be regarded as fixed by Figure 6, as well. Unless otherwise prescribed by this plan, the specific location of median breaks, network of local streets and on-site circulation characteristics for any segment of the Plan Area shall be subject to City review and approval of specific development plans and designs. Transit stops shall be provided at locations determined by the City at site plan review.
- G-2 Design and development standards as set out in this plan and in other City plans, policies and ordinances adopted and in effect at the time of any design review shall be applied to all projects in the Plan Area, to ensure the highest possible quality and character of development. The relevant provisions of the City's *General Plan* and *Zoning Ordinance* shall apply to all development and uses in the Plan Area, except where the standards and conditions prescribed by this plan are more restrictive, in which case this plan shall prevail.
- G-3 The ultimate objective of the Specific Plan is to facilitate the development of a community commercial center which has a combination of professional offices, general commercial uses, general retail/drug store, food service uses, professional office uses, and residential uses in conformance with Policy 3.5.8, Policy 3.6.2 and the other applicable policies of the *General Plan*.
- G-4 For any subarea which is to be implemented in more than a single phase, the developer(s) shall be required to provide a phasing plan to the City. This plan will be applicable to all future development of the Plan Area and shall be enforceable upon all future owners and/or developers of the properties included within the boundaries of the area.
- G-5 All costs for traffic facilities not funded by traffic impact fees but required to support development of the Plan Area, which can partially be attributed to Plan Area development, shall be borne by each development subarea in proportion to the incremental traffic generated by such development as calculated in Section IV of this Specific Plan. All costs for sewer, water and storm drainage facilities

required to serve each Subarea shall be borne by that Subarea at the time of its development.

- G-6 This plan provides a detailed level of planning prior to development. Consequently, permitted uses in conformance with this plan will not require subsequent environmental or traffic analysis.

Subarea A

The following policies are applicable to Subarea A:

- A-1 The maximum development intensity for individual land uses in Subarea A shall be:
1. 312,500 square feet Gross Leasable Area (GLA) of community center commercial.
 2. Further, development intensity and type shall be limited as follows, except that exceptions thereto may be approved by the City upon presentation of documentation as to community need:
 - a. No more than fifteen percent of maximum allowable GLA shall be devoted to professional office uses.
 - b. No building providing a single type of merchandise (e.g., food, hardware, general drug store, restaurants, personal service, automotive uses, etc.) shall comprise more than twenty percent of the maximum allowable subarea GLA. Where a tenant contains multiple lines of merchandise (e.g., grocery, drug, general merchandise), the maximum building size shall not exceed forty percent of total subarea GLA.
- A-2 Land uses and development in Subarea A shall conform to the requirements of the equivalent zone district (or its equivalent or comparable zone, should the City's *Zoning Ordinance* be modified) as set out in the City of Visalia's *Zoning Ordinance*, except as may otherwise be provided for specifically by this plan.
- A-3 Design and development standards shall ensure that nearby residential development is not adversely affected by commercial development which will take place in this subarea.
- A-4 Dedication and development of right-of-way along the subarea's frontages of Caldwell Avenue, Demaree and Chinowth Roads will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the

City's *General Plan*. Access points shall be generally consistent with those shown on Figure 7.

- A-5 Development within Subarea A shall be designed in such a way as to appear that the development is continuous and related.
- A-6 Access from Caldwell into the subarea shall be provided through a left turn pocket, and from Demaree into the Subarea through a left turn pocket.

Subarea B

The following policies are applicable to Subarea B

- B-1 The maximum development intensity for individual land uses in Subarea B shall be:
 - 1. 93,800 square feet of GLA of community center commercial.
 - 2. Further, development intensity and type shall be limited as follows, except that exceptions thereto may be approved by the City upon presentation of documentation as to community need.
 - a. No more than twenty percent of the maximum allowable GLA shall be devoted to professional office uses.
 - b. No building with a single type of merchandise (e.g., food, hardware, general drug, personal service, etc.) shall comprise more than fifty percent of the maximum allowable GLA. Where a major tenant contains multiple merchandise lines (e.g. drug, grocery, soft goods, sundries, etc.), such major tenant shall not exceed eighty percent of total maximum allowable GLA.
- B-2 Land uses and development in Subarea B shall conform to the requirements of the equivalent zone district (or its equivalent or comparable zone, should the City's *Zoning Ordinance* be modified) as set out in the City of Visalia's *Zoning Ordinance*, except as may otherwise be provided for specifically by this plan.
- B-3 Design and development standards shall ensure that nearby residential development is not adversely affected by commercial development which will take place in this subarea.
- B-4 Dedication and development of right-of-way along the subarea's frontages of Caldwell Avenue and Demaree Road will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the

City's *General Plan*. Access points shall be generally consistent with those shown on Figure 7.

- B-5 Development within Subarea B shall be designed in such a way as to appear that the development is continuous and related.
- B-6 A left turn pocket shall be provided on Caldwell to permit westbound traffic to enter the Subarea. A left-turn pocket shall be provided on Demaree to permit north bound traffic to enter the Subarea.

Subarea C

The following policies are applicable to Subarea C:

- C-1 The maximum development intensity for individual land uses in Subarea C shall be:
 - 1. 97,700 square feet GLA of professional office.
- C-2 Land uses and development in Subarea C shall conform to the requirements of the equivalent zone district (or its equivalent or comparable zone, should the City's *Zoning Ordinance* be modified) as set out in the City of Visalia's *Zoning Ordinance*, except as may otherwise be provided for specifically by this plan.
- C-3 Design and development standards shall ensure that nearby residential development is not adversely affected by commercial development which will take place in this subarea.
- C-4 Dedication and development of right-of-way along the frontages of Demaree Road and Packwood Drive will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the City's General Plan. Access points shall be generally consistent with those shown on Figure 7.
- C-5 Development within Subarea C shall be designed in such a way as to appear that the development is continuous and related.
- C-6 In order to ensure that development in Subarea C is not adversely affected by the adjacent commercial development to the north, the common drive located between Subarea B and C shall be a one directional, right-turn only, drive.

Subarea D

The following policies are applicable to Subarea D:

- D-1 The maximum development intensity for individual land uses in Subarea D shall be:
 - 1. 83,500 square feet GLA of professional office use.
- D-2 Land uses and development in Subarea D shall conform to the requirements of the equivalent zone district (or its equivalent or comparable zone, should the City's *Zoning Ordinance* be modified) as set out in the City of Visalia's *Zoning Ordinance*, except as may otherwise be provided for specifically by this plan.
- D-3 Dedication and development of right-of-way along the frontages of Demaree Road and Packwood Drive will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the City's General Plan. Each property comprising the subarea shall be responsible for development of its respective frontages.
- D-4 Development within Subarea D shall be designed in such a way as to appear that the development is continuous and related.
- D-5 Access to Subarea D from Demaree Road shall be limited to one driveway for ingress and egress.

Subarea E

The following policies are applicable to Subarea E:

- E-1 The development intensity permitted in Subarea E shall be limited to the equivalent of 147 multi-family residential units.
- E-2 Design and amenities associated with Subarea E shall be consistent with those typically associated with high density residential communities; privately-maintained landscaping will be created in conjunction with this development.
- E-3 Dedication and development of right-of-way along the frontages of Packwood Drive and Chinowth Road will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the City's General Plan.
- E-6 All streets interior to the development in Subarea E shall be privately-owned and maintained.

Public Facilities and Services

Development of the Plan Area will include the creation of public improvements. Similarly, the introduction of commercial, office and residential development in the Plan Area will create increased demand for public services. The following paragraphs of this section describe the scope and nature of plan-related public facilities and discuss the manner in which essential public services will be provided to the Plan Area. The Summary section indicates the improvements that are required at each phase of development, and the allocation of improvement costs to each subarea.

Traffic Circulation System

Traffic circulation issues raised by the plan fall into two categories: (1) the internal circulation system required to accommodate Plan Area traffic; and (2) the relationship of plan-generated traffic to required traffic circulation system improvements on surrounding streets and roads. The internal traffic circulation system for the *Demaree/Caldwell Specific Plan* area has been established on the basis of the type and intensity of land uses prescribed by the specific plan. The plan calls for the predominant majority of traffic circulation improvements internal to the Plan Area to be on-site, rather than on public streets, with necessary reciprocal access easements to be required by the City during site plan reviewing.

Subareas A, B, C and D will be served by an internal traffic flow system providing access to planned uses and appurtenant parking as shown on Figure 7. The distribution of drive approaches in the Plan Area is as follows:

- Subarea A*
 - a. At full development, a minimum of two additional drive approaches located along Caldwell Avenue, one with left turn access.
 - b. At full development, a minimum of two additional drive approaches located along Demaree Road, one with left turn access.
 - c. Two drive approaches located along Chinowth Road extension (one approach for freight deliveries)
- Subarea B*
 - a. A minimum of one drive approach located along Caldwell Avenue (with left turn access)
 - b. A minimum of one drive approach located along Demaree Road.
- Subarea C*
 - a. Two drive approaches (right turn in and out only) along Demaree Road.
 - b. One drive approach along Packwood Drive.
- Subarea D*
 - a. One drive approach along Packwood Drive.
 - b. One drive approach located on Demaree Road (right turn in and out only).
- Subarea E*
 - a. Access to this subarea shall be from Packwood Drive.

All existing drive approaches are to be maintained until development displaces the served land uses. An objective of the plan will be to eliminate existing drive approaches as the uses they serve are incorporated into the planned development.

Planned land uses and development provided for by the *Demaree/Caldwell Specific Plan* will be served by the abutting network of existing and proposed public streets. Caldwell Avenue and Demaree Road will be widened from its present two-lane configuration to a section including four lanes, two east-bound/ two west-bound and north-bound/ southbound; a landscaped median; left turn channelization; and median breaks at Chinowth Road and Demaree Road along Caldwell Road. Furthermore, median breaks would occur along Demaree Road at two locations, Packwood Drive and the left-turn worm to Subarea A. Chinowth Road, along the westerly boundary of the Plan Area, will be constructed to ultimately include two north-bound and two south-bound travel lanes, with a median, and will provide median breaks and left turn channelization at two access points to the Plan Area. The intersection of Caldwell and Demaree Road is currently signalized and will require left turn pockets for traffic in all directions. Along the southerly boundary of the Plan Area, a westerly extension of Packwood Drive will be used to facilitate access to Subarea E. Acceleration and deceleration lanes of length specified by the City are to be provided at access points to arterials; adequate additional right-of-way shall be dedicated.

Domestic Water Supply

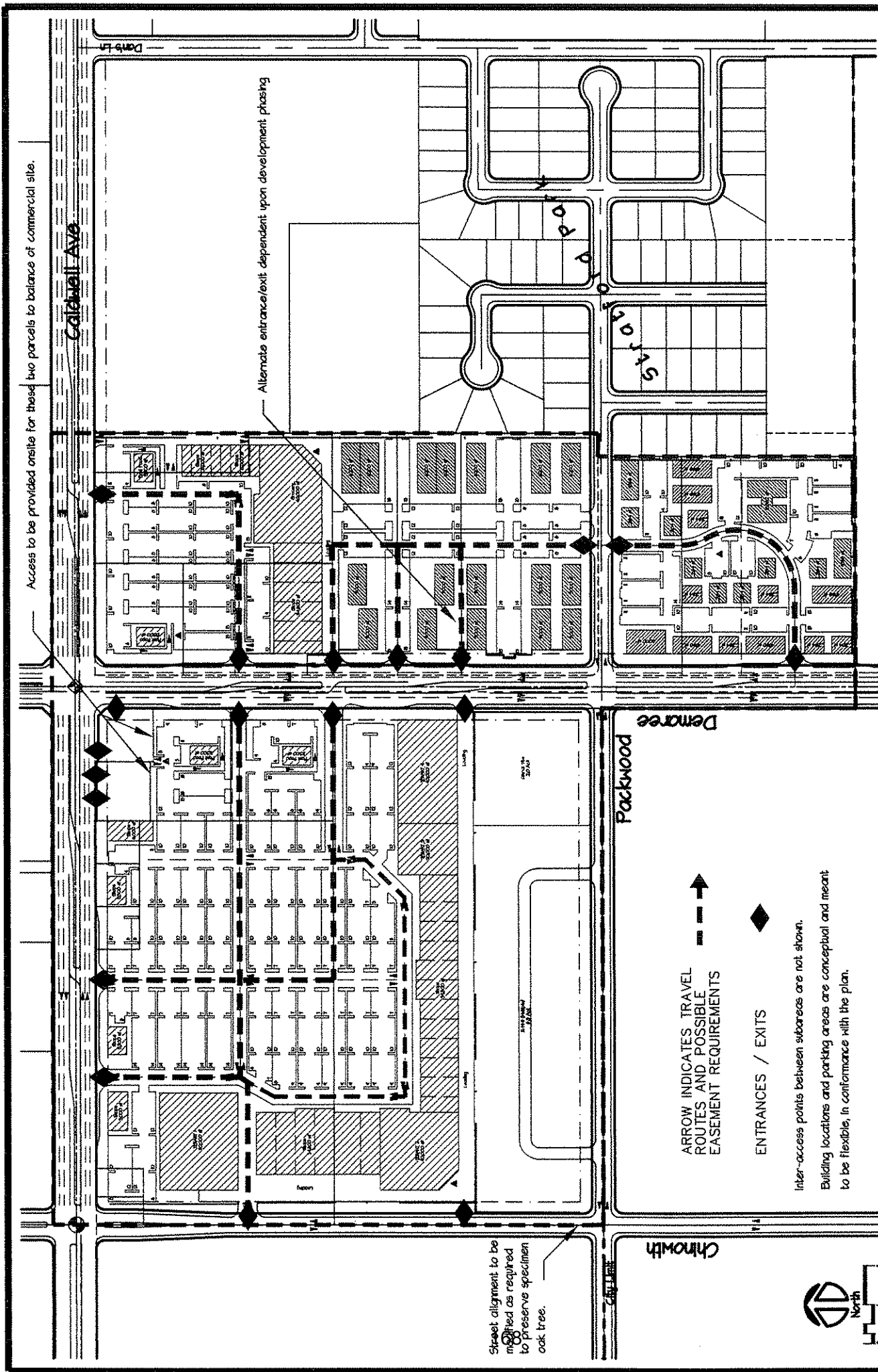
Domestic water will be supplied by the California Water Service Company. No improvements beyond normal main extensions will be required to adequately meet the domestic water and fire flow needs of development in accordance with the *Demaree/Caldwell Specific Plan*.

Wastewater Disposal

Wastewater flows generated by development of the Plan Area as prescribed by the *Demaree/Caldwell Specific Plan* will be typical of those associated with urban residential, commercial and office development. Waste discharge will be to the City of Visalia's domestic wastewater collection, treatment and disposal system, utilizing the existing twenty-four inch trunk sewer main in Caldwell Avenue. No improvements beyond normal main extensions and installations by the developer(s) of the Plan Area will be required to provide adequately for wastewater disposal.

Storm Drainage

Existing topography in the *Demaree/Caldwell Specific Plan* area is virtually flat. In general, Packwood Creek represents the discharge point for storm runoff collected from urban development located to the northeast and northwest. The City recently adopted a *Storm Drain Master Plan* which provides for continuing discharge of storm runoff to Packwood Creek from development in this vicinity, but also acknowledges that temporary detention of stormwaters may be required during periods of peak flow in the creek. It should also be noted that the current *Storm Drain Master Plan* does not include the portion of the Plan Area comprising Subareas B, C and D. The *Demaree/Caldwell Specific Plan* assumes the disposal of storm runoff from the Plan Area by discharge to Packwood Creek.



CALDWELL / DEMAREE SPECIFIC PLAN

TYPICAL ON-SITE CIRCULATION PLAN AND DRIVEWAY ACCESS

5110 N. CYPRESS AVE.
P.O. BOX 3099
VISALIA, CA 93278
TEL: (805) 733-0440
FAX: (805) 733-7821
EMAIL: quad@quadknopf.com

Quad Knopf

Parks, Open Space and Landscaping

Landscaping and open space elements of the design for Subarea E of the Plan Area are not intended to be explicitly defined by this plan. The plan provides that the residential development to take place in Subarea E be served by private streets. Consequently, public view of the area will be limited. At the same time, several key characteristics of the landscaping and open space components of Subarea E's ultimate development are determined by this plan, on the basis that they are perceived as key to the overall quality and character of the development of the Plan Area as a whole. Specifically:

- ▶ Landscaped setbacks, including public sidewalks, will be created along Demaree Road, the proposed extension of Chinowth Road bounding the project from the west, and Packwood Drive bounding the project on the south, between the edge of curb and the wall enclosing the project.
- ▶ The development of Subarea E will include internal landscaped open space, including pedestrian walkways.
- ▶ Planting materials utilized in the public and common area landscaping of Subarea E shall be thematically compatible with landscaping throughout the balance of the Plan Area.

Finally, for each subarea of the overall Plan Area, the design standards prescribed by this plan include landscaping requirements augmenting those already set out in the City's General Plan and zoning and development ordinances. Approval of any development application for the Plan Area will require City approval of an accompanying detailed landscaping plan for the proposed project.

Schools

The *Demaree/Caldwell Specific Plan* makes no overt provisions for public school facilities. The development of the Plan Area will be geared predominantly to commercial and professional office uses. Consequently, no project-related demand on local school facilities and education services is anticipated. The development of the Plan Area as provided for by this plan will be subject to the assessment of school impact fees, in accordance with adopted Visalia Unified School District and City of Visalia policy to mitigate direct and indirect school facility impacts.

Public Safety Services

The Plan Area will be served by the City's Police and Fire Departments. The City's Police Department is located near the intersection of Johnson and Willow Streets in the Downtown area, with protection provided by patrol units centrally dispatched by radio. Existing fire stations are located in the public safety complex at Johnson and Willow, and on Whitendale west of Mooney Boulevard.

Public Utilities

All new public utilities installed to serve development in the Plan Area shall be under ground, in conformance with established City standards. Undergrounding shall be the responsibility of the developer(s). Easements shall be provided by each developer as required to access and maintain undergrounded utilities.

Project Design

Within the framework of the land use designations and policies presented in the preceding sections of this chapter, the following specific design standards are prescribed for the Plan Area. These standards are intended to be applied in addition to or, where more restrictive, in place of those mandated by the City's *General Plan*, *Zoning Ordinance*, and other development regulations in effect at the time of any project proposal for the Plan Area.

Subarea A

- SA-1 The average landscaped setback from the Caldwell Avenue right-of-way shall be thirty (30) feet. Landscaped treatment of this setback shall consist of:
- ▶ Turf-covered berms, with maximum slope not to exceed that specified by City ordinance;
 - ▶ Fifteen gallon autumn gold maidenhair (*Ginko Biloba*) at the average rate of not less than one (1) tree for every fifty (50) lineal feet of frontage.
 - ▶ Fifteen gallon medium deciduous tree planting, at the average rate of not less than one (1) tree for every twenty-five (25) lineal feet of frontage. Clustering of tree planting is required. Tree species may include those permitted by the Street Tree Master Plan.
 - ▶ It is the intent of this plan to provide focal points, or landscape nodes, within this subarea, as well as providing consistency between subareas by the usage of common tree species. This is to be achieved by clustering these nodes at points of entrance in the parking area and along the building frontages

- SA-2 The average landscaped setback from the Demaree Road right-of-way shall be thirty (30) feet. Landscaped treatment of this setback shall be subject to the same standards as prescribed for the Caldwell Avenue frontage and described in standard SA-1, above.
- SA-3 Commercial structures developed in Subarea A shall, to the maximum extent feasible, provide "window" openings to the Caldwell Avenue corridor. It is recognized that such structures' primary orientation will be toward Caldwell Avenue.
- SA-4 Commercial structures developed in Subarea A shall be limited to a single story or floor.
- SA-5 Signage for commercial uses established in Subarea A shall be in accord with the applicable provisions of the City's sign ordinance (Chapter 48, Section 48.080). A sign program shall be established at the first phase of development. Monument signs at the major access points, on Demaree and on Caldwell, shall be approved as part of the program.
- SA-6 The architectural style shall not be specifically limited. However, roof treatment and materials shall be consistent for all connected buildings within each subarea.
- SA-7 Lighting fixtures shall be oriented to produce minimal lighting impacts to adjacent properties.
- SA-8 Development standards for Design District A shall apply unless otherwise specified.

Subarea B

- SB-1 The following design standards described for Subarea A, above, are applicable to all development in Subarea B of the overall Plan Area: SA-2, SA-6 and SA-7.
- SB-2 The average landscaped setback from the edge of right-of-way along Caldwell Avenue bounding the north edge of Subarea B shall be thirty (30) feet. The average landscape setback from the edge of right-of-way along Demaree Road bounding the west edge of Subarea B shall be thirty (30) feet. Landscaped treatment of these setbacks shall be subject to the same standards as prescribed for the Caldwell Avenue frontage of Subarea A and described in standard SA-1, above.
- SB-3 Commercial structures and uses established in Subarea B may be oriented toward Caldwell Avenue or internally to the Plan Area. The location of loading facilities

on the opposite side of any building from the nearest residential use is encouraged, when feasible.

- SB-4 Signage for commercial uses established in Subarea B shall be in accord with the applicable provisions of the City's sign ordinance. Dedication and development of right-of-way along the subarea's frontages of Caldwell Avenue, Demaree and Chinowth Roads will be required in conjunction with development of this subarea to enable development and/or improvement of these streets to adequate standards as prescribed in the *Circulation Element* of the City's *General Plan*. Access points shall be generally consistent with those shown on Figure 7.
- SB-5 Development standards for Design District A shall apply unless otherwise specified.

Subarea C

- SC-1 The following design standards described for Subarea A, above, are applicable to all development in Subarea C of the overall Plan Area: SA-2, SA-6 and SA-7.
- SC-2 The average landscaped setback from the edge of right-of-way along Demaree Road bounding the east edge of Subarea C shall be thirty (30) feet. Landscaped treatment of this setback shall be subject to the same standards as prescribed for the Caldwell Avenue frontage of Subarea A and described in standard SA-1, above.
- SC-3 Signage for commercial uses established in Subarea C shall be in accord with the applicable provisions of the City's sign ordinance.
- SC-4 Development standards for Design District F shall apply unless otherwise specified.

Subarea D

- SD-1 The following design standards described for Subarea A, above, are applicable to all development in Subarea C of the overall Plan Area: SA-2, SA-6 and SA-7.
- SD-2 The average landscaped setback from the edge of right-of-way along Demaree Road bounding the east edge of Subarea D shall be thirty (30) feet. The landscaped setback for right-of-way along Packwood Drive bounding the southern edge of Subarea D shall be twenty-five (25) feet. Landscaped treatment of this setback shall be subject to the same standards as prescribed for the Caldwell Avenue frontage and described in standard SA-1, above.
- SD-3 Signage for commercial uses established in Subarea D shall be in accordance with the applicable provisions of the City's sign ordinance.

SD-4 Development standards for Design District F shall apply unless otherwise specified.

Subarea E

SE-1 The following design standards described for Subarea A, above, are applicable to all development in Subarea C of the overall Plan Area: SA-2, SA-6 and SA-7.

SE-2 The landscaped setback from the edge of right-of-way along Chinowth Road and Packwood Drive shall be twenty-five (25) feet. Landscape setback for right-of-way along Demaree Road bounding the east edge of Subarea E shall be twenty-five (25) feet. Landscaped treatment of these setbacks shall be subject to the same standards as prescribed for the Caldwell Avenue frontage and described in standard SA-1, above.

PLAN IMPLEMENTATION AND PHASING

4

PLAN IMPLEMENTATION AND PHASING

Implementation of the *Demaree/Caldwell Specific Plan* will involve a variety of initiatives from both the private and public sectors. Private developers and property owners will bear the predominant responsibility for carrying out the substance of the plan. The plan addresses the quality and character of development that will take place in the Plan Area as a result of private actions. At the same time, there are public projects (e.g., Caldwell Avenue upgrading, the construction of the proposed Chinowth Road and Packwood Drive streets, storm drainage system improvements, etc.) which will be at least partially dependent upon public sector activities.

In the following sections of this chapter, plan implementation activities and procedures are described.

Phasing of Development and Improvements

In general, the phasing of development in the Plan Area will be left to the discretion of the private sector. For major components of the plan, however, a sequence of phasing is described, based upon the dependent relationships among these plan elements.

Circulation System Improvements

Right-of-way dedication and widening of Caldwell Avenue will be required to accommodate commercial development. The costs associated with these improvements will be borne by the developers, through the payment of transportation impact fees, the payment of proportional costs for essential off-site traffic facilities not covered by transportation impact fees, and installation of on-site and site-adjacent improvements.

The calculation of transportation impact fees for the Specific Plan area must take into account the fact that the ITE-derived "driveway volumes" in Table 3-2 are higher than the incremental traffic generated by Specific Plan development. These volumes must be reduced for the mixed use development of the Plan area and for the passby trips inherent in community commercial land use development. Chapters VII and VIII of the Fifth Edition of 'Trip Generation' by ITE provide bases for these two reductions. They have, for this project, been calculated as follows:

TABLE 4-1

Traffic Passby, Mixed Use, Calculation Results

Land Use	Base Units	Driveway Volume (ADT) (1) Rates per Unit	Passby Percentage (2) Reduction	Resulting Adjusted ADT Rate	Mixed Use Deduction (3)	Incremental Traffic Rate ADT
Community Commercial	406,300 s.f.	41.8	27.87%	30.15	0%	30.15
Professional Office	181,200 s.f.	12.2	0%	12.2	28.0%	8.78
Multi-Family	147 units	6.2	0%	6.2	28.0%	4.46

Sources:

- (1) Gross ADT Rate: Pages 1234, 952 and 311 of Trip Generation; formula used.
- (2) Passby trips: Page I-30 of Trip Generation, formula
- (3) Mixed Use Deduction: Table VIII-7, Page I-49 regarding capture at non-CBD sites; City-agreed average rate percent reduction applied to ADT rate.

The results of these calculations are summarized as follows:

TABLE 4-2

Incremental Traffic, ADT

	Driveway Volume	Passby Reduction	Mixed Use Reduction	Incremental Traffic	Percent of Total Incremental Traffic
Community Commercial					
Subarea A	13,062	3,640	0	9,422	65.0%
Subarea B	3,920	1,092	0	2,828	19.5%
Garden Office					
Subarea C	1,191	0	383	858	5.9%
Subarea D	1,018	0	285	733	5.1%
Multiple-Family					
Subarea E	911	0	255	656	4.5%
Total	20,102	4,732	873	14,497	100.00%

Transportation Impact Fees

The resulting transportation impact fees for each subarea's development are (at \$91.90 per trip):

Subarea A:	$\$91.90 \times 9,422 = \$865,881$
Subarea B:	$\$91.90 \times 2,828 = \$259,893$
Subarea C:	$\$91.90 \times 856 = \$ 78,850$
Subarea D:	$\$91.90 \times 733 = \$ 67,363$
Subarea E:	$\$91.90 \times 656 = \$ 60,286$

*Current fees (1998); subject to change

Demaree/Caldwell Specific Plan
Quad Knopf, Inc.

38
February, 1999

The mixed-use deductions shown in tables 4-1 and 4-2 are based on the project being designed to encourage pedestrian and/or vehicle inter access between the various land uses. If the design does not encourage pedestrian and/or vehicle inter access the mixed-use reductions can be disallowed and the transportation impact fees adjusted accordingly.

If only a portion of a subarea is developed, the transportation impact fee should be based on the percentage of GLA (gross leasable area) allotted to that subarea or, in the case of Subarea E, the percentage of the planned 147 units allotted to the subarea. (Allotments based on relative percentages of traffic generated are infeasible in the Community Commercial subarea and unnecessary in the other three subareas.)

These transportation impact fees will be reducible, in agreement with the City, for each subarea by the costs of eligible improvements required at the time of development. Eligible improvements are those listed in the City's Circulation Element; they do not include, for example, signal installations or relocations or improvements in the outside twenty feet of right-of-way of eligible arterials or collectors.

Offsite Improvements Not Financed by Transportation Impact Fees

Such improvements (signal relocation at Demaree/Caldwell, signals at Packwood/Demaree and Chinowth/Caldwell, for example) should be proportionately charged to each subarea as it develops or partially develops.

Such charges should be based upon the Incremental Traffic figures for each subarea (not the driveway volumes). The total cost of project-affected offsite improvements will in each instance be estimated by the City and the project's share of such cost estimated by proportioning the incremental traffic against the total traffic projected to be served by the facility.

Subarea – Adjacent Circulation System Improvements

Development in each subarea will be responsible, under City direction, for subarea-adjacent land dedication or non-eligible street improvements, or for non-eligible improvements in existing near-project right-of-way deemed essential by the City for project development. Conceptually, the developer will pay for the street improvements of collector street cross-section and width, with the balance of the width and cross-section for arterials to be paid for by the City.

Water, Sewer and Storm Drainage Improvements

Improvements for water, wastewater and storm drainage service to the Plan Area will be made by each developer of the area as specific projects are undertaken. Such improvements will be delineated and directed by the City at the time of site plan review.

Project Phasing and Development

Subarea A

It is anticipated that Subarea A will develop as one contiguous development. However, the northerly twenty acres of development may develop as an initial phase, with subsequent buildout of the southerly ten acres as outlined in the plan. Development of the northerly twenty acres only as the initial phase will require full improvement of the abutting streets, recordation of cross access easements for onsite vehicle circulation for access to Caldwell and Demaree, and development of signage, architectural, and landscaping standards for the entire Subarea.

Subarea B

Subarea B will develop as one unit. If Subarea B develops in advance of Subarea C, loading and unloading facilities shall be designed in a manner to protect the existing residences in Subarea C.

Subarea C

Subarea C may develop incrementally by individual property owners. A common access point to Demaree shall be installed in a manner that provides adequate ingress and egress.

A joint access agreement shall be executed between the respective properties. A joint access agreement shall also be developed between the three properties comprising Subarea C to ensure cross access to Packwood Drive.

Subarea D

Subarea D may be developed incrementally by individual property owners. Common access points to Packwood and Demaree shall be installed in a manner that provides adequate ingress and egress. A joint access agreement shall be executed between the respective properties.

Subarea E

Subarea E, the multifamily residential Subarea, may develop incrementally. Phasing shall be consistent with conditional use permits approved for the project.

Procedures for Project Approval, Plan Amendments and Modifications

This specific plan is intended to entitle the general configuration and intensity of development for each subarea. The actual configuration of development may vary from that indicated in the diagrams included in this specific plan to accommodate specific development projects and to reflect market conditions and factors in effect at the time that development is proposed for any individual subarea. Consequently, the provisions of this specific plan

are intended to be applied in a flexible manner, with precise development plans approved through the City's Site Plan Review Process, minor amendments approved by the Planning Commission, or major amendments approved by the City Council upon recommendation of the Planning Commission.

Subarea Master Plan Approval

Architecture, landscape, signage and the specific configuration of development for each Subarea shall be established by the Site Plan Review Committee to ensure consistency with this specific plan and City development regulations. The Site Plan Review Committee is authorized to approved variations in the configuration of site plans including the size of planned buildings or their building footprints, and distribution of parking. The Committee is likewise authorized to approve, upon request, corner identification signs similar to those currently approve for the northwest and northeast corners.

Minor Amendments

Minor amendments may be made to the Specific Plan by the Planning Commission as a public hearing agenda item in accordance with the City Council's delegation authority in Section 7192 *et. seq.* of the *Visalia Ordinance Code*. Minor amendments include:

1. A ten percent increase in the intensity of development as measured by planned square footage, subject to the development intensities being consistent with the *Zoning Ordinance*.
2. Modification of the landscaping palette for the specific plan.
3. Increase in the maximum percentage of total building area for an individual building or use limitations specified in the design regulations, subject to the finding that the increase will further the goals and objectives of the specific plan and the *General Plan*.

Major Amendments

Major amendments shall be approved in the same manner as adoption of the specific plan prescribed in Chapter 12.04 of the *Visalia Municipal Code*. Major amendments shall include any of the following:

1. Modification of design standards which will increase the impact of the specific plan on adjacent residential development, including standards for landscaping, lighting and traffic ingress and egress.
2. An aggregate increase in development intensity greater than 10 percent of that provided for in the specific plan.

3. An increase in the number of vehicular access points to Demaree or Caldwell.
4. All other plan amendments which are not Minor Amendments.

Relationship to City General Plan and Zoning

In accordance with State Planning Law, the *Demaree/Caldwell Specific Plan* is intended to conform to and be consistent with the *General Plan* of the City of Visalia. The broad land use designations and development policies and the circulation system prescribed by the *General Plan* shall be applicable to the *Demaree/Caldwell Specific Plan* area.

The *Zoning Ordinance* of the City prescribes permitted land uses and development standards throughout the community. Zone districts established by the *Zoning Ordinance* shall apply to all property located within the *Demaree/Caldwell Specific Plan* area. Applicable zone districts for each subarea of the overall Plan Area have been adopted as follows:

- ▶ Subarea A - C-CM and Design District A
- ▶ Subarea B - C-CM and Design District A
- ▶ Subarea C - OFFICE-GARDEN and Design District F
- ▶ Subarea D - OFFICE-GARDEN and Design District F
- ▶ Subarea E - R-M-3

The City's *Zoning Ordinance* establishes design and development standards for each of the zone districts listed above. However, this specific plan prescribes additional standards, and in any instance where the requirements of the specific plan are more restrictive than the standards mandated by the underlying zoning, the specific plan shall prevail and its standards shall apply.

Appendix B

CUP for Longs Drug Store

Conditional Use Permit No. 2006-32

USER

DATE

DATE RETURNED

Project I.D.: Conditional Use Permit No. 2006-32

Related Projects: SPA 2006-02

APN: 126-030-033/034/035/036

Site Plan No.: 06-002

Description: Conditional Use Permit No. 2006-32: is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq.ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Subarea B of the Demaree/Caldwell Specific Plan area. The site is located on the southeast corner of Caldwell Avenue and Demaree Street.
APNs: 126-030-033/034/035/036.

Logged on Project Chart: 06/14/06

NO * 2006-62

APPLICATION MATERIALS

APPLICATION CHECKLIST
CITY OF VISALIA

Conditional Use Permit No. 2006-32: is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq.ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Subarea B of the Demaree/Caldwell Specific Plan area.

The site is located on the southeast corner of Caldwell Avenue and Demaree Street.

APNs: 126-030-033/034/035/036.

Variance/Exception No. 2006-____ : request by _____ to allow a variance/exception from the standard _____ in the _____ zone. The site is located at/in

_____ APN(s): _____.

Change of Zone No. 2006-____ : is a request by

_____ to change the zoning from

_____ to _____ on _____ acres. The site is located _____

APN: _____.

Specific Plan Amendment No. 2006-02: A request by The Taylor Group to amend the Demaree/Caldwell Specific Plan to allow second access points from Caldwell Avenue and from Demaree Steet, and to allow a phased development of Sub-area B. The site is located on the southeast corner of Caldwell Avenue and Demaree Street. APNs: 126-030-033/034/035/036.

SUBMITTED**No** **Yes**

Application Form
Agency Authorization
Hazardous Materials Form
Environmental Check List
SPR Revise & Proceed Comments

x Operational statement

NEED**No** **Yes**

Light Study
Traffic Study
Noise Study
Visibility Screens
Landscape Plans
Sign Program
x Electronic Version of Exhibits
☐ Sign Program

Outside Agency Review:

Park and Recreation
Caltrans
Tulare County
Airport Commission

NEED
No **Yes**

x Additional Site Plan Review
Requirements
Site Plan (w/ Reductions)
Elevations (w/ Reductions)
Floor Plans (w/ Reductions)
Master Plans (w/ Reductions)
Operational Statement
Hazardous Materials List

Redevelopment Agency

Fish and Game
Historic Committee
Police Department
LAFCO
Other

Name of Person Completing Checklist: Paul Scheibel Date: 5-28-06

____ Cat-x X Mit Neg Dec ____ Neg Dec ____ EIR
____ x Not Complete ____ Complete x \$45 Check

Completeness Comments:

CUP Application-

Please provide an operational statement that clearly describes the phasing and development plan of the project, including access and parking calculations by phase, and site amenities proposed by phase. It is unclear how the present property lines correspond to the proposed development plan. It appears a parcel map to eliminate the current PLs in favor of the development plan would be essential for the project.

Specific Plan Amendment:

Please provide a justification along with any available technical analyses to justify the request for an additional access points on Caldwell Ave., and Demaree St. With regard to phasing, the explanation should be similar to the contents of the operational statement noted above, with a justification why phasing the project will not adversely impact ultimate buildout of the site in accordance with the Specific Plan.

CUP Application-

The site plan has too much extraneous information unrelated to the CUP application. Please prepare a revised site plan in accordance with the site plan checklist portion of the CUP application (attached). For example Note 20 references "section lines", which apparently do not relate to either phases or parcels.

Provide a building coverage, parking and landscaping breakdown by proposed phase.

Cross section details of both Demaree and Caldwell are required.

Identify your proposed method of providing access to Subarea C (easement reference in a margin note is acceptable).

The cmu wall (note 27) does not show up on the south building elevation.

A preliminary landscape plan is required [please see landscape plan checklist, (enclosed)]. In addition, all parking and drive-thrus must be screened by minimum three-foot high landscape hedges or similar features.

The bale storage shown on the site plan does not appear to be depicted on the elevations.

Design Comments:

Staff cannot support the southern access on Demaree St. because it will provide access to an essentially undeveloped majority of the site.

The pharmacy building should be moved closer to the street corner and provide a maximum of landscaping and pedestrian amenities around the building.

The east and south elevations and adjacent areas have too much competing traffic (access, parking, drive-thru, loading to be practical for this portion of the site. These comments were provided at Site Plan and were not addressed with this formal application.

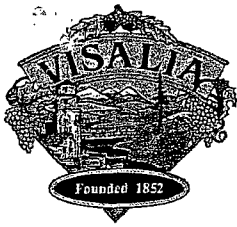
Staff recommends a different roof material than standing seam roofs. Structures of this height and mass will appear to be shorted by such a minimalist roof material and design.

Trash enclosures that function as entry features to the site cannot be supported.

Property line walls in excess of seven feet cannot be supported as sound attenuation measures. Staff will recommend these walls not exceed seven feet in height, and shall be architecturally integral with the building designs for the balance of the development.

The drive-thru pads are excessive to any known fast food outlet. It is strongly recommended that retail spaces be brought closer to and parallel with the streets. The drive-thrus force a pad layout that conflicts with good pedestrian access and street appeal.

Planner: Andrew Chamberlain, AICP Unset X PC Date: unset CC Date:



CITY OF VISALIA
PLANNING DIVISION PERMIT
APPLICATION

DATE STAMP

PERMIT APPLICATION(S):

Check all permits being applied for with this application.

- | | |
|--|---|
| <input checked="" type="checkbox"/> CONDITIONAL USE PERMIT | <input type="checkbox"/> LOT LINE ADJUSTMENT |
| <input type="checkbox"/> AMENDMENT TO EXISTING CUP | <input type="checkbox"/> TENTATIVE PARCEL MAP |
| <input type="checkbox"/> ZONING VARIANCE/EXCEPTION | <input type="checkbox"/> TENTATIVE SUBDIVISION MAP |
| <input type="checkbox"/> NOISE VARIANCE | <input type="checkbox"/> GENERAL PLAN AMENDMENT |
| <input type="checkbox"/> CHANGE OF ZONE | <input checked="" type="checkbox"/> SPECIFIC PLAN AMENDMENT |
| <input type="checkbox"/> ANNEXATION | |

****Staff Use Only****
Project Number(s) _____

Planning Commission
Date: _____

Short title or name of proposed project:

Summary description of the proposed project: Proposed commercial retail shopping
center comprising seven buildings, including a 15,800 s.f. Longs
Drug store, a 32,900 s.f. major, shops, and pads. Total
building area is 81,037 sqft. Drive-thrus are proposed for
three of the buildings.

SITE:

Site Plan Review number(s) 06-002

Date of SPR Committee revise & precede authorization 4/5/06

Address or nearest major street intersection Caldwell Ave. & Demaree Rd. SE corner

APN(s) 126 - 030 - 33 thru 36

Existing Zone CCM Existing General Plan Land Use Designation CCM - Community Commercial

Proposed Zoning Designation same

Proposed Land Use Designation same

Site area (acres, or square feet if less than one acre) 8.5 acres

Existing streets directly adjacent to the site Caldwell Ave. & Demaree Rd.

Existing use(s) Agricultural, vacant, one SFR

Existing improvements/structures one residence at northeast corner of property

PROPERTY OWNER(S):

If more than two owners, please provide information and signature(s) on a separate sheet.

Name (print) Harold Clements Name (print) _____

Clements Development Ltd.

Mailing Address 891 S. McAuliff Mailing Address _____

Visalia, Ca. 93292

Phone 627-0537 Phone _____

Statement: I/We declare under penalty of perjury that I am/we are the legal owner(s) of the property involved in this application. I/We authorize the person named in this application as the Project Main Contact to act as my/our representative with City Staff regarding the processing of this application.

5-16-06
Date


Property Owner Signature

Date

Property Owner Signature

PROJECT MAIN CONTACT/REPRESENTATIVE

(This is the person who will be the main contact with City Staff, and will receive all correspondence.)

Name (print) Russel F. Taylor


Firm/Company Taylor Group Architects

Mailing Address 978 W. Alluvial Ave. #107

Phone 437-5000 Fax 437-5005 E-Mail rtaylor@ttqarchitects.com

Statement: I will be the main contact and representative of the proposed project with City Staff during the processing of this application. I declare under penalty of perjury that all statements and documents submitted with this application are true and correct to the best of my knowledge.

Date


Project Main Contact/Representative Signature

OTHER INVOLVED PARTIES:

Fill in all that apply.

Is the property currently in escrow? If so, to whom? _____

(Write "none" if property is not in escrow.)

Developer/Builder _____

Mailing Address _____

Phone _____ Fax _____

Contractor _____

Engineer _____

Architect _____

NAMES OF PRINCIPALS, PARTNERS, AND/OR TRUSTEES:

List the names of any and all principals, partners, and/or trustees where any property owner or developer/builder is a corporation, partnership, or trust. For corporations provide names of officers and directors. For trusts provide names of trustees and beneficiaries.

_____	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____



CITY OF VISALIA
PLANNING DEPARTMENT
CONDITIONAL USE PERMIT (CUP)
SUPPLEMENTAL APPLICATION

PROJECT DESCRIPTION

Describe Project And Listing Of All Components of the CUP (i.e. drive-through, private roads, modified residential standards PUD or PRD, etc.): Proposed commercial retail shopping center comprising seven buildings, including a 15,800 s.f. Longs Drug, a 32,900 s.f. major, shops, and pads. Three drive thru's are proposed at Longs and two freestanding pads.

PROJECT DETAILS

(You may be required to provide a separate Operational Statement including the information below)

Gross Acreage _____ Net Acreage 8.5 acres
Building Area 81,037 s.f. No. of Parking Stalls 364
Days of Operation 7 days Hours of Operation _____
No. of Employees _____ Max Employees per Shift _____
List All Outdoor Activities Possible limited outdoor seating at pad; loading/unloading at major
No. of Outdoor Vendors _____ Per Week _____ Per Day _____
No. of Vehicles Used _____
No. of Vehicles Kept Onsite Overnight None typical, except occasional temporary
If Residential: Number of Single-Family Units _____ Number of Multi-Family Units _____
Other Components _____
Gross Density _____ Net Density _____
Open Space _____ Acreage _____
Recreation Facilities: Yes / No Specify _____
Modified Setbacks or Standards Requested: Yes / No Specify _____

PROVIDE THE FOLLOWING FOR ALL CUP PROJECTS

(Person Preparing Application Shall Place Initials On The Space Provided. Required Application Materials Not Included Will Result In Application Being Deemed INCOMPLETE.)

- _____ 34 Copies of Site Plan (See Supplemental Information required For Site Plans)
- _____ 1 Copy of Reduced Size Site Plan (8 1/2in. X 11in.)
- _____ 10 Copies of Conceptual Building Elevations (See Supplemental Information Required For Building Elevations)
- _____ 1 Copy of Reduced Size Conceptual Building Elevations (8 1/2in. X 11in.)
- _____ 10 Copies of Floor Plans
- _____ 1 Copy of Reduced Size Floor Plans (8 1/2in. X 11in.)
- _____ 10 Copies of Preliminary Landscape Plans (Including Fencing and Wall Details)
- _____ 1 Copy of Reduced Size Preliminary Landscape Plans (8 1/2in. X 11in.)
- _____ 2 Copies of Site Plan Review Committee Revise & Proceed Comments
- _____ 1 Copy of Signage and/or Sign Program
- _____ 2 Copies of Noise Study (If required by Site Plan Review Committee)
- _____ 2 Copies of Traffic Impact Study (If required by Site Plan Review Committee)
- _____ 1 Copy Of Trash Enclosure Details
- _____ Electronic Files of all Exhibits (Adobe Acrobat, or similar format) Provided on CD Disk

VERSION 1-9-06 SUPERCEDES ALL PREVIOUS VERSIONS

Page 1 of 1



CITY OF VISALIA
PLANNING DEPARTMENT
SPA SUPPLEMENTAL APPLICATION

Is the area located inside a Specific Plan, Master Plan or Implementation Plan? If so, which Plan? Caldwell/Demaree Specific Plan

Explain how the proposed project consistent with policies, criteria and land use designations inside the existing Specific Plan, Master Plan or Implementation Plan? Proposed commercial retail center is consistent with the specific plan in all aspects except as described below

List all proposed changes to any policies, criteria and/or land use designation: Additional right in/out approach is proposed along Caldwell Ave.

CITY OF VISALIA
COMMUNITY DEVELOPMENT DEPARTMENT'S RECEIPT

PAGE NO. ____ OF ____

BUILDING DIVISION

0011-431102-18241		Building Permit Fee
0011-481620-18241		Misc. Revenue
		Other:
		Other:

PLANNING DIVISION

0011-461521-18111		Administrative Adjustment
0011-461517-18111		Annexation
0011-250115		Annexation Holding Account for Board of Equalization
0011-250110		Annexation Holding Account for LAFCO
0011-461522-18111		Certificate of Compliance
0011-461512-18111		Change of Zone
0011-461524-18111	<u>3,123-</u>	Conditional Use Permit Amendment ✓ #1769496
0011-461513-18111		Conditional Use Permit
0011-461529-18111		Development Agreement
0011-461515-18111		Draft Environmental Impact Report
0011-461516-18111	<u>524-</u>	Environmental ✓ #1769494
0011-461525-18111		Flood Elevation Certificate Copies
0011-461520-18111		General Plan Amendment
0011-461514-18111		Home Occupation Permit
0011-461518-18111		Lot Line Adjustment
0011-471621-18111		Maps and Publications
0011-461527-18111	<u>3,960-</u>	Specific Plans - Residential / Commercial ✓ #1769492
0011-461510-18111		Temporary Conditional Use Permit
0011-461548-18111		Tentative Parcel Map
0011-461534-18111		Tentative Subdivision Map
0011-461528-18111		Text Amendments
0011-461526-18111		Time Extension
0011-461511-18111		Variance/Exception
0011-481620-18111		Misc. Revenue
		Other:
		Other:

TOTAL **7602-**

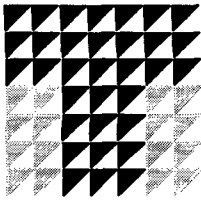
Planning use only:
Tulare County Recording Fee
Check Received _____
Amount \$ _____
Check No. _____

**no
45⁰⁰
check**

0011-250102- _____ DEPOSITS PAYABLE-MONEY TO BE REFUNDED
0011-211102- _____ REVENUE COLLECTED IN ADVANCE - MONEY TO BE USED FOR FUTURE CONSTRUCTION.

PROJECT: **Lougg Drugs CUP/SPA**
PAID BY: **Lougg Drugs Stores** CASH: _____ CHECK NO. **See Above**
RECEIVED BY: **Andy C.** DATE: **5-18-06**

Finance use only:
Batch # _____
Post Date _____
Receipt # _____



LETTER OF TRANSMITTAL

978 WEST ALLUVIAL AVE.

SUITE 107

FRESNO, CA 93711

TEL 559.437.5000

FAX 559.437.5005

TO: City of Visalia, Community
Community Development Planning Division
315 E. Acequia Avenue
Visalia, CA 93291

DATE: 17-May-06

ARCHITECTS
PROJECT NO: 20050801

ATTN: Andy Chamberland; 559-713-4003

REGARDING: Long's Drugs CUP

TRANSMITTED VIA:

☐ COURIER ☐ MAIL ☐ BLUEPRINTER ☐ FAX ☒ CALIF.OVERNIGHT ☐ FEDEX ☐ PICK-UP

INCLUDED HEREWITH:

☐ COPY OF LETTER ☐ DRAWINGS ☐ TRACINGS / SPEIAS ☐ SHOP DRAWINGS - PRINTS ☐ SHOP DRAWINGS - REPRODUCIBLES ☐ SPECIFICATIONS
☒ SUBMITTALS ☐ SAMPLES ☐ CHANGE ORDER ☐ ADDENDUM ☐ PRODUCT LITERATURE ☐ PAYMENT REQUEST APPLICATION
☐ CALCULATIONS ☐ TITLE 24 DOCS ☐

NO. OF COPIES	DATE	DRAWING OR ITEM NO.	DESCRIPTION / REMARKS	ACTION CODE
1	5/17/06		Conditional Use Permit Application	
1	5/11/06		Check Numbers 1769492, 1769494 & 1769496	
1	5/17/06		Reduced Sheets SPR1, SPR2, Colored Elevations, SPR3 & Floor Plan & Elect. Exhibits	
2	4/5/06		Site Plan Review Revise and Proceed	
34	5/17/06		SPR1 Site Plan	
10	5/17/06		SPR2 Landscape Plan	
10	5/17/06		SPR3 Elevations	
10	5/17/06		Floor Plan	
IF ENCLOSURES ARE NOT AS NOTED, PLEASE INFORM US IMMEDIATELY				

THESE ITEMS ARE TRANSMITTED AS FOLLOWS:

☒ FOR APPROVAL ☐ FOR YOUR FILES/RECORDS
☒ FOR YOUR USE ☐ FOR YOUR INFORMATION
☐ AS REQUESTED ☐ FOR YOUR DISTRIBUTION
☐ FOR YOUR REVIEW & COMMENT ☐ DRAWINGS RETURNED AFTER LOANED TO US
☐ RESUBMIT COPIES FOR REVIEW ☐ RETURN CORRECTED DRAWINGS
☐ SUBMIT COPIES FOR DISTRIBUTION ☐ OTHER:

ACTION CODE:

A. REVIEWED
B. REVISE & RESUBMIT
C. REJECTED
D. FURNISH AS CORRECTED
E. NOT REVIEWED
F. DO NOT RESUBMIT
G. RESUBMIT FOR FILES
H. ACTION INDICATED ON ITEM TRANSMITTED
I. FOR SIGNATURE & RETURN TO THIS OFFICE
J. FOR SIGNATURE & FORWARDING AS NOTED UNDER REMARKS
K. SEE REMARKS
L. NO ACTION REQUIRED

COPIES TO:

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☐ WITH ENCLOSURES
☐ WITH ENCLOSURES
☐ WITH ENCLOSURES

PROJECT FILE NO:

IF CHECKED BELOW, PLEASE:

☒ ACKNOWLEDGE RECEIPT OF ENCLOSURES
☐ RETURN ENCLOSURES TO US

VERY TRULY YOURS,

Aaron MacDonald, Architect
The Taylor Group Architects of CA, Inc.

$$\begin{array}{r}
 1 \\
 3123 \\
 3960 \\
 524 \\
 \hline
 7607
 \end{array}$$



CITY OF VISALIA
PLANNING DEPARTMENT
ENVIRONMENTAL CONDITIONS
REQUIRED FOR ALL PROJECTS

SITE CHARACTERISTICS:

Flood Zone Designation: _____ Height Of Required Minimum Building Elevation: _____

Is The Project Site Within A:

Historic District: Yes ☒ No

Specific Plan Or Master Plan Area: ☒ Yes (If Yes, Name) Caldwell/Demaree Specific Plan

Special Study Area: Yes ☒ No (If Yes, Name) _____

Agricultural Preserve: Yes ☒ No

Williamson Act Contract: Yes ☒ No If Yes, Preserve # _____ Contract # _____

Has A Notice Of Non-Renewal Been Filed? Yes ☒ No Date Filed: _____

Please Check All Items Applicable To The Project:

_____ Mature Oak Trees On Site Or Within Forty Feet Of The Site

_____ Within Protected Species Or Habitat Area

_____ Evidence of Hazardous Waste Or Previous Hazardous Uses Or Processes Occurring On Site

_____ Waterways Adjacent To The Project Site, And/Or Any Planned Changes In Streams, Waterways, Rivers, Ditches

_____ Known Cultural Resources On Site

_____ Within ¼ Mile Of Any School

_____ Increase In Light Or Glare To Immediate Vicinity After Project Is Completed

_____ Increase In Noise To Immediate Vicinity After Project Is Completed

_____ Within Two Miles Of An Airport

AGENCY AUTHORIZATION

OWNER:

Attached Letter of Agency provided to City
of Visalia previously

I, _____, declare as follows:
(Owners Name)

I am the owner of certain real property bearing assessor's parcel number (APN):

_____	_____
_____	_____
_____	_____

AGENT:

I designate _____, to act as my duly authorized
(Agent's Name) (Please type or print)

agent for all purposes necessary to file an application for, and obtain a permit to

(Action Sought)

relative to the property mentioned herein.

I declare under penalty of perjury the foregoing is true and correct.

Executed this _____ day of _____, 20_____.

<u>OWNER</u>	<u>AGENT</u>
_____ (Signature of Owner)	_____ (Signature of Agent)
_____ (Owner Mailing Address)	_____ (Agent Mailing Address)
_____	_____
_____ (Owner Telephone)	_____ (Agent Telephone)

APPROVED:
CITY OF VISALIA

By: _____
(Signature)

Date: _____

*NOTE: Attach acknowledgment of signature(s) by Notary Public if executed outside the State of California.

December 17, 2005

City of Visalia
707 W. Acequia
Visalia, CA 93291

RE: Letter of Authorization
SEC of Caldwell and Demaree, Visalia, CA

To Whom It May Concern:

This letter is to serve as my authorization to have The Taylor Group Architects act on my behalf in the development of a commercial retail shopping center at the southeast corner of Demaree and Caldwell Avenues. This includes submitting the attached SPR Application.

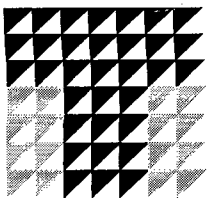
Sincerely,



Harold Clements
Clements Development Limited, a California Limited Partnership

Attachment

PLANNING CORRESPONDENCE



LETTER OF TRANSMITTAL

978 WEST ALLUVIAL AVE.
SUITE 107
FRESNO, CA 93711
TEL 559.437.5000
FAX 559.437.5005

TO: Mr. Andrew Chamberlain, Sr. Planner
City of Visalia
315 E. Acequia Ave.
Visalia, CA 93291

DATE: 28-Sep-06

ARCHITECTS
PROJECT NO: 20050801

REGARDING: Long's Drugs

ATTN:

TRANSMITTED VIA:

☐ COURIER ☐ MAIL ☐ BLUEPRINTER ☐ FAX ☒ CALIF.OVERNIGHT ☐ FEDEX ☐ PICK-UP

INCLUDED HEREWITH:

☐ COPY OF LETTER ☒ DRAWINGS ☐ TRACINGS / SPEIAS ☐ SHOP DRAWINGS - PRINTS ☐ SHOP DRAWINGS - REPRODUCIBLES ☐ SPECIFICATIONS
☐ SUBMITTALS ☐ SAMPLES ☐ CHANGE ORDER ☐ ADDENDUM ☐ PRODUCT LITERATURE ☐ PAYMENT REQUEST APPLICATION
☐ CALCULATIONS ☐ TITLE 24 DOCS ☐

NO. OF COPIES	DATE	DRAWING OR ITEM NO.	DESCRIPTION / REMARKS	ACTION CODE
1			One full-size copy of Site Plan	
2			Two 11x17 copies of Site Plan	

IF ENCLOSURES ARE NOT AS NOTED, PLEASE INFORM US IMMEDIATELY

THESE ITEMS ARE TRANSMITTED AS FOLLOWS:

☐ FOR APPROVAL ☐ FOR YOUR FILES/RECORDS
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ACTION CODE:

A. REVIEWED
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C. REJECTED
D. FURNISH AS CORRECTED
E. NOT REVIEWED
F. DO NOT RESUBMIT
G. RESUBMIT FOR FILES
H. ACTION INDICATED ON ITEM TRANSMITTED
I. FOR SIGNATURE & RETURN TO THIS OFFICE
J. FOR SIGNATURE & FORWARDING AS NOTED UNDER REMARKS
K. SEE REMARKS
L. NO ACTION REQUIRED

COPIES TO:

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☐ WITH ENCLOSURES

PROJECT FILE NO:

IF CHECKED BELOW, PLEASE:

☐ ACKNOWLEDGE RECEIPT OF ENCLOSURES
☐ RETURN ENCLOSURES TO US

VERY TRULY YOURS,

Brian Richmond, Project Architect
The Taylor Group Architects of CA, Inc.

CITY OF VISALIA
315 E. ACEQUIA AVENUE
VISALIA, CA 93291

**NOTICE OF A PROPOSED
NEGATIVE DECLARATION**

Project Title: Conditional Use Permit No. 2006-32, Specific Plan Amendment No. 2006-02

Project Description: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a drive-thru for a pharmacy. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

Specific Plan Amendment No. 2006-02 is a request to relocate an access drive on Caldwell Avenue, and to allow phased development of Sub-Area "B".

Contact Person: Andrew Chamberlain, Senior Planner. Phone: (559) 713-4003.

Time and Place of Public Hearing: A public hearing will be held before the Planning Commission on August 14, 2006, at 7:00 p.m. in the City Hall Council Chambers located at 707 W. Acequia, Visalia, CA.

Pursuant to City Ordinance No. 2388, the Environmental Coordinator of the City of Visalia has reviewed the proposed project described herein and has found that it will not result in any significant effect upon the environment because of the reasons listed below:

Reasons for Negative Declaration: Initial Study No. 2006-67 has not identified any significant, adverse environmental impacts that may occur because of the project. Copies of the initial study and other documents relating to the subject project may be examined by interested parties at the Planning Division in City Hall East, at 315 E. Acequia Ave., Visalia, CA.

Comments on this proposed Negative Declaration will be accepted until August 14, 2006.

Date: _____

7-13-06

Signed: _____

Paul Scheibel, AICP
Environmental Coordinator
City of Visalia

Y-103

NEGATIVE DECLARATION

Project Title: Conditional Use Permit No. 2006-32

Project Description: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq. ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Sub area B of the Demaree/Caldwell Specific Plan area. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

Project Facts: Refer to Initial Study for project facts, plans and policies, discussion of environmental effects and mitigation measures, and determination of significant effect.

Attachments:

Initial Study	(X)
Environmental Checklist	(X)
Maps	()
Mitigation Measures	()
Letters	()

DECLARATION OF NO SIGNIFICANT EFFECT:

This project will not have a significant effect on the environment for the following reasons:

- (a) The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory.
- (b) The project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.
- (c) The project does not have environmental effects which are individually limited but cumulatively considerable. Cumulatively considerable means that the incremental effects of an individual project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.
- (d) The environmental effects of the project will not cause substantial adverse effects on human beings, either directly or indirectly.

This Negative Declaration has been prepared by the City of Visalia Planning Division in accordance with the California Environmental Quality Act of 1970, as amended. A copy may be obtained from the City of Visalia Planning Division Staff during normal business hours.

APPROVED
Paul Scheibel, AICP
Environmental Coordinator

By: 

Date Approved: _____

Review Period: 20 days

INITIAL STUDY

I. GENERAL

A. Description of the Project: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq. ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Sub area B of the Demaree/Caldwell Specific Plan area. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

B. Identification of the Environmental Setting: The site is currently vacant. The surrounding zoning and land uses are as follows:

North: CCM (Community Commercial)
South: OG (Office Garden)
East: CCM (Community Commercial)/ future community commercial center
West: CCM (Community Commercial)

Fire and police protection services, street maintenance of public streets, refuse collection, and wastewater treatment will be provided by the City of Visalia upon development of the area.

C. Plans and Policies: The General Plan Land Use Element (LUE) designates the site as Community Commercial. The site is zoned C-CM (Community Commercial). The proposed project is consistent with the Land Use Element of the General Plan.

II. ENVIRONMENTAL IMPACTS

No significant adverse environmental impacts have been identified for this project. The City of Visalia Land Use Element and Zoning Ordinance contain land use mitigation measures that are designed to reduce/eliminate impacts to a level of non-significance.


III. MITIGATION MEASURES

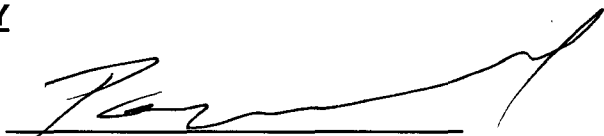
None. The City of Visalia Zoning Ordinance contains guidelines, criteria, and requirements for the mitigation of potential impacts related to light/glare, visibility screening, noise, and traffic/parking to eliminate and/or reduce potential impacts to a level of non-significance.

IV. PROJECT COMPATIBILITY WITH EXISTING ZONES AND PLANS

The project is compatible with the General Plan and Zoning Ordinance as the project relates to surrounding properties.

V. NAME OF PERSON WHO PREPARED INITIAL STUDY


Andy Chamberlain
Senior Planner


Paul Scheibel, AICP
Environmental Coordinator

**INITIAL STUDY
ENVIRONMENTAL CHECKLIST**

Name of Proposal	Conditional Use Permit No. 2006-32		
NAME OF PROPONENT:	Harold Clements	NAME OF AGENT:	Russel F. Taylor
Address of Proponent:	891 S. McAuliff	Address of Agent:	978 W. Alluvial Ave. #107
	Visalia, CA 93292		
Telephone Number:	(559)627-0537	Telephone Number:	(559)437-5000
Date of Review	June 22, 2006	Lead Agency:	Taylor Group Architects

The following checklist is used to determine if the proposed project could potentially have a significant effect on the environment. Explanations and information regarding each question follow the checklist.

1 = No Impact 2 = Less Than Significant Impact
3 = Less Than Significant Impact with Mitigation Incorporated 4 = Potentially Significant Impact

I. AESTHETICS

Would the project:

- 1 a) Have a substantial adverse effect on a scenic vista?
- 1 b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?
- 1 c) Substantially degrade the existing visual character or quality of the site and its surroundings?
- 2 d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

II. AGRICULTURAL RESOURCES

Would the project:

- 1 a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency to non-agricultural use?
- 1 b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?
- 1 c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to nonagricultural use?

III. AIR QUALITY

Would the project:

- 1 a) Conflict with or obstruct implementation of the applicable air quality plan?
- 1 b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?
- 2 c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?
- 1 d) Expose sensitive receptors to substantial pollutant concentrations?
- 1 e) Create objectionable odors affecting a substantial number of people?

IV. BIOLOGICAL RESOURCES

Would the project:

- 1 a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 1 b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 1 c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?
- 1 d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?
- 1 e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?
- 1 f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

V. CULTURAL RESOURCES

Would the project:

- 1 a) Cause a substantial adverse change in the significance of a historical resource as defined in Public Resources Code Section 15064.5?
- 1 b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Public Resources Code Section 15064.5?
- 1 c) Directly or indirectly destroy a unique paleontological resource or site, or unique geologic feature?

V. CULTURAL RESOURCES (continued)

Would the project:

- 1 d) Disturb any human remains, including those interred outside of formal cemeteries?

VI. GEOLOGY AND SOILS

Would the project:

- a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
- 1 i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?
- 1 ii) Strong seismic ground shaking?
- 1 iii) Seismic-related ground failure, including liquefaction?
- 1 iv) Landslides?
- 1 b) Result in substantial soil erosion or loss of topsoil?
- 1 c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?
- 1 d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

VII. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- 1 a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?
- 1 b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?
- 1 c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?
- 1 d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?
- 1 f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?
- 1 g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?
- 1 h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

VIII. HYDROLOGY AND WATER QUALITY

Would the project:

- 1 a) Violate any water quality standards of waste discharge requirements?

- 1 b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

- 1 c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?

- 1 d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?

- 1 e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

- 1 f) Otherwise substantially degrade water quality?

- 1 g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

- 1 h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?

- 1 i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

- 1 j) Inundation by seiche, tsunami, or mudflow?

IX. LAND USE AND PLANNING

Would the project:

- 1 a) Physically divide an established community?

- 1 b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?

- 1 c) Conflict with any applicable habitat conservation plan or natural community conservation plan?

X. MINERAL RESOURCES

Would the project:

- 1 a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

- 1 b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

XI. NOISE

Would the project:

- 2 a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

XI. NOISE (continued)

Would the project:

- 1 b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

- 2 c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?
- 2 d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?
- 1 f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

XII. POPULATION AND HOUSING

Would the project:

- 2 a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?
- 1 b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?
- 1 c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

XIII. PUBLIC SERVICES

Would the project:

- 2 a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:
 - 2 i) Fire protection?
 - 2 ii) Police protection?
 - 1 iii) Schools?
 - 1 iv) Parks?
 - 1 v) Other public facilities?

XIV. RECREATION

Would the project:

- 1 a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?
- 1 b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

XV. TRANSPORTATION / TRAFFIC

Would the project:

- 2 a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?
- 1 b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?

- 1 c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?
- 1 d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?
- 1 e) Result in inadequate emergency access?
- 1 f) Result in inadequate parking capacity?

XVI. UTILITIES AND SERVICE SYSTEMS

Would the project:

- 1 a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
- 2 b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 2 c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 1 d) Have sufficient water supplies available to service the project from existing entitlements and resources, or are new or expanded entitlements needed?
- 1 e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?
- 1 f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?
- 1 g) Comply with federal, state, and local statutes and regulations related to solid waste?

XVII. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- 1 a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?
- 1 b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?
- 1 c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly.

DISCUSSION OF ENVIRONMENTAL EVALUATION

I. AESTHETICS

- a. The project will not adversely affect the view of the Sierra Nevada mountain range, a scenic vista that can be seen from Visalia on clear days.
- b. There are no scenic resources on the site.
- c. Development of the site will change the visual character from a vacant piece of property to a commercial development. Although this loss of visual open space may be significant to some members of the community, the site has been planned for urban uses for several years. The City has development standards related to landscaping and other amenities to ensure that the visual character of the area is not degraded.
- d. The project will allow for new sources of light that is typical of urban development. The City has development standards that require that light be directed and/or shielded so it does not fall upon adjacent residential and commercial properties.

II. AGRICULTURAL RESOURCES

- a. The site is not zoned for agricultural use and will not involve the conversion of farmland. The project will not have an effect on any agricultural resources.
- b. The site is not under Williamson Act contract or not within an Agricultural Preserve.
- c. The project will not involve other changes in the existing environment which due to their location or nature, could result in conversion of Farmland to nonagricultural use.

III. AIR QUALITY

- a. The project in itself does not disrupt implementation of the San Joaquin Valley Unified Air Pollution Control District's air quality plan.
- b. The project will not violate an air quality standard or contribute substantially to an existing or projected air quality violation.
- c. The San Joaquin Valley is a region that is already at non-attainment status for air quality. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use.
- d. The project's proposed land uses generally do not generate substantial pollutant concentrations.
- e. The project's proposed land uses generally do not generate objectionable odors.

IV. BIOLOGICAL RESOURCES

- a. There are no identified candidate, sensitive, or special status species on the site.
- b. The project is not located within or adjacent to a sensitive riparian habitat or other natural community.

- c. The project is not located within or adjacent to federally protected wetlands as defined by Section 404 of the Clean Water Act.
- d. No animal movement will be affected by the proposed development.
- e. The City has a municipal ordinance in place to protect oak trees. All existing oak trees on the project site will be under the jurisdiction of this ordinance. Any Oak Trees to be removed from the site are subject to the jurisdiction of the municipal ordinance.
- f. There are no local or regional habitat conservation plans for the area.

V. CULTURAL RESOURCES

- a. There are no known historical resources located within the project area. If some potentially historical or cultural resource is unearthed during construction all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- b. There are no known archaeological resources located within the project area. If some archaeological resource is unearthed during construction all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- c. There are no known unique paleontological resources or geologic features located within the project area.
- d. There are no known human remains buried in the project vicinity. If human remains are unearthed during construction all work should cease until the proper authorities are notified and a qualified professional archaeologist can evaluate the finding and make any necessary mitigation recommendations.

VI. GEOLOGY AND SOILS

- a. The State Geologist has not issued an Alquist-Priolo Earthquake Fault Map for Tulare County. The project area is not located on or near any known earthquake fault lines. Therefore, the project will not expose people or structures to potential substantial adverse impacts involving earthquakes.
- b. The project will not result in substantial soil erosion or a loss of topsoil.
- c. The project area is relatively flat and the underlying soil is not known to be unstable. Soils in the Visalia area have few limitations with regard to development. Due to low clay content and limited topographic relief, soils in the Visalia area generally have low expansion characteristics.
- d. Due to low clay content, soils in the Visalia area have an expansion index of 0-20, which is defined as very low potential expansion.

VII. HAZARDS AND HAZARDOUS MATERIALS

- a. Hazardous materials such as gasoline and pesticides will most likely be found within the project area after the project is developed, but only in quantities typically used for residential or commercial use or application. This will not create a significant hazard.
- b. There is no reasonably foreseeable condition or incident involving the project that could result in release of hazardous materials into the environment.
- c. The project site is not located within one-quarter mile of a school. There are no reasonably foreseeable conditions or incidents involving the project that could affect this or proposed school sites or areas within one-quarter mile of school sites.
- d. The project area does not include any sites listed as hazardous materials sites pursuant to Government Code Section 65692.5.
- e. The project area is not located within any airport land use plan or within 2 miles of a public airport.
- f. The project area is not within the vicinity of any private airstrip.
- g. The project will not interfere with the implementation of any adopted emergency response plan or evacuation plan.
- h. There are no wildlands within or near the project area.

VIII. HYDROLOGY AND WATER QUALITY

- a. The project will not violate any water quality standards. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- b. The project will not substantially deplete groundwater supplies in the project vicinity.
- c. The project will not result in substantial erosion on- or off-site.
- d. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- e. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- f. There are no reasonably foreseeable reasons why the project would result in the degradation of water quality.
- g. The project area is located within Zones X500. Sites designated as being within X500 are areas of 500-year flood.
- h. The project area is located within Zones X500. Sites designated as being within X500 are areas of 500-year flood.

- i. The project would not expose people or structures to risks from failure of levee or dam.
- j. Seiche and tsunami impacts do not occur in the Visalia area. The site is relatively flat, so there will be no impacts related to mudflow.

IX. LAND USE AND PLANNING

- a. The project will not physically divide an established community.
- b. The site is within the current Urban Development Boundaries of the City of Visalia. The City of Visalia designates the area for urban development. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use.
- c. The project does not conflict with any applicable conservation plan.

X. MINERAL RESOURCES

- a. No mineral areas of regional or statewide importance exist within the Visalia area.
- b. There are no mineral resource recovery sites delineated in the Visalia area.

XI. NOISE

- a. The project will result in noise generation typical of urban development. There will be noise generated by traffic along designated arterial and collector streets. The City's standards for setbacks and/or construction of walls along major streets will reduce noise levels to a level that is less than significant.
- b. The project will not result in ground-borne vibration or ground-borne noise levels. There are no existing uses near the project area that create ground-borne vibration or ground-borne noise levels.
- c. Noise levels will increase beyond current levels as a result of the project. These levels will be typical of noise levels associated with urban development. Therefore, the increase is less than significant.
- d. Noise levels will increase during the construction of the project but shall remain within the limits defined by the City Noise Ordinance.
- e. The project area is not within an airport land use plan, nor is it within 2 miles of a public airport.
- f. There is no private airstrip near the project area.

XII. POPULATION AND HOUSING

- a. The requested project will facilitate for development of this site which will result in increased housing in the area. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for urban use.
- b. There are no houses on the site. This does not displace substantial numbers of housing units which would necessitate the construction of replacement housing elsewhere.

- c. There is a vacant house on the site; development of the site will not displace people living within existing residences.

XIII. PUBLIC SERVICES

- a.
 - i. Current fire protection facilities can adequately serve the site without a need for alteration.
 - ii. Current police protection facilities can adequately serve the site without a need for alteration.
 - iii. Current school facilities can adequately serve the site without a need for alteration.
 - iv. Current park and recreation facilities can adequately serve the site without a need for alteration.
 - v. Other public facilities can adequately serve the site without a need for alteration.

XIV. RECREATION

- a. The proposed project will not increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated.
- b. The proposed project does not include recreational facilities. It will not require the construction or expansion of recreational facilities within the area that might have an adverse physical effect on the environment.

XV. TRANSPORTATION AND TRAFFIC

- a. Development of the site will result in increased traffic in the area. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for urban use.
- b. The site is projected for urban development by the City and County General Plans. The project is not proposed to exceed what has already been planned for in this area.
- c. The project will not result in nor require a need to change air traffic patterns.
- d. Upon development of the site, roads will be designed and constructed to City standards so that there will be no increased hazards.
- e. Upon development of the site, roads will be constructed to City standards that will provide adequate emergency access.

- f. The project will be required to meet the City's parking requirements for shopping centers

XVI. UTILITIES AND SERVICE SYSTEMS

- a. The site is projected for urban development by the City General Plan. The project is not proposed to exceed what has already been planned for in this area.
- b. The project calls for the extension of sanitary sewer facilities. Expansion will not result in a significant effect. The trunk line expansion is consistent with the City Sewer Master Plan.
- c. The project calls for the extension of storm drain facilities. Expansion will not result in a significant effect. The trunk line expansion is consistent with the City Storm Water Master Plan.
- d. There are sufficient water supplies to support the project.
- e. The City has determined that there is adequate capacity existing to serve the site's projected wastewater treatment demands at the City wastewater treatment plant.
- f. Current solid waste disposal facilities can adequately serve the site without a need for alteration.
- g. The project should be able to meet the applicable regulations for solid waste.

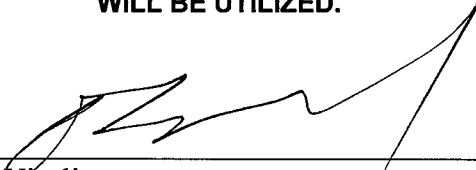
XVII. MANDATORY FINDINGS OF SIGNIFICANCE

- a. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- b. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- c. The project will not create adverse environmental effects to a human population.

DETERMINATION OF REQUIRED ENVIRONMENTAL DOCUMENT

On the basis of this initial evaluation:

- ☒ I find that the proposed project **COULD NOT** have a significant effect on the environment. **A NEGATIVE DECLARATION WILL BE PREPARED.**
- ☐ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on the attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION WILL BE PREPARED.**
- ☐ I find the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required
- ☐ I find that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 90020160). The Environmental Impact Report prepared for the City of Visalia Land Use Element (Amendment No. 90-04) was certified by Resolution NO. 91-105 adopted on September 3, 1991. **THE PROGRAM ENVIRONMENTAL IMPACT REPORT WILL BE UTILIZED.**



City of Visalia

7-13-06

Date

ENGINEERING CORRESPONDENCE

PLANNING
COMMISSION
&
CITY COUNCIL



REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: September 11, 2006

PROJECT PLANNER: Andrew Chamberlain, A.I.C.P., 713-4003

SUBJECT: **Specific Plan Amendment No. 2006-02:** A request by The Taylor Group to amend the Demaree/Caldwell Specific Plan to allow the relocation of an access point on Caldwell Avenue, and to allow a phased development of Sub-area B.

Conditional Use Permit No. 2006-32: A request by The Taylor Group to have a Pharmacy with a drive-thru lane.

The site is located on the southeast corner of Caldwell Avenue and Demaree Street (APNs: 126-030-033/034/035/036/014/015).

STAFF RECOMMENDATION *Item # 8*

Staff recommends approval of Conditional Use Permit No. 2006-29 based upon the findings and conditions in Resolution No 2006-90; and

Staff recommends approval of the phased development of Sub-area "B" but denial of the relocation of the access drive for Specific Plan Amendment No 2006-02 based on the findings in Resolution No. 2006-91.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No 2006-32 by adoption of Resolution No. 2006- 90

I move to approve Specific Plan Amendment No 2006-02, to approve the phased development of Sub-area "B", and to deny relocating the second Caldwell Avenue drive access from the east property line of the sub-area, by adoption of Resolution No. 2006-91

PROJECT DESCRIPTION

Specific Plan Amendment No. 2006-02 is a request to amend the Demaree/Caldwell Specific Plan to allow the relocation of an access point on Caldwell Avenue, and to allow a phased development of Sub-area B as shown in Exhibit "A". The Master Development Plan, as shown in Exhibit "C", contains two access drives onto Caldwell Avenue. The applicant is requesting to relocate the easternmost drive from the east property line by approximately 300 feet to the west. The second part of the specific plan request is to allow the phasing of Sub-area "B" to allow the construction of the proposed pharmacy as the first phase.

Conditional Use Permit No. 2006-32 is a request for a drive-thru window for a pharmacy as illustrated in Exhibit "A". Pharmacies are a permitted use in the underlying Community Commercial Zone and the Specific Plan, but the drive-thru component requires a Conditional Use Permit approval .

BACKGROUND INFORMATION

General Plan Land Use Designation: CCM
Zoning: C-CM (Community Comm.)
Surrounding Land Use and Zoning: North: C-CM / Commercial
South: C-CM / Commercial
East: C-CM / Commercial
West: C-CM / Commercial
Environmental Review: Negative Declaration No. 2006-67
Special Districts: Demaree/Caldwell Specific Plan, Sub-area "B"
Site Plan: Site Plan Review No. 2006-02

RELATED PLANS & POLICIES

Please refer to Chapter 3 of the Demaree/Caldwell Specific Plan regarding the development plan for Sub-area "B". This document has been placed on the City web-site and may be found at, www.ci.visalia.ca.us under Community Development/Publications.

RELATED PROJECTS

The Demaree/Caldwell Specific Plan was adopted in 1996, and encompasses approximately 66 acres on the south side of Caldwell Avenue on the east and west sides of Demaree Street. This location is one of the four Community Commercial locations in the City which are designated in the General Plan Land Use Element to serve a quadrant of the community through a Specific Plan.

PROJECT EVALUATION

Condition Use Permit No.2006-32

The proposed drive-thru is similar to those approved for the pharmacies at the southeast corner of Walnut Avenue and Ben Maddox Way, and the northwest corner of Akers Street and Cypress Avenue.

Staff raised the issue of on-site circulation at the southeast corner of the proposed building with the applicants during the Site Plan Review process. The site layout has the two main access/drive aisles converging at the southeast corner of the pharmacy building where the loading dock and drive-thru access are also located. The number of potentially conflicting vehicle maneuvers in the future, when other tenants are established on the site, is a concern to staff. The Planning Commission may determine that added conditions to redesign this portion of the site should be included in this use permit. There are several options to enhance the function of this portion of the site. They would include, but not be limited to, reversing the vehicle direction for the drive-thru, which would reduce the need for vehicles entering and exiting the drive-thru from having to cross on-coming traffic in the main drive aisle. The addition of pavement striping and landscaping to further separate the loading dock and drive-thru from the main aisles would also reduce the potential for vehicle conflicts in this area. There is sufficient land on the site to move the building north up to five feet, or the east/west access drive south by

the same amount to allow more room for the added landscaping and/or striping. The addition of a larger landscape island along the south edge of the loading dock would also significantly enhance the visual appearance of the dock area.

It should be noted that one of the issues which has caused staff to recommend denial of the requested relocation of the drive aisle on Caldwell Avenue is related to the fact that the drive-thru would substantially contribute to potential peak hour traffic conflicts on the site due to vehicles exiting the drive-thru and having to cross oncoming traffic that is entering the site from Caldwell Avenue.

The applicant has included proposed building elevations in Exhibit "D", along with tentative elevations of the other shops and buildings in the sub-area in Exhibit "E". Staff has included a requirement for consistency with the elevations in Exhibit "D" in Condition No. 2. The shops and other building elevations are not conditioned by this action since they are on separate properties. Their future development is required to be compatible with the first approved building in the sub-area, as specified in Policy B-5 of the Specific Plan.

Specific Plan Amendment No 2006 -02

Phased Development of Sub-Area "B"

Staff supports the phased development of Sub-area "B". The site plan provided in Exhibit "A" demonstrates that there are opportunities to develop portions of the site independently based upon the common access drives which would serve any of the phases.

The applicant intends to install the cross-hatched area as Phase One for the proposed pharmacy. The Phase One improvements include all four access drives, with approximately 30 feet of paving width for the drive aisles to the south and east. Staff supports providing full vehicle access on the site with Phase One. The southerly extension of the proposed main north/south access drive all the way to the southern property line of the sub-area will create a large pocket of un-improved frontage along Demaree Street.

As a part of Phase One, staff has included a condition requiring a seven foot high block wall along a portion of the southern property line of the Sub-area, adjacent to the existing single family residence. The wall will provide view and noise protection to the house, and should extend approximately 25 feet east of the house. The remaining 460 feet to the west (approximate) will be addressed at the time of a development proposal for the "Major Retail A", the interim wall is being proposed as needed.

Relocation of Caldwell Avenue Access Drive

Staff is recommending denial of the relocation of the Caldwell Avenue access drive from the east property line to the proposed location to provide direct access to the pharmacy. As the access drive exists now in the Specific Plan, it is a secondary access drive along the eastern property line which would typically be for service and delivery vehicles.

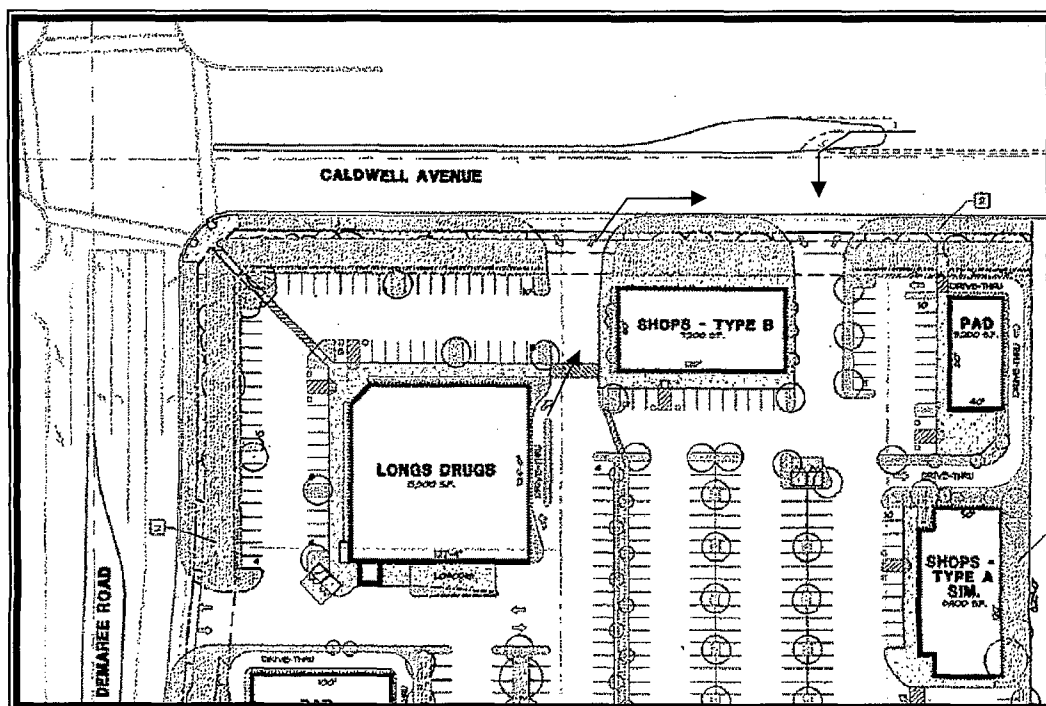
The applicant has provided an Access Analysis, Exhibit "G", which indicates that the benefits of greater safety and convenience will result, along with increased speeds of traffic on Caldwell Avenue. The Analysis concludes that the relocation of the second access drive from a typical service vehicle access to a major access point, thereby creating two major access points on Caldwell Avenue, will give drivers quicker access and reduce frustration. The analysis also recommends that a "decel lane" be added to afford additional protection. The addition of a "decel lane" would typically require that the building be moved back further to accommodate the loss of landscaping along the frontage.

The recommendation to deny the relocation of the access drive is based on the issues listed below:

1. Potential conflicting turning maneuvers from vehicles exiting the site east-bound on Caldwell Avenue while west-bound vehicles are trying to cross the east-bound lanes at the median break 150 feet away.
2. The drive-thru would substantially contribute to potential peak hour traffic conflicts on/off the site due to vehicles exiting the drive-thru having to cross oncoming traffic is that is entering the site from Caldwell Avenue, which would typically be most problematic at the peak hours. The access analysis does not cover the potential of on-site conflicts from the proposed drive-thru.
3. The proposed two access drives have approximately 80 feet of separation between the radius returns which does not meet City Standards for 500 feet between the access points. It should be noted that the existing Specific Plan does not have the required separation for the approved access points on Caldwell Avenue in Sub-area "B".
4. Two close access points gives bicycles and pedestrians two areas of conflict with vehicles utilizing the site.

The City Public Works Department has reviewed the applicant's Access Analysis and has also noted that the access point should not be relocated based upon the lack of separation between the two Caldwell Avenue access points. Engineering recognizes that the existing plan does not meet the separation standards, and that the proposed relocation of the access drive does meet the minimum 200 foot distance from the arterial intersection.

The City Engineer has included a condition in the Use Permit that the medians in Caldwell and Demaree shall be installed with Phase One to control conflicting turning maneuvers entering/exiting the site. Staff has included Condition No. 7 requiring the medians and striping as a part of the building permit for the pharmacy/Phase One. At the southern access point on Demaree, there may not be enough existing right-of-way to accommodate the median and striping, wherein the Public Works Department would work with the applicant to create an interim solution.



Landscaping

Landscaping plans will be submitted with the project's building permit package for review by the Planning Division. Staff is further recommending a condition of approval that requires placing a permanent landscape screen or mounding that achieves a height of three-foot to visually screens parking stalls and drive-thru lanes from any adjacent public street. The applicant has provided Caldwell and Demaree cross sections, Exhibit "F", which show a three-foot high wall or evergreen hedge to screen the parking areas adjacent to the street. The provision for a three-foot high wall, mound or evergreen hedge is one of the landscaping standards in the Specific Plan.

Environmental Review

An Initial Study was prepared for this project, consistent with the California Environmental Quality Act (CEQA). The Initial Study disclosed that environmental impacts are determined to be not significant. Therefore, staff recommends that Negative Declaration No. 2006-67 be adopted for this project.

RECOMMENDED FINDINGS

Conditional Use Permit No. 2006-32

1. That the proposed drive-thru is similar to other pharmacy drive-thru approvals.
2. That the proposed drive-thru will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
3. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located.
 - The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity.
3. That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
4. That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

Specific Plan Amendment No 2006 -02

1. That the phased development of the sub-area can be done in an orderly fashion as shown in Exhibit "A".
2. That the proposed Specific Plan Amendments, as amended by staff, are consistent with the purpose and intent of the Demaree Caldwell Specific Plan, Zoning Ordinance , and the Land Use Element.
3. That the proposed relocation of the access drive from the eastern property line to the proposed location is not supported based upon the following:

- Potential conflicting turning maneuvers from vehicles exiting the site east-bound on Caldwell Avenue while west-bound vehicles are trying to cross the east-bound lanes at the median break 150 feet away.
 - The drive-thru would substantially contribute to potential peak hour traffic conflicts on/off the site due to vehicles exiting the drive-thru having to cross oncoming traffic is that is entering the site from Caldwell Avenue, which would typically be most problematic at the peak hours. The access analysis does not cover the potential of on-site conflicts from the proposed drive-thru.
 - The proposed two access drives have approximately 80 feet of separation between the radius returns which does not meet City Standards for 500 feet between the access points. It should be noted that the existing Specific Plan does not have the required separation for the approved access points on Caldwell Avenue in Sub-area "B".
 - Two close access points gives bicycles and pedestrians two areas of conflict with vehicles utilizing the site.
4. That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
 5. That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

RECOMMENDED CONDITIONS OF APPROVAL

Conditional Use Permit No. 2006-32

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2006-002.
2. That the site be developed in substantial compliance with the site plan shown in Exhibits "A" and "B", and the building elevations in Exhibit "D", attached herein, and with modifications to match the Sub-area access points in the Specific Plan,.
3. That landscaping and irrigation plans be submitted for review by Planning Division staff with building permits.
4. That an evergreen hedge, mound, or wall approximately 3 foot-high be installed along drive-thru lanes and parking lots which are directly adjacent to Caldwell Avenue or Demaree Street, consistent with the Specific Plan requirements.
5. That a seven foot high block wall be constructed along the southern property line of Sub-area "B" from approximately 25 feet behind the right-of-way line on Demaree to approximately 25 feet past the east end of the house on the adjacent property. An additional wood fence, minimum height six foot, will be required to define the property line and reduce headlight glare across the balance of the property to the south where a fence of this description may not already be in place.
6. That this conditional use permit shall not be deemed approved if Specific Plan Amendment No. 2006-02 is not approved by the City Council.
7. That the Caldwell and Demaree medians and striping shall be installed by the developer as a part of the pharmacy/Phase One building permit (such medians to control/eliminate left turns

into the subject site). This condition shall be met prior to the first building occupancy in Sub-area "B".

8. That all other city codes and ordinances be met.
9. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2006-32, prior to the issuance of any building permits for this project.

Specific Plan Amendment No 2006-02

1. That the phasing of the proposed project be substantially consistent with Exhibit "A".
2. That the adopted vehicle circulation and access plan (figure 9, Caldwell /Demaree Specific Plan) be met.
3. That all other requirements of the Caldwell /Demaree Specific Plan be met.

Attachments:

- Exhibit "A" – Site Plan
- Exhibit "B" – Site Plan - Pharmacy
- Exhibit "C" – Sub-area "B" site plan from adopted Specific Plan
- Exhibit "D" – Pharmacy Elevations
- Exhibit "E" – Tentative Elevations for Shops and Other Buildings
- Exhibit "F" – Cross Sections for Caldwell Avenue and Demaree Street
- Exhibit "G" – Access Analysis
- General Plan Land Use Map
- Zoning Map
- Aerial Photo
- Location Map

APPEAL INFORMATION

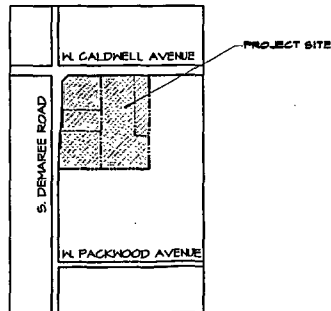
According to the City of Visalia Zoning Ordinance Section 17.38.120, an appeal to the City Council may be submitted within five working days following the date of a decision by the Planning Commission on a conditional use permit application. An appeal shall be in writing and shall be filed with the City Clerk at 707 W. Acequia Ave., Visalia, CA 93291. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record.

The Specific Plan Amendment goes on to the City Council for final action, any appeal of the Conditional Use Permit would typically be heard at the same time. If the conditional use permit is approved and not appealed, the City Council would not review the use permit, and only the specific plan amendment would be reviewed.

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 18 SOUTH, RANGE 24 EAST, NORTH IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 80 FEET THEREOF.

VICINITY MAP



DEMAREE ROAD

CALDWELL AVENUE

LONG'S DRUGS
13,000 S.F.

SHOPS - TYPE B
1,200 S.F.

SHOPS - TYPE A
SIM.
1,000 S.F.

MAJOR RETAIL
52,400 S.F.

SHOPS - TYPE A
10,000 S.F.

CROSS-HATCH
INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION
AT TIME OF FULL DEVELOPMENT

KEYNOTES:

- LANDSCAPING AREA
- DASHED LINE INDICATES BUMPER OVERHANG, TYP.
- 8" HIGH CONTINUOUS CONCRETE CURB, TYP., SEE CIVIL DRAWINGS
- PAINTED WHITE DIRECTIONAL ARROW, TYP., SEE DETAIL XX
- 4" WIDE PAINTED WHITE PARKING STRIPE, TYP., SEE DETAIL XX
- HCAP SYMBOL, PAINTED ON PAVEMENT, TYP., SEE DETAIL XX
- HCAP PARKING SIGN, TYP., SEE DETAIL XX
- BUILDING ENTRANCE
- DASHED LINE INDICATES RED PAINTED CURB WITH 8" WHITE LETTERS STATING "NO PARKING", TYP.
- VAN ACCESSIBLE HCAP LOADING AREA 14' 4" PAINTED WHITE STRIPE, TYP., SEE DETAIL XX
- CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-10) REMOVE (2) CONC. CURBS & BUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
- SITE HCAP "TOWNAUT" SIGN, TYP., SEE DETAIL XX
- (2) CONC. CURBS AND BUTTER
- DISABLED ACCESS RAMP (5:20 MAX. SLOPE)
- INDICATES PROPERTY OR PARCEL LINE, TYP.
- (2) CONC. SIDEWALK
- C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CURB, SEE DETAILS 24/5/SPR2 PER CITY OF VISALIA, SLOPE CONC. TO FLUSH WITH PAVEMENT, COLOR OF MALL AND GATES TO BE PAINTED TO MATCH
- (2) CABLE/TELECOM BOX
- DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
- DASHED LINE INDICATES SECTION BOUNDARIES
- BUS STOP WITHIN 50 FT. OF INTERSECTION
- SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
- 8 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
- 8-12 FT. HIGH CMU FENCE, HEIGHT TO BE DETERMINED, FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED, SEE DETAIL USPR2
- STOP SIGN
- INDICATES PROPOSED LEFT TURN POCKET
- CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

- ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO BE CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS SHALL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
- A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
- PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/AGREEMENTS MUST NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
- ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
- ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
- ALL SIGNS REQUIRE A SEPARATE PERMIT.
- ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
- VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	572,497 SQ. FT. (13.0 ACRES)
BUILDING AREA:	21,051 SQ. FT.
COVERAGE:	3.68%
APN:	128-050-05,34,35, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 8' x 18' THROUGH OUT	= 541 STALLS
HCAP PARKING STALLS 8' x 18' THROUGH OUT	= 17 STALLS
PARKING REQ'D. (1 PER 225 S.F.)	= 560 STALLS
TOTAL PARKING PROVIDED	= 558 STALLS
PARKING RATIO:	= 4.3 STALLS/1000 S.F.

SITE PLAN

SCALE: 1"=40'-0"

THE TAYLOR GROUP
ARCHITECTS

978 WEST ALLUVIAL AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

PROPOSED MASTER SITE PLAN FOR
LONG'S DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

© The Taylor Group reserves the right to make any changes or modifications to this plan without notice. This plan is not to be used for any other purpose without the written consent of The Taylor Group.

DRAWN BY: [Signature]
DATE: [Date]
REVISIONS: [Table]
SHEET NO: [Number]

SPR1

OF [Number] SHEETS



STATISTICAL INFO.	
SALES AREA	12,320 SQ. FT.
WAREHOUSE	3,981 SQ. FT.
USEABLE WAREHOUSE	1,250 SQ. FT.
TOTAL	16,671 SQ. FT.

DATE: 4/20/00
DRAWN BY:
SCALE: 1/8" =

1. The following are the names of the persons who have been appointed to the various committees of the Board of Directors of the Corporation for the year ending December 31, 1968:

[illegible]

REVISIONS

Longs Drug Stores
PROPERTIES DEPARTMENT
141 N. CIVIC DRIVE
WALNUT CREEK, CALIF. 94596
(916) 211-6000

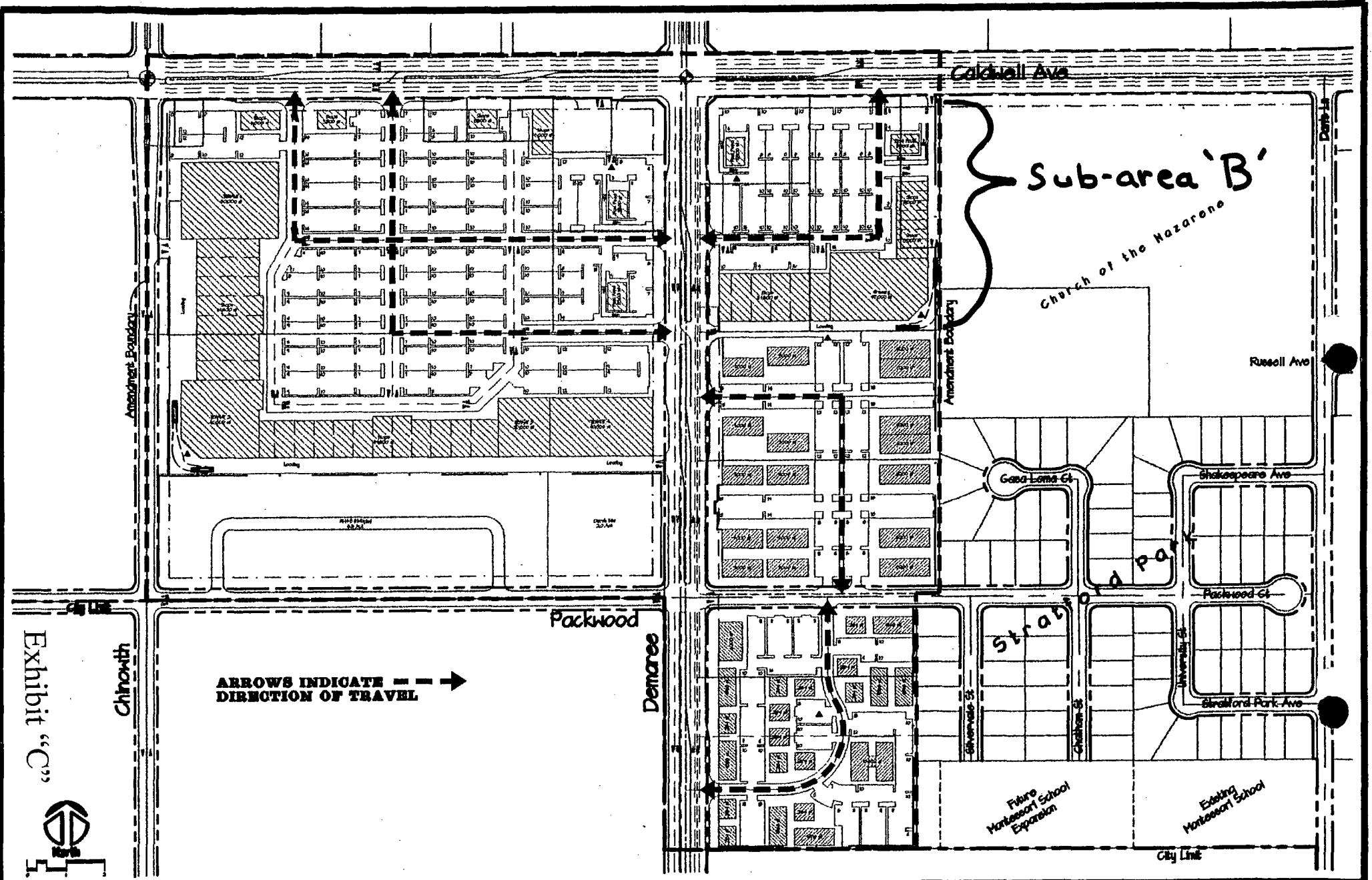
STORY #	
---------	--

VISALIA

Exhibit "C"



QUAD



CALDWELL / DEMAREE SPECIFIC PLAN
ON-SITE CIRCULATION PLAN

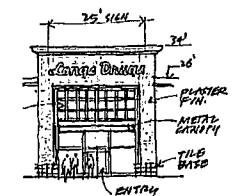
Figure 9



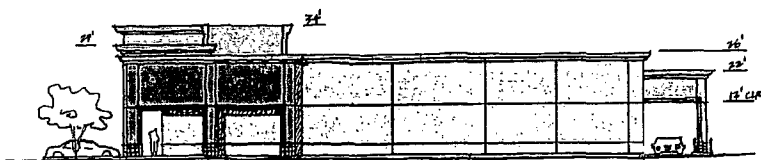
NORTH ELEVATION



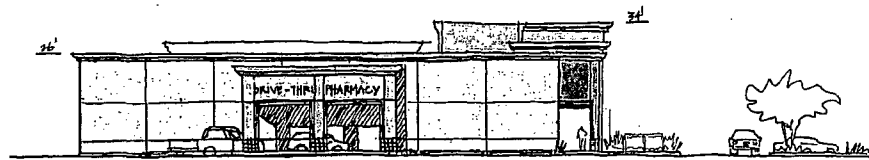
WEST ELEVATION



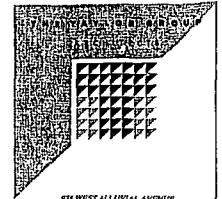
TOWER HEAD-ON ELEVATION



SOUTH ELEVATION



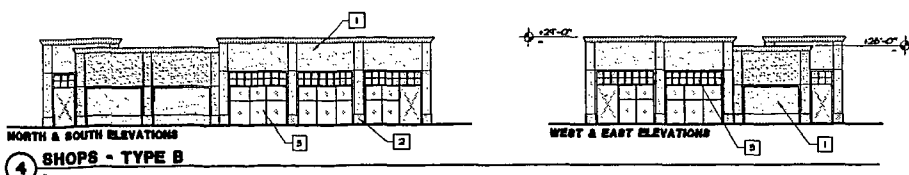
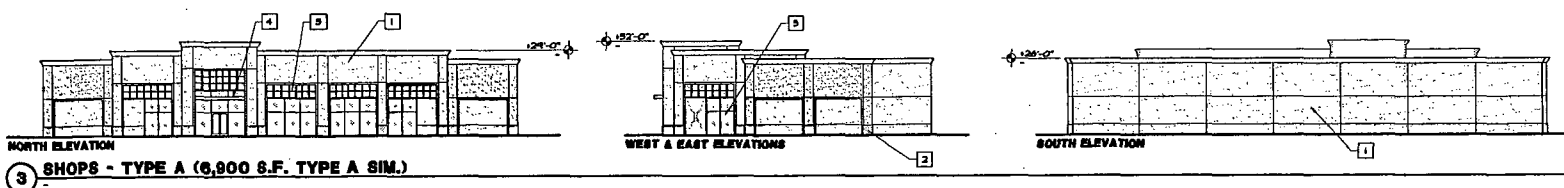
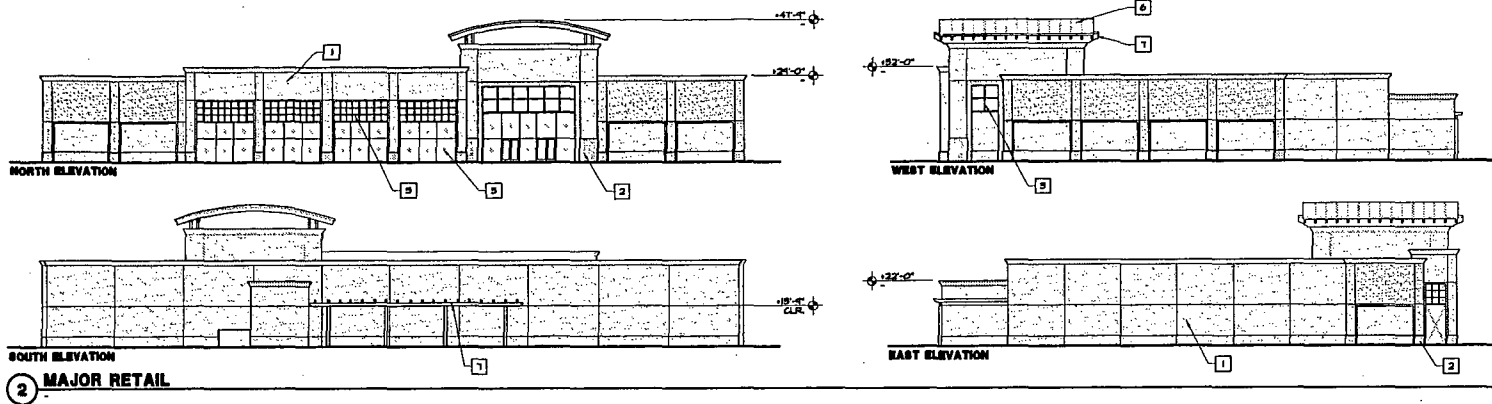
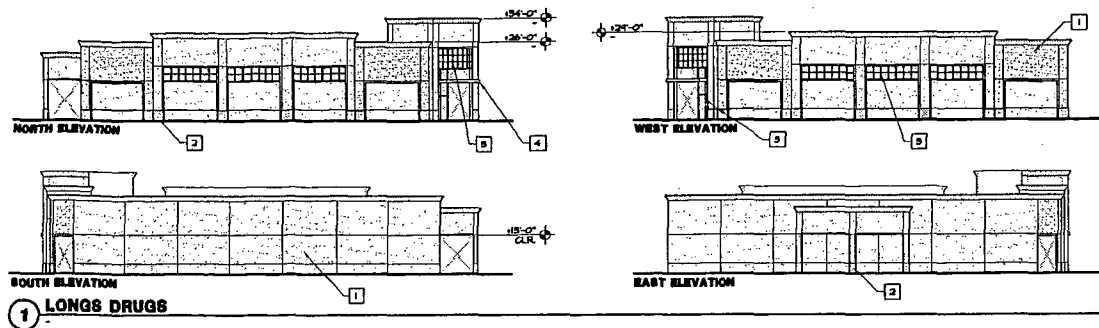
EAST ELEVATION



874 WEST ALLIANCE AVENUE
SUITE 107
FREMONT, CA 94531
TEL. 510-437-5000
FAX 510-437-5005

The following is a preliminary drawing and is not to be used for construction purposes. It is intended for informational purposes only. The final drawing will be provided upon completion of the project.

DESIGNED BY _____
DATE _____
REVISIONS _____
SHEET NO. _____
OF _____ SHEETS



ELEVATIONS
SCALE: 1"=30'-0"



**THE TAYLOR GROUP
ARCHITECTS**

978 WEST ALLIANCE AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 658 . 437 . 5000
FAX. 658 . 437 . 5005

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

© The Taylor Group is hereby notified that the design and construction of the project shown on this plan is the property of the Taylor Group. The design and construction of the project shown on this plan is the property of the Taylor Group. The design and construction of the project shown on this plan is the property of the Taylor Group.

DRAWN BY	DATE
REVISIONS	
SHEET NO.	
SPR3	
OF SHEETS	

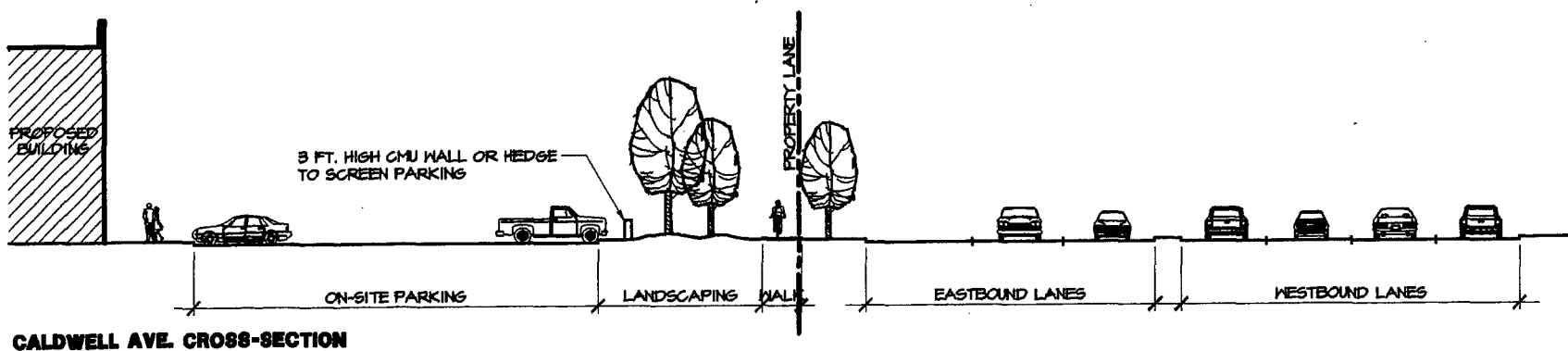
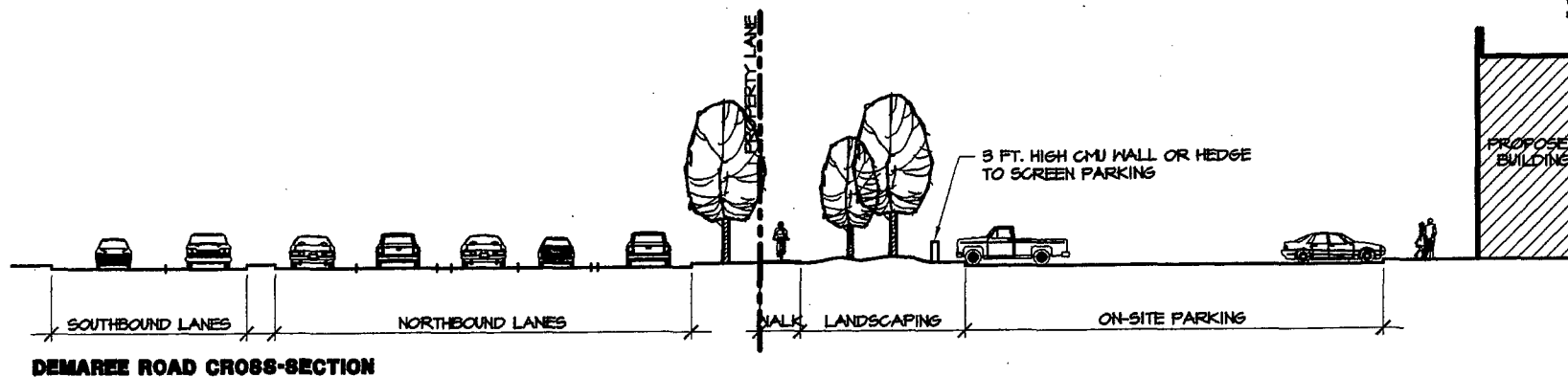
THE TAYLOR GROUP
ARCHITECTS



1000 10th Street, Suite 100

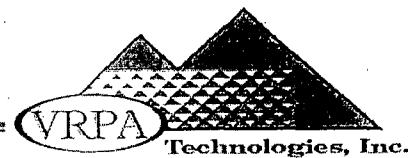
San Francisco, CA 94103

415.774.1000
415.774.1001



PROPOSED STREET CROSS SECTION FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

DATE	10/10/00
BY	XXX
CHECKED BY	
APPROVED BY	
SCALE	AS SHOWN



August 10, 2006

Mr. Joe Cavanagh
Longs Drug Stores Inc.
141 N. Civic Drive
Walnut Creek, CA 94596

Re: Caldwell/Demaree Shopping Center Access Analysis – 2nd Driveway along Caldwell Avenue

Dear Mr. Cavanagh:

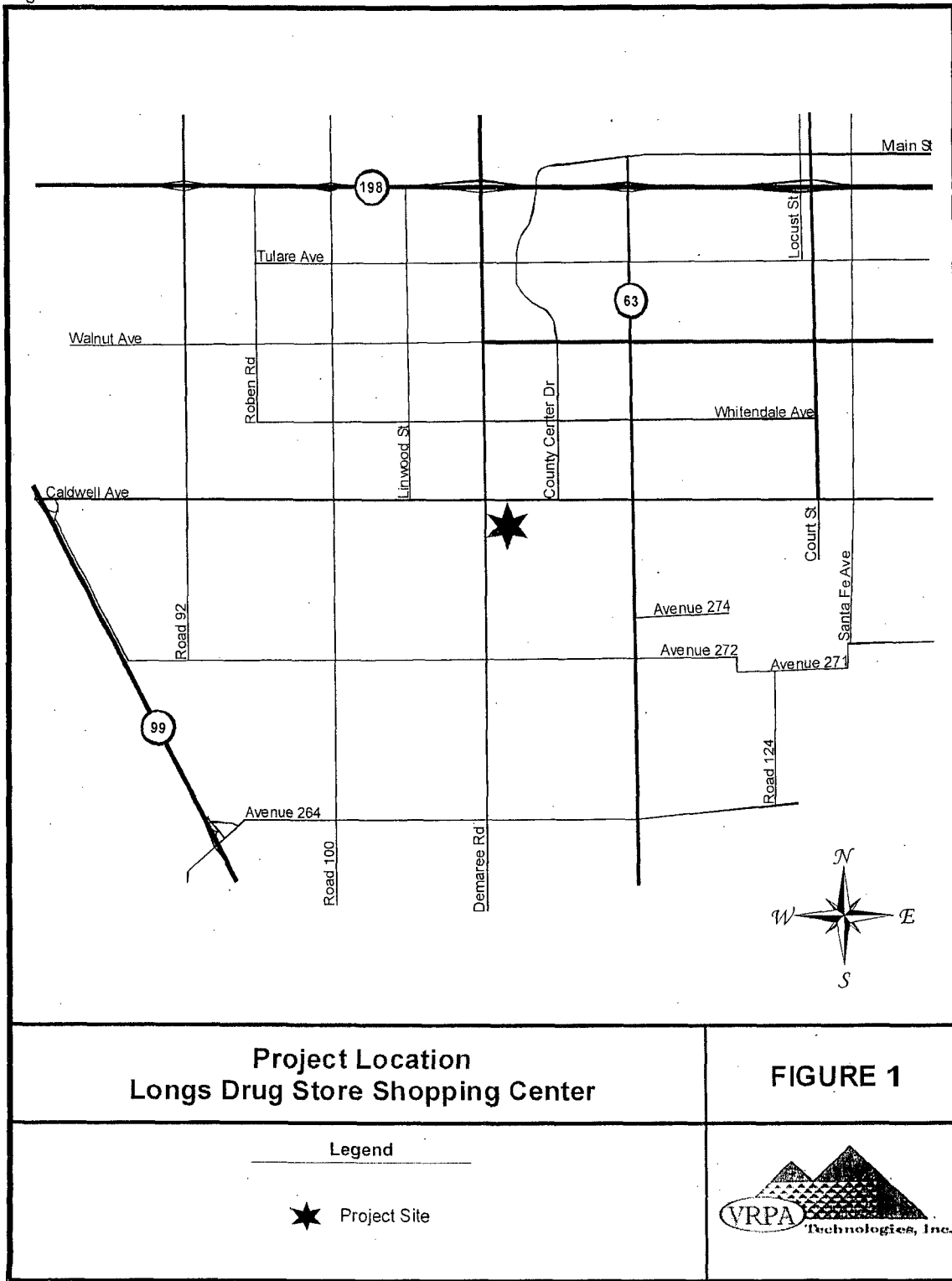
As requested, VRPA Technologies, Inc. has prepared an analysis regarding a second driveway along Caldwell Avenue in Visalia to the proposed shopping center located at the southeast corner of Caldwell and Demaree (reference Figure 1). The City of Visalia Specific Plan for this property includes a westbound left turn pocket from Caldwell to the easternmost driveway along Caldwell which is also taken into consideration in this study. VRPA has analyzed the benefits of the 2nd access point or driveway (reference Figure 2).

STEP 1

The first step applied to analyze the proposed access point or driveway spacing, was to generate peak hour trips for the shopping center. To accomplish this step, VRPA applied the total square footage of the proposed shopping center (divided by 1,000) to the shopping center trip generation rate in (Use 820) in the Institute of Transportation Engineer's (ITE) Trip Generation Manual. Table 1 shows the results of this first step.

STEP 2

The second step was to determine existing and future traffic volumes along Caldwell and Demaree and at the intersection of Caldwell and Demaree. Existing AM and PM Peak Hour volume counts were conducted by VRPA in May 2006 (reference Figure 3). Trips generated by the proposed shopping center were then added to the existing AM and PM Peak Hour traffic counts as depicted in Figures 4 and 5. Figures 4 and 5 provide an assessment of how the proposed shopping center trips were distributed to the four proposed access points or driveways along Demaree and Caldwell. Future year (2030) volumes were derived using the Tulare County Association of Governments' Regional Traffic Model. The Traffic Model indicates a 3.5% increase in traffic in the Study Area between 2006 and 2030. VRPA then increased the existing traffic volumes by 3.5% to derive the Future Year turning movement volumes without the proposed shopping center at the Demaree and Caldwell intersection and the segment volumes along Demaree and Caldwell (reference Figure 6). Future AM and PM Peak Hour traffic with the proposed shopping center trips were then developed as shown in Figures 7 and 8.



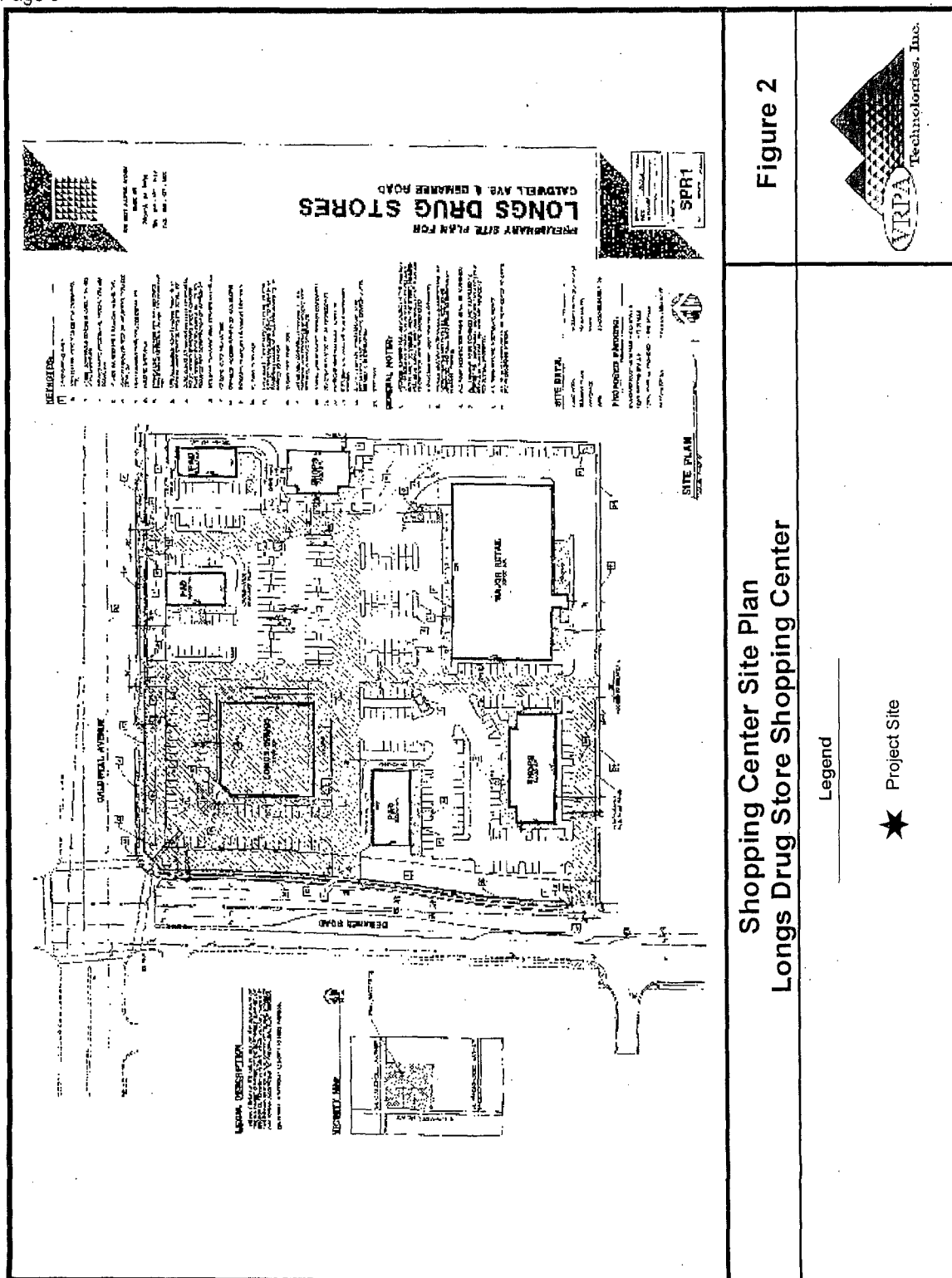


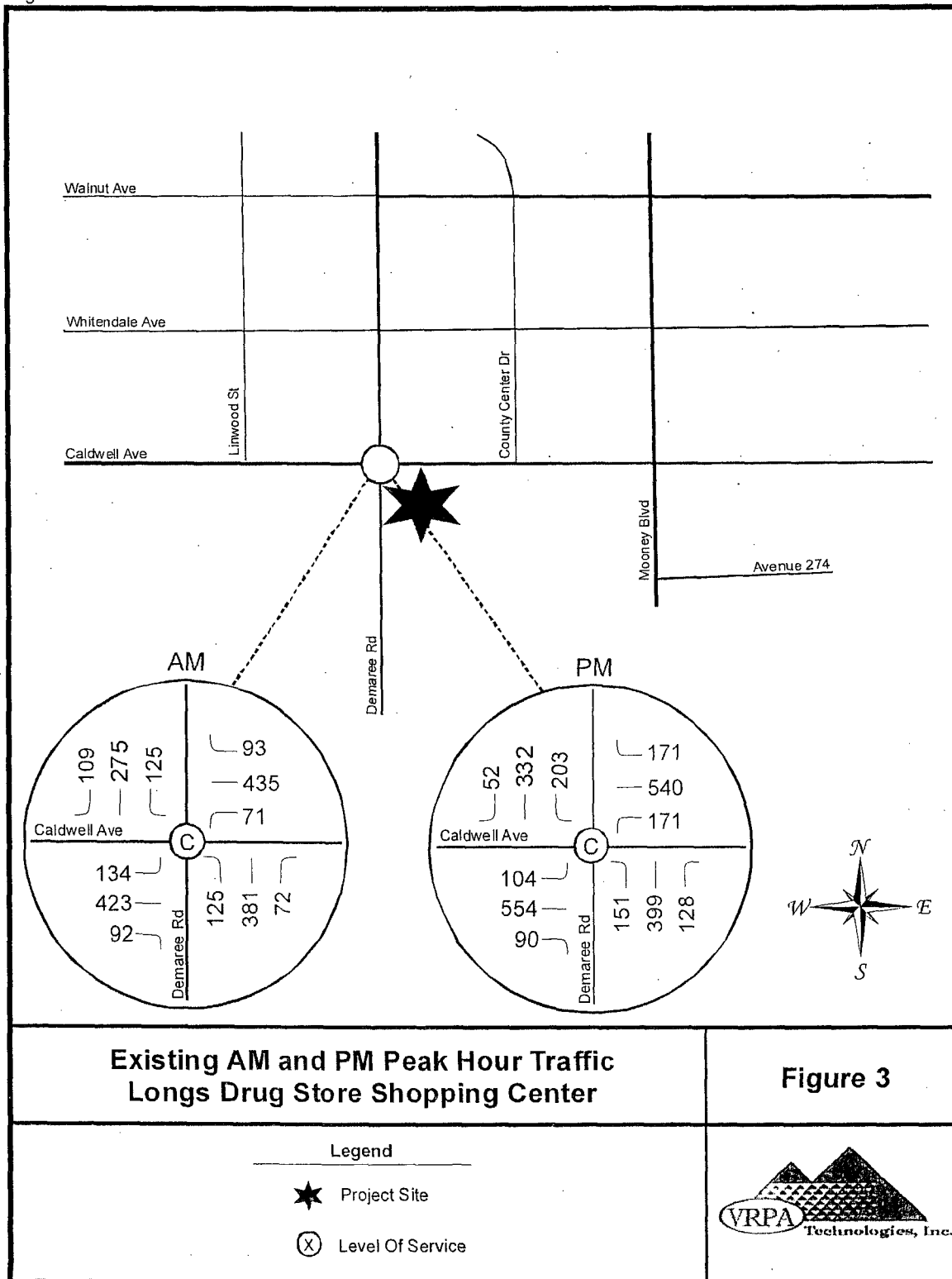
Table 1
PROJECT TRAFFIC GENERATION
Longs Drug Store Shopping Center

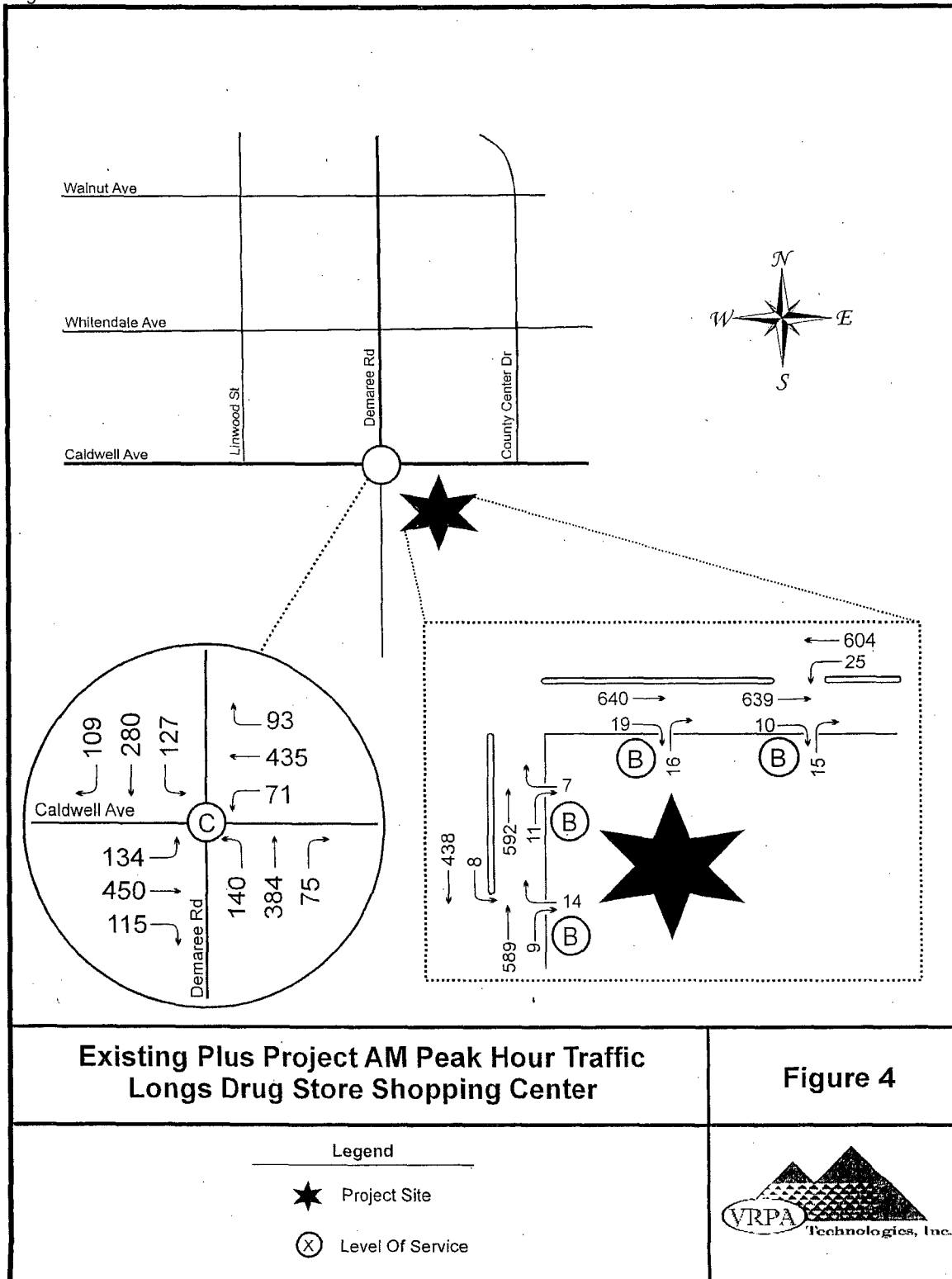
USE	SIZE	DAILY TRIP ENDS		AM PEAK HOUR				PM PEAK HOUR			
		RATE	VOLUME	RATE	IN:OUT SPLIT	VOLUME		RATE	IN:OUT SPLIT	VOLUME	
						IN	OUT			IN	OUT
Shopping Center (820)	77,400	74.28	5,749	1.73	61:39	82	52	6.83	48:52	254	275
TOTAL PROJECT TRIPS			5,749			82	52			254	275

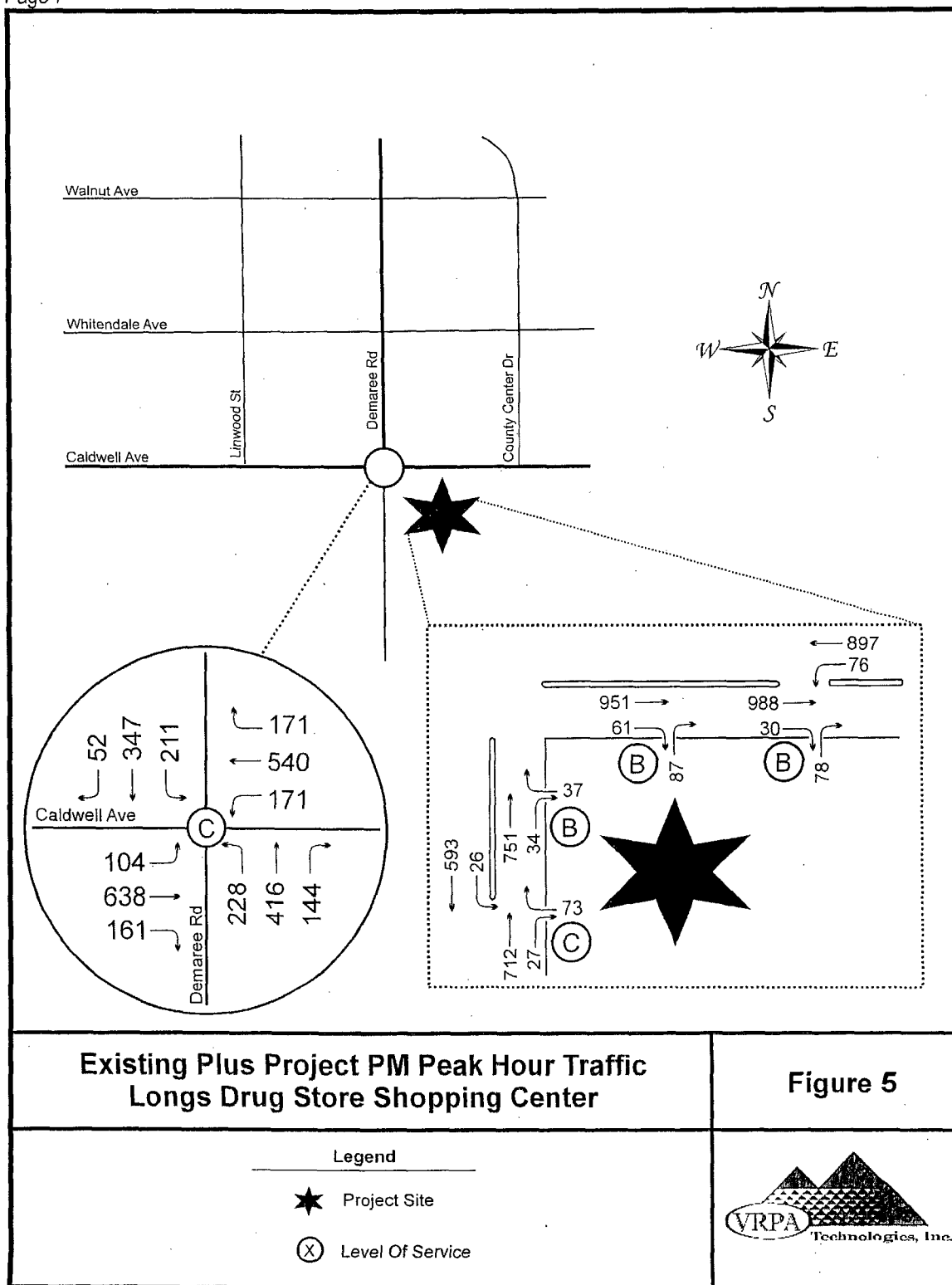
Source: Generation factors from ITE Trip Generation Manual, 7th Edition.

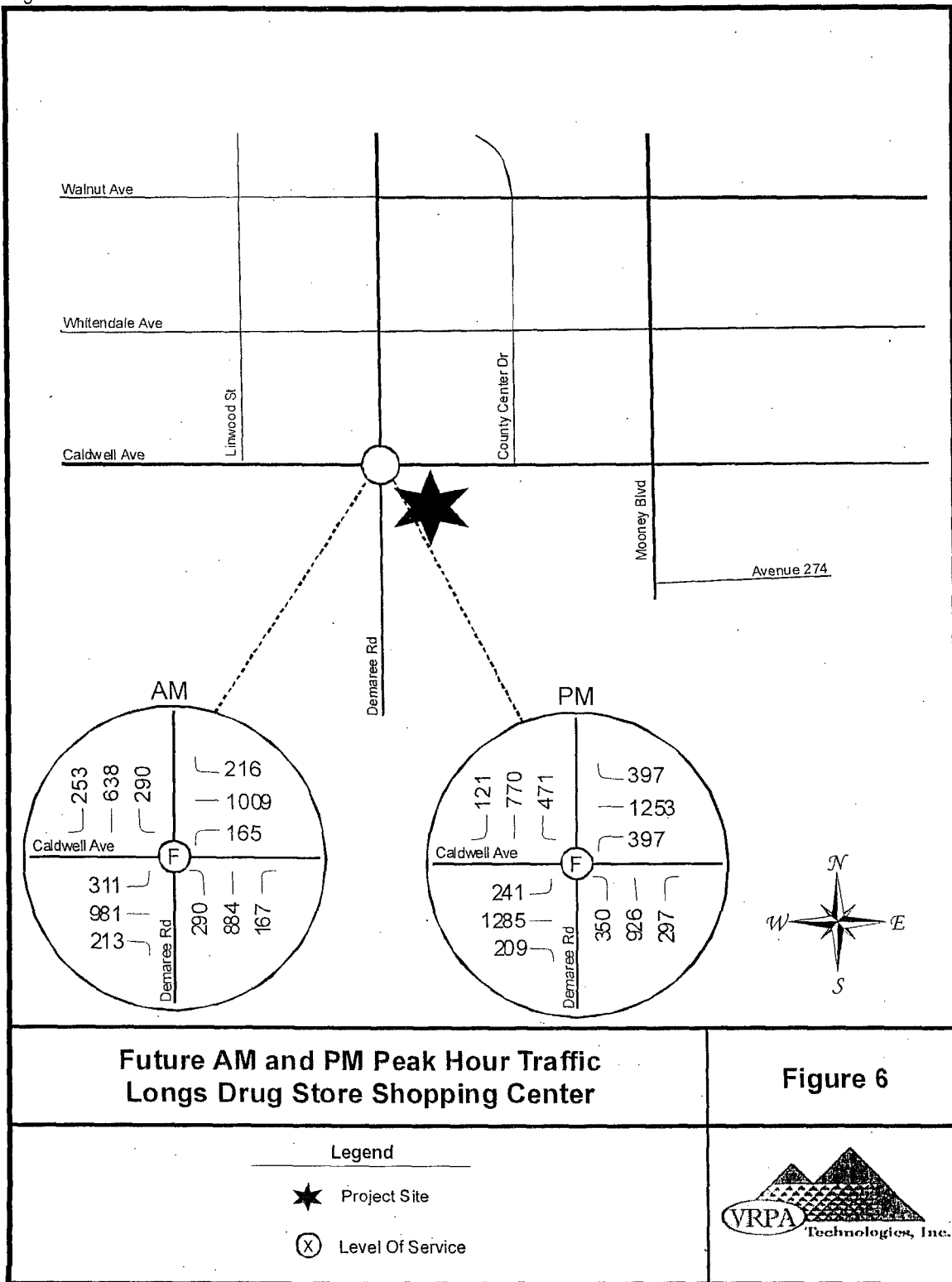
Trip ends are one-way traffic movements, entering or leaving.

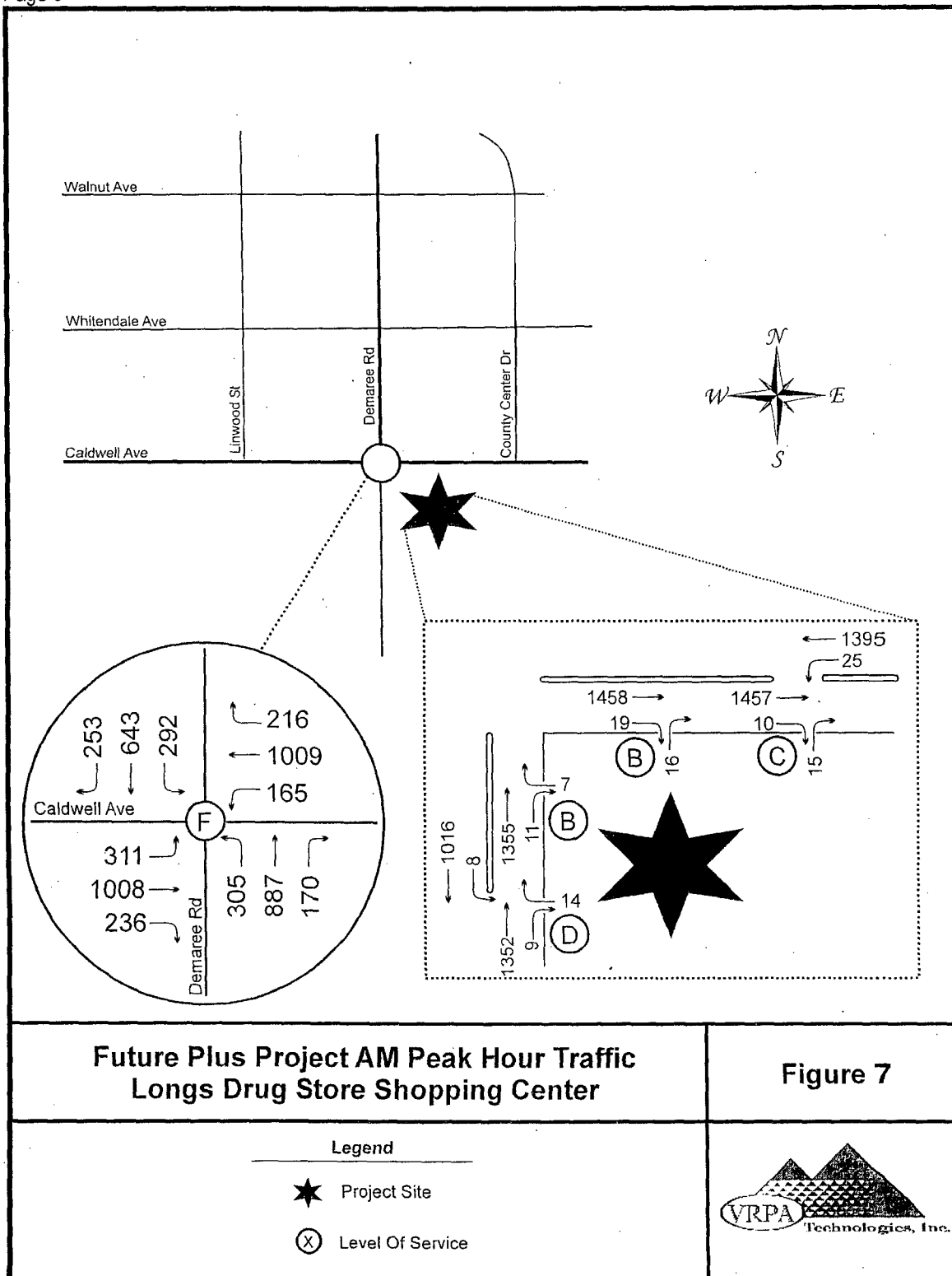
The numbers in parenthesis are ITE land use codes.

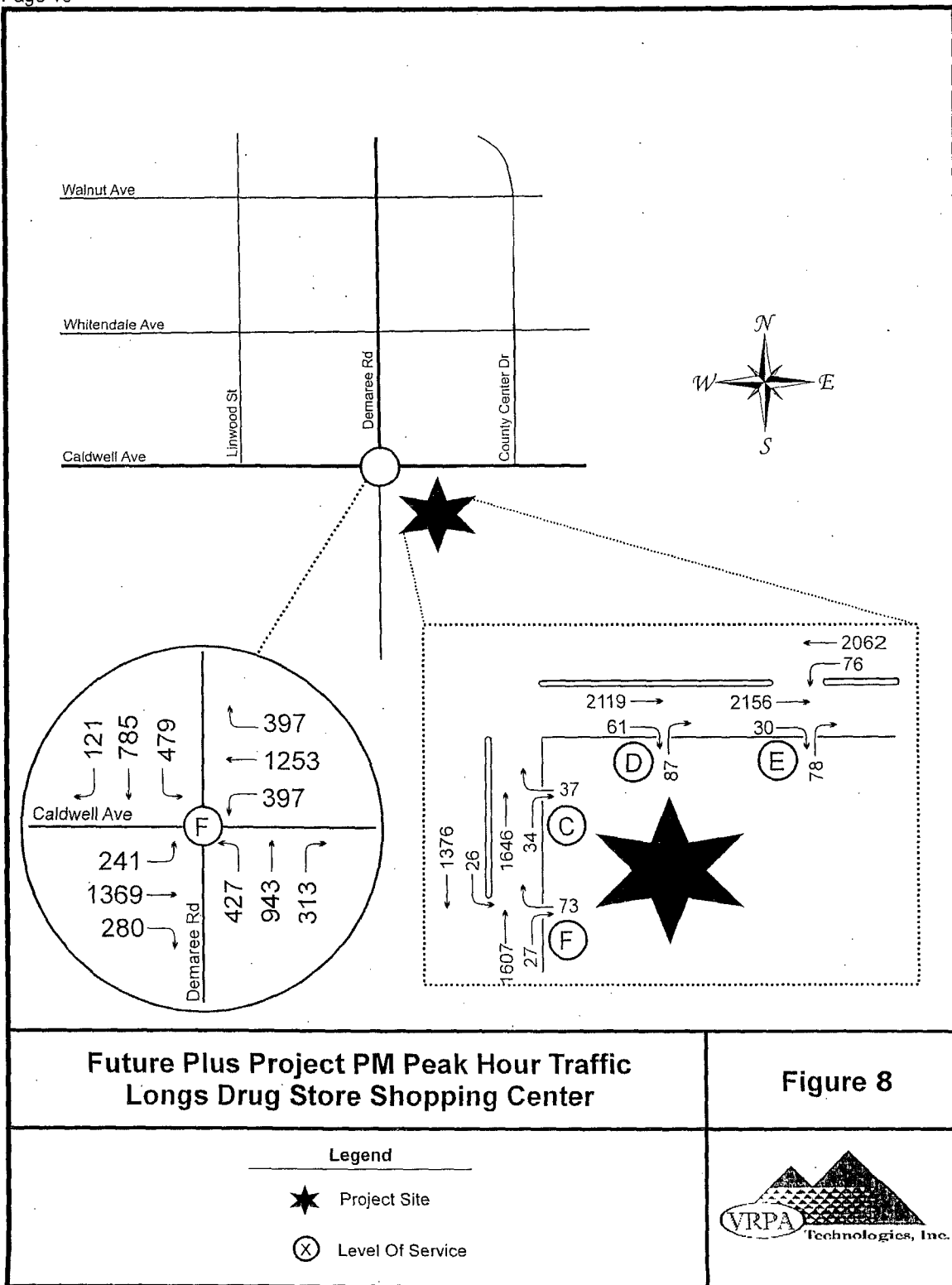












STEP 3

The third step in this process was to analyze the level of service associated with each of the proposed access points and the intersection of Demaree and Caldwell. The Highway Capacity Manual (HCM) software (HCS) was applied to determine LOS for each of the scenarios discussed in Step 2 (Existing, Existing plus the proposed shopping center, and Future Year 2030 without and with the proposed shopping center. The results of this step are shown in Table 2 and in each of the figures referenced in Step 2 (Figures 3 through 8). Referencing Table 2, the intersection at Demaree and Caldwell is projected to function at LOS F during the AM and PM without further improvement. The minimum LOS standard for the City of Visalia is LOS D. According to the TCAG Traffic Model, by the Year 2030, only 2 lanes in each direction are expected to be in place along Demaree and Caldwell.

Table 2
INTERSECTION OPERATIONS WITH 2 ACCESS POINTS ON CALDWELL
Longs Drug Store Shopping Center

INTERSECTION	PEAK HOUR	EXISTING		EXISTING PLUS PROJECT		FUTURE NO PROJECT		FUTURE PLUS PROJECT	
		DELAY	LOS	DELAY	LOS	DELAY	LOS	DELAY	LOS
Caldwell Ave / Demaree Rd ⁽¹⁾	AM	29.5	C	29.8	C	>80.0	F	>80.0	F
	PM	30.8	C	32.8	C	>80.0	F	>80.0	F
Southern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	D
	PM			N/A	C			N/A	F
Northern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	C
Eastern Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	C
	PM			N/A	B			N/A	E
Western Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	D

DELAY is measured in seconds.

LOS = Level of Service

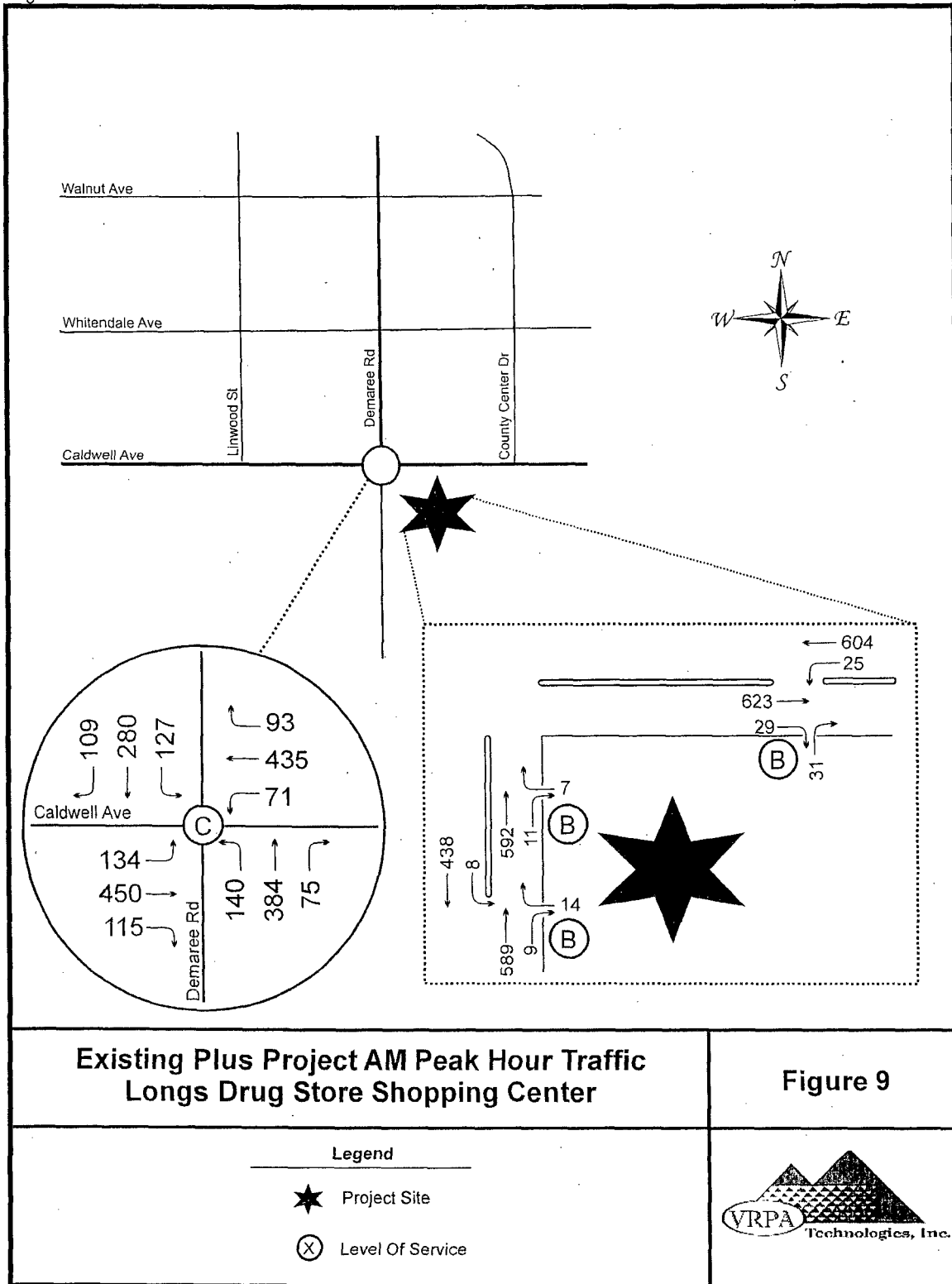
(1) Signalized intersection

(2) One-way stop controlled intersection

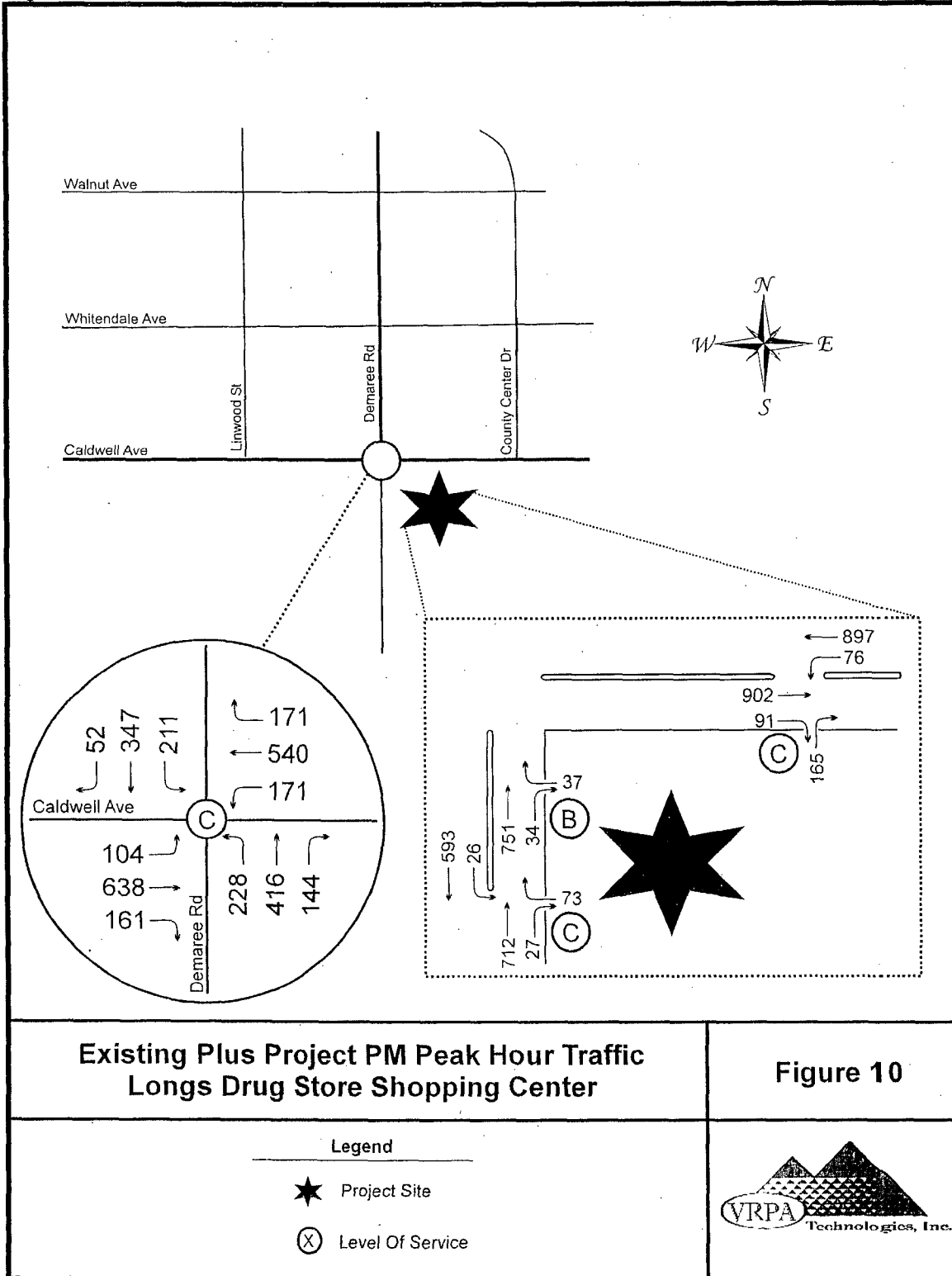
Referencing Table 2, the LOS at each of the proposed access points under Existing plus the proposed shopping center and Future Year 2030 without and with the proposed shopping center will operate at acceptable levels with the exception of the southernmost driveway along Demaree and easternmost driveway along Caldwell. These intersections will operate at LOS F and E respectively in the PM Peak Hour. As a result of the westbound left turn pocket along Caldwell, the "U" turns and left turns at the Caldwell and Demaree intersection, as well as the access points along Demaree to the project will be reduced. The volumes at these intersections or access points could very well increase in only one (1) access point along Caldwell is provided since entering vehicles would be concentrated at one (1) driveway thereby increasing delay and congestion.

An analysis of just one access point along Caldwell with a westbound left turn pocket from Caldwell was analyzed to compare the benefits of two (2) access points versus one (1) access point. Steps 1 – 3 above were also completed for the analysis of one (1) access point along Caldwell and results of this analysis are shown in Figures 9-12 and in Table 3.

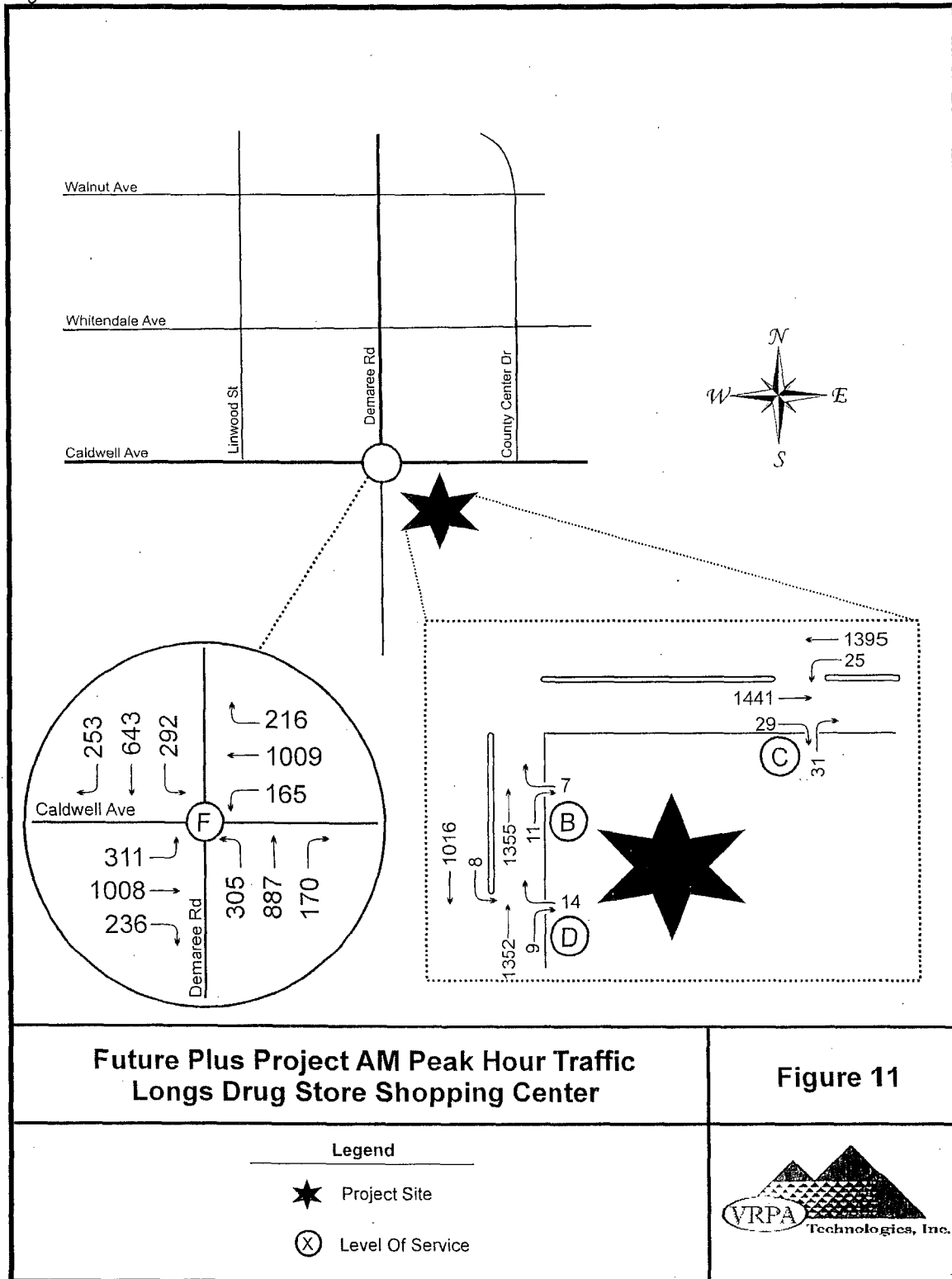
Mr. Chris Finn



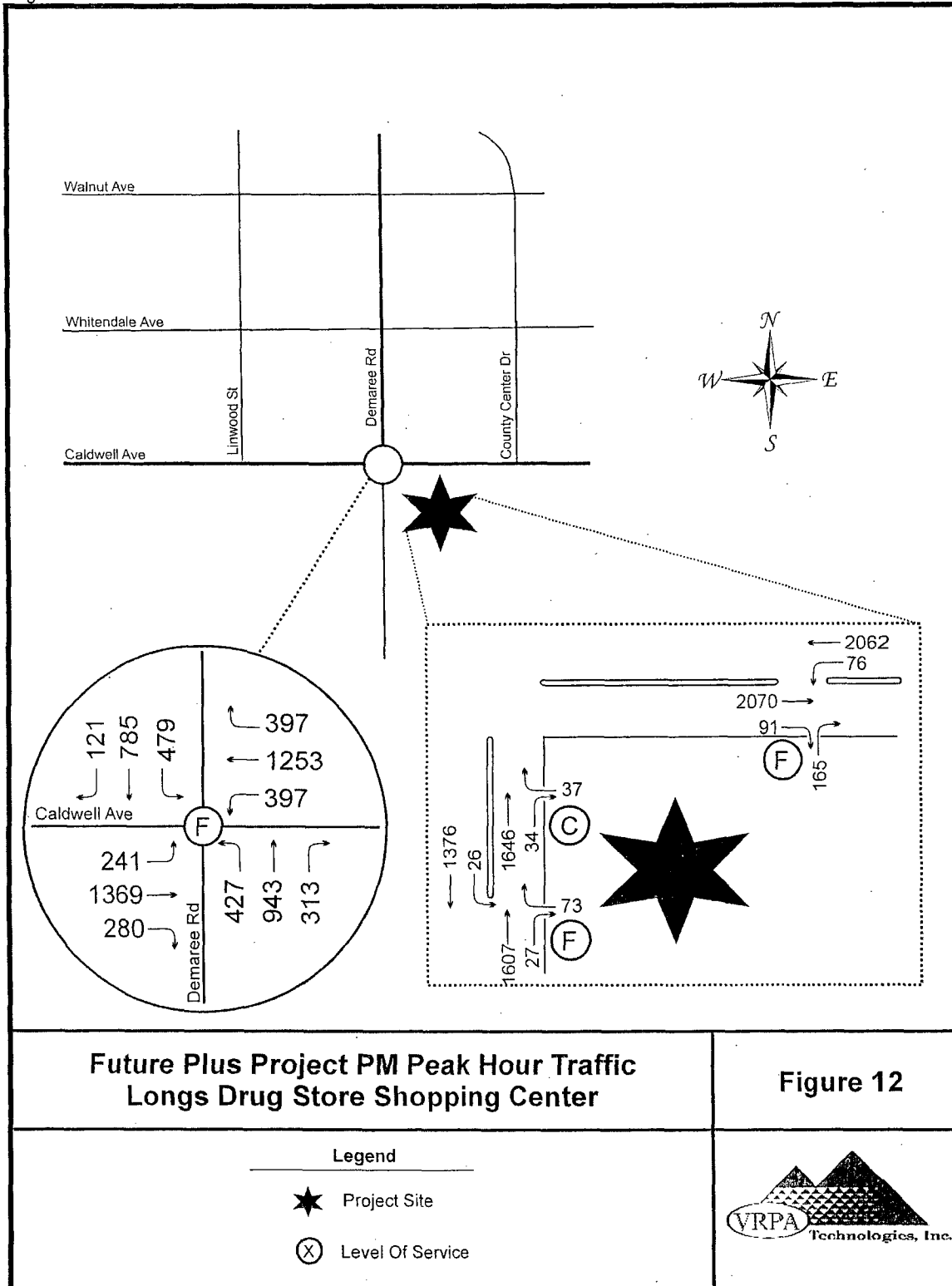
Mr. Chris Finn



Mr. Chris Finn



Mr. Chris Finn



Mr. Chris Finn

Table 3
INTERSECTION OPERATIONS WITH 1 ACCESS POINT ON CALDWELL
Longs Drug Store Shopping Center

INTERSECTION	PEAK HOUR	EXISTING		EXISTING PLUS PROJECT		FUTURE NO PROJECT		FUTURE PLUS PROJECT	
		DELAY	LOS	DELAY	LOS	DELAY	LOS	DELAY	LOS
Caldwell Ave / Demaree Rd ⁽¹⁾	AM	29.5	C	29.8	C	>80.0	F	>80.0	F
	PM	30.8	C	32.8	C	>80.0	F	>80.0	F
Southern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	D
	PM			N/A	C			N/A	F
Northern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	C
Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	C
	PM			N/A	C			N/A	F

DELAY is measured in seconds.

LOS = Level of Service

(1) Signalized intersection

(2) One-way stop controlled intersection

Referencing Table 3, the LOS at each of the proposed access points under Existing plus the proposed shopping center and Future Year 2030 without and with the proposed shopping center will operate at acceptable levels with the exception of the southernmost driveway along Demaree and the driveway along Caldwell. These intersections will operate at LOS F in the PM Peak Hour.

BENEFITS OF RELOCATED ACCESS/DRIVEWAY

The provision of two (2) closer spaced driveways will result in the following benefits:

- ◆ Greater safety
- ◆ Greater convenience
- ◆ Increase in speed along Caldwell Avenue

Specifics regarding these benefits are as follows:

- ◆ Provision of two (2) driveways or access points as depicted in Figure 13 along Caldwell Avenue will improve safety to bicyclists and pedestrians by providing more opportunity for motoring patrons to exit and enter the proposed development. With only one (1) driveway or access point for that portion of the development, patrons desiring to turn into the development will only have one opportunity to make a right-turn.
- ◆ With only one (1) driveway or access point to the shopping area, the queues on-site will grow long during the PM Peak Hour; drivers will get frustrated, and as a result, will begin to take chances when merging into eastbound Caldwell Avenue traffic. Two (2) closer spaced access points will reduce the resulting delay and frustration.

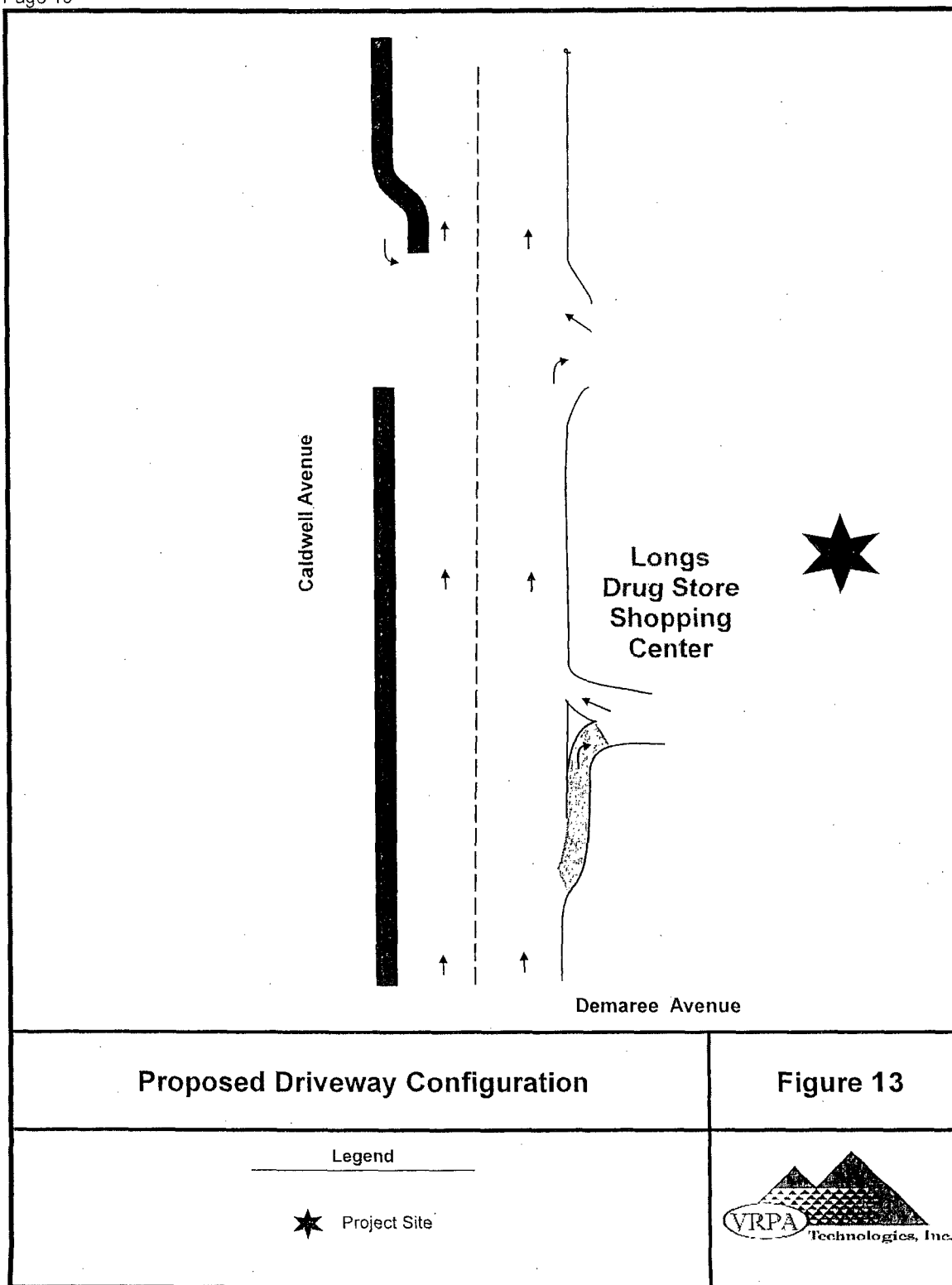
Mr. Chris Finn
August 10, 2006
Page 17

- ◆ With two (2) driveways or access points providing access to the shopping areas, patrons turning right into the development will have two opportunities to access the development, lessening the potential for "quick" or perceived "last opportunity" decisions, which may cause rear-end collisions and detrimentally affect passing pedestrians and bicyclists. The two (2) driveways along Caldwell will reduce the accidents related to vehicles turning right into the site because the vehicles will have two (2) opportunities to enter vs. one (1) opportunity.
- ◆ Patrons exiting the project along Caldwell Avenue will have two (2) opportunities to exit resulting in shorter on-site queuing during the PM Peak Hour and less frustration when trying to turn right and merge into traffic along eastbound Caldwell Avenue.
- ◆ The provision of one (1) vs. two (2) closely spaced access points does not negatively affect the resulting levels of service or seconds of delay experienced by motorists along Caldwell Avenue. In fact, provision of two (2) closer spaced access points or driveways along Caldwell Avenue would improve travel speed in the project area given the availability of a 2nd point of convenient access to the proposed development.

CONCLUSIONS

Based upon the results of Steps 1 through 3, the following conclusions can be drawn:

- ◆ The proposed driveway configuration along Caldwell Avenue depicted in Figure 13 will operate at LOS D or better on opening day or under Future Year conditions regardless of where the driveways are located along Caldwell Avenue with the exception of the easternmost driveway in the Future Year condition with project for the PM peak hour.
- ◆ To ensure that safety is addressed along Caldwell adjacent to the proposed shopping center, a minimum spacing of 100' should be provided between the proposed access points or driveways along Caldwell.
- ◆ To ensure additional safety and to reduce the potential for conflicts between vehicles traveling east and turning right into and out of the proposed access points or driveways along Caldwell, it is recommended that the westernmost driveway be configured as a "right turn deceleration lane with a raised island" prohibiting a right turning vehicle from changing its mind and deciding to turn right into the easternmost access point or driveway along Caldwell (reference Figure 13).

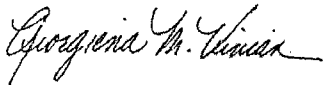


Mr. Joe Cavanagh
August 10, 2006
Page 19

Should you have any questions regarding our analysis, please feel free to contact me at (559) 259-9257 or by email at gvivian@vrpatechnologies.com.

Sincerely,

VRPA TECHNOLOGIES, INC.

A handwritten signature in cursive script, reading "Georgiena M. Vivian".

Ms. Georgiena M. Vivian,
Vice President

GV/ldb
Attachments

cc: Mr. Erik Ruehr, P.E., Dir. Of Traffic Engineering,
VRPA Technologies, Inc.

RESOLUTION NO 2006-90

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT 2006-32, A REQUEST BY THE TAYLOR GROUP TO ALLOW A DRIVE-THRU FOR A PHARMACY IN THE CALDWELL /DEMAREE SPECIFIC PLAN (COMMUNITY COMMERCIAL ZONE). THE SITE IS LOCATED AT THE SOUTHEAST CORNER OF CALDWELL AVENUE AND DEMAREE STREET

WHEREAS, Conditional Use Permit 2006-32 a request by the Taylor Group to allow a drive-thru for a pharmacy in the Caldwell /Demaree Specific Plan (Community Commercial Zone). The site is located at the southeast corner of Caldwell Avenue and Demaree Street. APNs: 126-030-033/034/035/036/014/015; and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on September 11, 2006; and

WHEREAS, the Planning Commission of the City of Visalia finds the Conditional Use Permit to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed drive-thru is similar to other pharmacy drive-thru approvals.
2. That the proposed drive-thru will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
3. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located.
 - The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity.
4. That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
5. That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2006-002.
2. That the site be developed in substantial compliance with the site plan shown in Exhibits "A" and "B", and the building elevations in Exhibit "D", attached herein, and with modifications to match the Sub-area access points in the Specific Plan,.
3. That landscaping and irrigation plans be submitted for review by Planning Division staff with building permits.
4. That an evergreen hedge, mound, or wall approximately 3 foot-high be installed along drive-thru lanes and parking lots which are directly adjacent to Caldwell Avenue or Demaree Street, consistent with the Specific Plan requirements.
5. That a seven foot high block wall be constructed along the southern property line of Sub-area "B" from approximately 25 feet behind the right-of-way line on Demaree to approximately 25 feet past the east end of the house on the adjacent property. An additional wood fence, minimum height six foot, will be required to define the property line and reduce headlight glare across the balance of the property to the south where a fence of this description may not already be in place.
6. That this conditional use permit shall not be deemed approved if Specific Plan Amendment No. 2006-02 is not approved by the City Council.
7. That the Caldwell and Demaree medians and striping shall be installed by the developer as a part of the pharmacy/Phase One building permit (such medians to control/eliminate left turns into the subject site). This condition shall be met prior to the first building occupancy in Sub-area "B".
8. That all other city codes and ordinances be met.
9. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2006-32, prior to the issuance of any building permits for this project.

RESOLUTION No. 2006-91

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA FOR SPECIFIC PLAN AMENDMENT NO. 2006-02, RECOMMENDING APPROVAL OF THE PHASED DEVELOPMENT OF SUB-AREA "B", AND DENIAL OF THE REQUEST TO ALLOW THE RELOCATION OF AN ACCESS POINT ON CALDWELL AVENUE, THE SITE IS LOCATED AT THE SOUTHEAST CORNER OF CALDWELL AVENUE AND DEMAREE STREET

WHEREAS, the Taylor Group has requested a Specific Plan Amendment to allow the phased development of Sub-area "B", and the relocation of an access point on Caldwell Avenue in the Caldwell /Demaree Specific Plan (Community Commercial Zone). The site is located at the southeast corner of Caldwell Avenue and Demaree Street. APNs: 126-030-033/034/035/036/014/015; and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice held a public hearing before said Commission on September 11, 2006; and

WHEREAS, the Planning Commission of the City of Visalia finds the requested relocation of the access drive on Caldwell Avenue to not be consistent with the Demaree/Caldwell Specific Plan, and the requested phased development of Sub-area "B" to be in accordance with Chapter 12.04 of the Ordinance Code of the City of Visalia based on evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, an Initial Study was prepared which disclosed that no significant environmental impacts would result from this project; and

NOW, THEREFORE, BE IT RESOLVED that Negative Declaration No. 2006-67 was prepared consistent with the California Environmental Quality Act and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia recommends approval of the phased development of Sub-area "B", and denial of the requested relocation of the Caldwell Avenue access point based on the following specific findings and based on the evidence presented:

- 1) That the phased development of the sub-area can be done in an orderly fashion as shown in Exhibit "A", attached herein.
- 2) That the proposed Specific Plan Amendments, as amended by staff, are consistent with the purpose and intent of the Demaree Caldwell Specific Plan, Zoning Ordinance , and the Land Use Element.
- 3) That the proposed relocation of the access drive from the eastern property line to the proposed location is not supported based upon the following:
 - Potential conflicting turning maneuvers from vehicles exiting the site east-bound on Caldwell Avenue while west-bound vehicles are trying to cross the east-bound lanes at the median break 150 feet away.

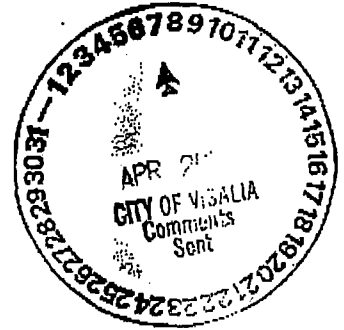
- The drive-thru would substantially contribute to potential peak hour traffic conflicts on/off the site due to vehicles exiting the drive-thru having to cross oncoming traffic is that is entering the site from Caldwell Avenue, which would typically be most problematic at the peak hours. The access analysis does not cover the potential of on-site conflicts from the proposed drive-thru.
 - The proposed two access drives have approximately 80 feet of separation between the radius returns which does not meet City Standards for 500 feet between the access points. It should be noted that the existing Specific Plan does not have the required separation for the approved access points on Caldwell Avenue in Sub-area "B".
 - Two close access points gives bicycles and pedestrians two areas of conflict with vehicles utilizing the site.
- 4) That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
 - 5) That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia recommends approval of the phased development of Sub-area "B" subject to the following conditions and in accordance with the terms of this resolution under the provisions of Section 12.04.010 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the phasing of the proposed project be substantially consistent with Exhibit "A".
2. That the adopted vehicle circulation and access plan (figure 9, Caldwell /Demaree Specific Plan) be met.
3. That all other requirements of the Caldwell /Demaree Specific Plan be met.



MEETING DATE 4/5/2006
SITE PLAN NO. 06-002
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.



Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with
☐ Planning ☐ Engineering prior to resubmittal plans for Site Plan Review.
☐ Solid Waste ☐ Parks and Recreation ☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☐ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 8 a.m. and 5 p.m., Monday through Friday.

☒ Your plans must be reviewed by:

<input type="checkbox"/> CITY COUNCIL	<input type="checkbox"/> REDEVELOPMENT
<input checked="" type="checkbox"/> PLANNING COMMISSION	<input type="checkbox"/> PARK/RECREATION
<input type="checkbox"/> HISTORIC PRESERVATION	<input type="checkbox"/> OTHER _____

☐ **ADDITIONAL COMMENTS** _____

If you have any questions or comments, please call Dennis Lehman, Building Official, Site Plan Committee member at 713-4495.

Site Plan Review Committee





ITEM NO: 9

DATE: APRIL 5, 2006

SITE PLAN NO:

06-002 RESUBMITTAL

PROJECT:

LONGS DRUG STORE

APPLICANT TITLE:

COMMERCIAL RETAIL FOR LONGS ON 8.5 ACRES (CCM ZONED)

LOCATION TITLE:

THE TAYLOR GROUP ARCHITECTS, CLEMENTS DEV. (PROP. OWNER)

APN TITLE:

SE CORNER OF CALDWELL & DEMAREE

126-030-033, 034, 035, 036.

- ☒ Submit 3 sets of engineered plans and 2 sets of calculations.
- ☐ Indicate abandoned wells, septic systems and excavations on construction plans.
- ☒ Meet city and state requirement for the physically handicapped. *PARKING ACCESS*
- ☐ Submit 3 sets of plans signed by architect or engineer. Must comply with 2001 California Building Code Sec. 2320 for conventional light-frame construction or submit 2 sets of engineered calculations.
- ☐ Maintain sound transmission control between units.
- ☐ Maintain fire-resistive requirements at property lines.
- ☐ Obtain required permits for Air Pollution Board, Tulare County.
- ☐ Location of cashier must provide clear view of gas pump island.
- ☐ Calculations of free-standing carport.
- ☒ Treatment connection charge to be assessed based on use. *RETAIL @ \$1.50 per 1000 ft*
- ☒ Must comply with state energy requirements.
- ☒ Plans must be approved by the Tulare County Health Department. *FOOD STORAGE & SERVICE*
- ☒ Common areas must comply with requirements for access by the handicapped.
- ☐ Project is located in flood zone _____. Minimum finished floor elevations requires _____.
- ☐ All accessible units required to be adaptable for the physically handicapped.
- ☐ Acceptable as submitted.
- ☐ Arrange for an in-site inspection.
- ☐ Hazardous materials report.
- ☐ A demolition permit is required.
- ☒ School Development fees. Commercial \$0.36 per square foot. Residential \$2.93 per square foot.
- ☐ Park Development fees \$ _____, per unit collected with building permits.
- ☐ Existing address must be changed to be consistent with city address policy.

Site Plan Review Comments For:

Visalia Fire Department
Doyle Sewell, Fire Marshal
309 S. Johnson
Visalia, CA 93291
559-713-4486 office
559-713-4808 fax

ITEM NO: 9**SITE PLAN NO:****PROJECT:****APPLICANT TITLE:****LOCATION TITLE:****APN TITLE:****DATE:** APRIL 5, 2006

06-002 RESUBMITTAL

LONGS DRUG STORE

COMMERCIAL RETAIL FOR LONGS ON
8.5 ACRES (CCM ZONED)THE TAYLOR GROUP ARCHITECTS,
CLEMENTS DEV. (PROP. OWNER)SE CORNER OF CALDWELL &
DEMAREE

126-030-033, 034, 035, 036,

The following comments are applicable when checked:

- ☐ No comments at this time for this project.
- ☐ Refer to previous comments dated
- ☐ No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire protection requirements.
- ☒ Address numbers must be placed on the exterior of the building in such a position as to clearly and plainly visible from the street. Numbers will be at least six inches (6") high and shall be of a color to contrast with their background.
- ☐ No additional fire hydrants are required for this project; however, additional fire hydrants may be required for any future development.
- ☒ There is/are 6 fire hydrants required for this project.
- ☒ The turning radius for emergency fire apparatus is 20 feet inside diameter and 35 feet outside diameter. Ensure that the turns identified to you during site plan comply with the requirements. An option is a hammer-head constructed to City standards.
- ☒ An access road is required and shall be a minimum of 20 feet wide. The road shall be an all-weather driving surface accessible prior to and during construction.
- ☐ A fire lane is required for this project. The location will be given to you during the site plan meeting.
- ☒ A Knox Box key lock system is required. Applications are available at the Building Department counter.
- ☐ The security gates, if to be locked, shall be locked with a typical chain and lock that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system. Applications are available at the Building Department counter.

- ☐ That portion of the building that is built upon a property line shall be constructed as to comply with Section 503.4 and Table 5-A of the California Building Code.
- ☐ Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system.
- ☐ If you handle hazardous material in amounts that exceed the exempt amounts listed on Table 3-D of the California Building Code, you are required to submit an emergency response plan to the Tulare County Health Department. Prior to the building final inspection, we will want a copy of the plan and any Material Safety Data Sheets.
- ☒ An automatic fire sprinkler system will be required for this building. A fire hydrant is required within 50 feet of the fire department connection. The fire hydrant, fire department connection and the PIV valve should be located together and in the front of the building, if possible.
- ☐ An automatic fire extinguishing system for protection of the kitchen grease hood and ducts is required.
- ☒ Fire Department Impact Fee - \$1393.49 per acre.
- ☒ Fire Department Permit Fee - complete application during Building Department permit process.
- ☐ Special comments: _____

Doyle Sewell, Fire Marshal

QUALITY ASSURANCE DIVISION
SITE PLAN REVIEW COMMENTS

ITEM NO: 2 DATE: APRIL 5, 2006
SITE PLAN NO: 08-002 RESUBMITTAL
PROJECT: LONGS DRUG STORE
APPLICANT TITLE: COMMERCIAL RETAIL FOR LONGS ON
8.5 ACRES (CCM ZONED)
LOCATION TITLE: THE TAYLOR GROUP ARCHITECTS,
CLEMENTS DEV. (PROP. OWNER)
APN TITLE: SE CORNER OF CALDWELL &
DEMAREE
126-030-033, 034, 035, 036,

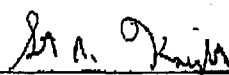
YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS

- ☐ PRETREATMENT PROGRAM _____
☐ RESOURCE CONSERVATION AND RECOVERY ACT (RCRA) _____
☐ SAND AND GREASE TRAP - 3 COMPARTMENT _____
☐ GREASE TRAP _____ LB. CAPACITY _____
☐ GARBAGE GRINDER - 3/4 HP. MAXIMUM _____
☐ SUBMISSION OF A DRY PROCESS DECLARATION _____
☒ NO SINGLE PASS COOLING WATER IS PERMITTED _____
☐ OTHER _____

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA
PUBLIC WORKS DEPARTMENT
QUALITY ASSURANCE DIVISION
7579 AVE. 288 336 N. BEN MADDOX WAY
VISALIA, CA 93292 93277


AUTHORIZED SIGNATURE

3-31-06

DATE

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4177

ITEM NO: 9

DATE: APRIL 5, 2006

SITE PLAN NO:

06-002 RESUBMITTAL

PROJECT:

LONGS DRUG STORE

APPLICANT TITLE:

COMMERCIAL RETAIL FOR LONGS ON
8.5 ACRES (CCM ZONED)

LOCATION TITLE:

THE TAYLOR GROUP ARCHITECTS,
CLEMENTS DEV. (PROP. OWNER)

APN TITLE:

SE CORNER OF CALDWELL &
DEMAREE

CHECK MARK INDICATES APPLICABILITY

☐

No comments.

☐

Same comments as _____

☒

Revisions required prior to submitting final plans. See comments below.

☐

Resubmittal required. See comments below.

☐

Individual can refuse service will be provided curbside.

☐Backyard fence gates must be a minimum of 32 inches in width for backyard
automation container storage.☒Commercial bin refuse service will be provided at bin enclosure built according
to city standards. All refuse enclosures must be a () T - 1 or () T - 2☐

Type of refuse service not indicated.

☐

Location of bin enclosure not acceptable. See comments below.

☐

Bin enclosure not to city standards double.

☐

Inadequate number of bins to provide sufficient service. See comments below.

☐

Drive approach too narrow for refuse trucks access. See comments below.

☐Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.☒

Paved areas should be engineered to withstand a 55,000 lb. refuse truck.

☒

Bin enclosure gates are () required (x) optional.

Hammerhead turnaround must be built per city standards.

☐

Cul - de - sac must be built per city standards.

☒Bin enclosures are for city refuse containers only. Grease drums or any other
items are not allowed to be stored inside bin enclosures.☒

Area in front of refuse enclosure must be marked off indicating no parking

comments:

Need to have concrete slab in front of enclosure. The width of the enclosure by
10 feet in front, by inches in depth.

Satisfied with angle of bin enclosure.

Name Frank Rodriguez

Phone # 713-4338

**BUILDING PLAN REQUIREMENTS
ENGINEERING/TRAFFIC DIVISION**

<input type="checkbox"/> Andrew Benelli	713-4340
<input type="checkbox"/> Doug Damko	713-4268
<input checked="" type="checkbox"/> Ken McSheehy	713-4447
<input type="checkbox"/> Peter Spiro	713-4256
<input type="checkbox"/> Norm Goldstrom	713-4638
<input type="checkbox"/> Greg Dais	713-4419
<input type="checkbox"/> Steven Son	713-4259

ITEM NO: 9 DATE: APRIL 5, 2006

SITE PLAN NO.: 06-002 2ND RESUBMITTAL
 PROJECT: COMMERCIAL RETAIL FOR LONGS DRUG WITH
 15,000 S.F. BLDG ON 8.5 ACRES (CCM ZONED)
 (REFER 01-020)
 APPLICANT: THE TAYLOR GROUP ARCHITECTS, CLEMENTS
 DEV. (PROP OWNER)
 LOCATION: SE CORNER CALDWELL & DEMAREE
 APN: 126-030-033, 034, 035, 036, 015, 014

SITE PLAN REVIEW COMMENTS

- ☒ **REQUIREMENTS** (indicated by checked boxes)
- ☐ Install curb return with ramp, with _____ radius;
- ☐ Install curb; ☐ gutter
- ☒ Drive approach size: **36' min** ☒ Use radius return; **City standard C-19 major commercial drive approaches with 35' radius returns.**
- ☒ Sidewalk 6' width; ☒ 10' parkway width at **Caldwell & Demaree**
- ☐ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- ☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- ☒ Right-of-way dedication required; **Need an easement for S/W and utilities on Demaree and Caldwell for a minimum of 16' behind gutter line. See comment #8**
- ☒ Bond required prior to issuing building permit;
- ☒ **City Encroachment Permit Required.**
- Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit.
- ☐ ~~Caltrans Encroachment Permit required.~~ ☐ ~~Caltrans comments required prior to issuing building permit.~~
- ~~Contacts: Al Dias (planning) 488-7306;~~
- ☒ Grading & Drainage plan required. ☒ Prepared by registered civil engineer or project architect. ☒ Storm Water Discharge permit is required. ☒ All elevations shall be based on the official City of Visalia datum. Storm run-off from the site needs to be directed to the City's storm drainage system.
- ☐ ~~Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.~~
- ☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- ☒ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- ☒ Install landscape curbing (typical at parking lot planters).
- ☒ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☒ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☐ Provide "R" value tests: _____ each at _____
- ☐ Written comments required from ditch company
- Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide _____ wide riparian dedication from top of bank.
- ☒ Show Oak trees with drip lines and adjacent grade elevations. ☒ Protect Oak trees during construction in accordance with City requirements.
- ☒ A permit is required to remove oak trees. Contact David Pendergraft at 713-4295 for an Oak tree evaluation or permit to remove. ☒ A pre-construction conference is required.

- ☐ Ponding basin required (: maximum slope); ☐ Fence required; ☐ Provide access to bottom for maintenance
- ☒ Developer shall relocate existing utility poles and/or facilities.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer.
- ☒ Dust control is required on site during construction and for all material excavated, graded, and/or transported.
- ☐ Comply with prior comments. ☒ Resubmit with additional information. ☐ Redesign required. ☐ Summary of Estimated Engineering Fees is attached.

Additional Comments:

1.) Site is Sub Area 'B' of the Demaree/Caldwell Specific Plan and needs to comply with same.

2.) Show cross section of Demaree and Caldwell to include proposed and existing public improvements on both sides of the streets with dimensions.

3.) The specific Plan allows two drive approaches to sub area B one each on Caldwell & Demaree. The easterly approach on Caldwell needs to align with the proposed west bound left turn worm into site. The southerly approach on Demaree is south bound left turn worm into site. Any other approaches will require amending the Specific Plan and are right in/out approaches and are to be protected from left in/out by median islands.

4.) Provide a master plan for sanitary sewer, storm drain and utilities for Sub Area 'B'.

5.) Provide an easement for access south to Sub Area 'C' per Specific Plan.

7.) Show proposed phasing of the project if any.

8.) Provide right of way dedication of 10' behind gutter plus a 6' sidewalk easement on Caldwell and Demaree.

9.) Move the south drive approach on Demaree north to allow for a full 35' radius return on the south side of drive.

10.) The proposed left turn median cuts in Caldwell and Demaree need to be designed so as not interfere with the turn pockets at the intersection.

11.) This sub area is subject to an assesement in the amount of \$11,569.81 for the signal located at Demaree/Packwood.

SUMMARY OF ESTIMATED ENGINEERING FEESSite Plan No: 06-002 2nd Resubmittal

Date: 4/05/06

Summary of Estimated Engineering Fees: (Preliminary estimate only based on current fee schedule. The fee schedule is periodically adjusted and final fees will be based on the fee schedule at time of issuance of building permit.)

(Basis of fee calculation: Community Center- Shopping Center under 100,000 s.f.)

(Credits:)

☐ Additional Information is required prior to estimating applicable Engineering Fees.

- ☒ Parking Lot Plan Check & Inspection
- ☒ Encroachment Permit 3.1% of Estimated Construction Cost for Public Improvements n/a n/a
- ☒ Transportation Impact Fee \$15,355.21/1000 s.f.
- ☒ Trunk Line Capacity Fee \$17.48/1000 s.f.
- ☒ Sewer Front Foot Fee \$32.45/ft.
- ☒ Storm Drain Fee \$4205.70/ac

☐ Park Acq/Dev Fee☐ Northeast Fees

- ☒ Waterways Acquisition Fee \$3868.00/ac
- ☒ Public Safety Impact Fee: Police \$6368.75/ac
- ☒ Public Safety Impact Fee: Fire \$1263.49/ac
- ☒ Public Facility Impact Fee \$306.46/1000 s.f.

Reimbursement:

- 1.) Reimbursement is available for the development of arterial/collector streets identified in the City's Circulation Element. The developer will be reimbursed for all costs associated with the development of these streets from curb to curb plus street lights. The specific details regarding reimbursement are identified in the City's reimbursement policy. The right of way dedication shall be reimbursed in the form of a Transportation Impact Fee credit. The credit will be calculated per lot and applied at the time of building permit issuance.
- 2.) Reimbursement is available to a developer for storm drain trunk lines and sewer trunk lines identified in the City's Storm Water Master Plan and Sewer System Master Plan. The developer will be reimbursed for 100% the cost of storm drain trunk lines. The developer will be reimbursed for 100% the cost of sewer trunk lines not needed by the developer. If the developer needs a sewer line and is required to enlarge the sewer line in accordance with the Sewer System Master Plan, then the developer will be reimbursed for the differential cost. The City will reimburse the developer for design and construction costs associated with the installation of these trunk lines.


 Ken McSheehy

TRAFFIC SAFETY

☒ Eric Bons 713-4350
☐ Myron Rounsfull 713-4412
☐ Doug Damko 713-4268

ITEM NO: 009

DATE: APRIL 5, 2006

SITE PLAN NO.:
PROJECT:06-002 RESUBMITTAL
LONGS DRUG STORE
COMMERCIAL RETAIL FOR LONGS ON 8.5
ACRES (CCM ZONED)

APPLICANT:

THE TAYLOR GROUP ARCHITECTS, CLEMENTS
DEV. (PROP. OWNER)

LOCATION:

SE CORNER OF CALDWELL & DEMAREE

APN:

126-030-033, 034, 035, 036

SITE PLAN REVIEW COMMENTS**THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY**☐ No Comments☒ See Previous Site Plan Comments☐ Install Street Light(s) on Marbelite Standards.☐ Install Street Name Blades at Locations.☐ Install Stop Signs at Locations.**Additional Comments:**
Eric Bons

City of Visalia Police Department

303 S. Johnson St.
Visalia, Ca. 93292
(559) 713-4370

ITEM NO: 9

DATE: APRIL 5, 2006

SITE PLAN NO:

06-002 RESUBMITTAL

PROJECT:

LONGS DRUG STORE

APPLICANT TITLE:

COMMERCIAL RETAIL FOR LONGS ON
8.5 ACRES (CCM ZONED)

LOCATION TITLE:

THE TAYLOR GROUP ARCHITECTS,
CLEMENTS DEV. (PROP. OWNER)

APN TITLE:

SE CORNER OF CALDWELL &
DEMAREE


126-030-033, 034, 035, 036,

Site Plan Review Comments

- ☒ No Comment at this time.
- ☒ Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- ☒ Public Safety Impact fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.

- ☐ Not enough information provided. Please provide additional information pertaining to:
- ☐ Territorial Reinforcement: Define property lines (private/public space).
- ☐ Access Controlled / Restricted etc:
- ☐ Lighting Concerns:
- ☐ Landscaping Concerns:
- ☐ Traffic Concerns:
- ☐ Surveillance Issues:
- ☐ Line of Sight Issues:
- ☐ Quality of life / other Concerns:


Angie de Alba
Visalia Police Department
Crime Prevention Through Environmental Design

SITE PLAN REVIEW COMMENTS**PLANNING DIVISION****Andrew Chamberlain (559) 713-4003 / Cass Cook**

Date: April 5, 2006

Site Plan No.: 06-02 C
Project: Commercial Retail for Longs with a 15,800 Sq. Ft. Building on 8.5 Acres
Applicant: The Taylor Group Architects, Clements Development (Property Owner)
Location: SE Corner of Caldwell & Demaree
APN: 126-030-033, 034, 035, 036, 015, 014

General Plan: CCM-Community Commercial (Specific Plan Required)
Existing Zoning: CCM
Specific Area Plan: Caldwell/Demaree Specific Plan

PROJECT SPECIFIC INFORMATION:

1. The site plan for phase 1 shows 66 spaces, 70 spaces are required.
2. A landscaping well is required for every 10 spaces. The landscaping well on the west side of the Longs pad needs to be moved north one space. A well should be added to the 3,200 square foot pad in the northeast section of the site plan. A well should be added to the 12,100 square foot pad in the southwest section of the site.
3. The plan for the entire site shows 401 parking stalls, only 355 stalls are required. We would like to see the parking provided not to exceed the required amount of 355 stalls.
4. The 6,000 square foot pad at the north of the site should face Caldwell if possible.
5. Increased pedestrian connectivity is suggested for this site. Pedestrian access from the 6,000 sf pad and the 12,100 sf pad to the sidewalk is suggested. Pedestrian paths between the pads are also recommended.
6. A landscaped pedestrian walkway running the length of the parking aisle is preferred for the parking aisle running north/south in the center of the site plan over the square parking islands.

See Previous Comments Below

Note: Changes in the site plan may add new comments or make some of the previous comments below not applicable.

Site Plan Review Comments 02/15/06

7. This project as presented in this site plan will require an amendment to existing SP for the addition of new access drive on Caldwell Avenue. At this time staff does not support the proposed added access drive due to its proximity to the drive to the west and location on a high-speed arterial street. The two points produce conflicting maneuvers to enter and exit the site which may be injurious to the public health and safety.
8. The elimination of the western access drive on Caldwell Avenue would eliminate the need for a Specific Plan Amendment, and only a CUP for the drive-thru would be required.
9. Provide Tentative building elevations for the drug store at site Plan Review if possible. Final building elevations are required with the CUP application for the drive thru. The first building sets the required common architectural theme for all the other buildings in the sub-area.
10. Provide connecting sidewalks from the interior of the site to the perimeter public sidewalks.
11. Landscaping along the entire street frontages will be required along Caldwell and Demaree.
12. The drive-thru pad next to Demaree needs to be reconfigured so the drive-thru does not exit anywhere near the Demaree access point, and that there is no stacking near the Demaree access point.

13. The 3 foot high wall or ever-green hedge adjacent to drive-thru lanes and parking along the public streets needs to be located directly in front of the lane or stalls, not at property line as indicated by Keynote No. 23.
14. See Solid Waste comments.
15. The proposed wall along the southern property line needs to be detailed. Staff does not support a continuous wall along the entire south property line 12 feet in height. It may be appropriate to vary the wall height. The access point and approximately 20 feet on either side of it needs to be planned for a removable wall section or even a different type of fencing material. Typically a wall over the maximum 7 feet will require a variance or similar action. Staff will work with the applicant, with input from the property owner to the south, in refining the wall. At the rear of the property, east end, a standard wall seven feet in height may be more appropriate.
16. The "Major Retail" box, while only a typical, should include a stem wall along the loading dock to contain noise and be planned for trash compaction to occur inside the building, and similar actions so that community noise standards can be met.
17. Show the median in Caldwell Avenue
18. While this plan does show phasing by a cross hatch of the areas to be developed with the drug store, please include if the existing residential unit on Caldwell will remain during the first phase of this project, if so, how will it be protected from the construction and subsequent commercial operations.
19. The site is eligible for a single monument sign which may be of a reader board style, dedicated to several users or simply giving the center name.
20. Typically, vacant pads are required to be turfed with grass, if it is anticipated that there will be eminent action on most of the site this may be discussed with staff to limit the turf areas. Turf may be in the form of hydro-seeding and agricultural sprinklers for ease of installation and limited cost.

The project will require the following:

- Master Plan - for Sub-area "B" (Draft done for area)
- Conditional Use Permit for drive-thru
- Approved Site Plan
- Phasing Plan
- Sign Program
- Major Amendment to Specific Plan for addition of 2nd access point on Caldwell nearest Longs and 2nd access point on Demaree nearest Longs
- South Cross access needs to be included
- The correct location of the 30 foot setbacks relative to the property line after dedications
- Trash enclosures for all areas need to be provided

The first building will set the architectural character for the sub-area. Attention should be paid to providing a character which can be reflected in various sized buildings. The style is not a lock-in to exact specifics of a particular building, but an overall design, materials and composition which can be shared between buildings in the sub-area and tailored to meet the needs of individual users.

Site Plan Review Comments 01/11/06

- Pads with drive-thru's should be rotated so the drive-thru's are not adjacent to Caldwell or Demaree.
- Staff recommends that the walkway along the western exposure of Longs be shaded.
- A trash enclosure needs to be added near the northeast pads.
- The southeast trash enclosure needs to face west. (See City enclosure standards)
- No light or glare should encroach on the area to the south.

- A block wall should be provided along the southern property line, see attached letter. The height of the wall shall be determined as buildings along the southern property line are finalized. Expect the wall to be a minimum of 8', but it may exceed 10'. The Specific Plan reads as follows. "Sub-area B will develop as one unit. If Sub-area B develops in advance of Sub-area C, loading and unloading facilities shall be designated in a manner to protect the existing residences in Sub-area C." (Caldwell/Demaree Specific Plan, page 40)
- Setbacks need to be adjusted to be measured 10' from future right of way dedication.
- An 80 sq. ft. minimum landscape well is required for every 10 contiguous parking stalls.

Development Standards

Setbacks

The proposed project lies within Design District "J" and is subject to the Caldwell/Demaree Specific Plan standards:

Design District: "A" (Caldwell/Demaree Specific Plan)

Maximum Building Height: Single story

Minimum Setbacks:

	<u>Building</u>	<u>Landscaping</u>
• Caldwell Ave:	30 feet	30 feet
• Side:	0 feet	5 feet*
• Demaree Ave:	30 feet	30 feet
• Side abutting residential zone:	15 feet	5 feet
• Rear:	0 feet	5 feet*
• Rear abutting residential zone:	15 feet	5 feet

*(Except where building is on property line)

The 30 foot setback is measured from the property line, after any dedications. The site plan does not appear to reflect the correct location of the property line and 30 foot setback.

Phasing & Development

Sub-area "B" shall be developed as one unit. If it develops in advance of Sub-area "C", loading/unloading facilities shall be designated so as to protect the existing residences.

The western access drive on Caldwell is not provided for in the specific plan. The relocation of one of the access points on Caldwell is a **minor** amendment; the addition of a new access point on Caldwell is a **major** amendment. The northern access drive on Demaree is not provided for in the specific plan. The relocation of one of the access points on Demaree is a **minor** amendment; the addition of a new access point on Demaree is a **major** amendment.

Parking

Parking will be required at a ration of 1 stall per 225 sq. ft. of gross building area (Shopping Center Major). Parking and access requirements need to be met on a building by building basis when permits are to be issued.

Compact stalls can be a maximum of 30 percent of the total required parking

Provide shared parking/access agreements. The agreements/easements will need to be approved and recorded prior to issuance of building permits.

No parking shall be permitted in required front/rear/side yard.

Parking lot shall be screened from view by a 3-foot tall solid wall or screens when located adjacent to a public street.

Landscaping

All landscape areas are to be protected with 6-inch concrete curbs.

An 80 sq. ft. minimum landscape well is required for every 10 contiguous parking stalls.

All parking lots to be designed to provide a tree canopy for shading in the hot season and sunlight in the winter.

Detailed landscape and irrigation plans are required as a part of the building permit package. Identify all landscaped areas on the site.

Fencing and Screening

All roof-mounted equipment must be screened.

Provide screened trash enclosure with solid screening gates.

Lighting

All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.

The light poles may be a maximum of 15 feet high with the light element to be completely recessed into the can.

Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.

Signage

A sign program needs to be considered at the Site Plan Review level. As a Sub-area and uniform commercial center, a single monument sign may be allocated for the entire Sub-area.

All signs require a separate building permit.

Comments

While the Specific Plan layouts were generalized, it should be noted that they provided protection of the primary access points to the site. Other requirements of the Specific Plan are required to be met. This would include, but not be limited to, signage, uniform architecture, delineation of the shared access points and parking areas along with the requirement that shared access and maintenance agreements be reviewed and approved by the City of Visalia and recorded prior to the issuance of building permits for structures on the site.

Signature





CITY OF VISALIA
SOLID WASTE DIVISION
 336 N. BEN MADDOX
 VISALIA CA. 93291
 713 - 4177

ITEM NO: 9

DATE: APRIL 5, 2006

SITE PLAN NO:

06-002 RESUBMITTAL

PROJECT:

LONGS DRUG STORE

APPLICANT TITLE:

COMMERCIAL RETAIL FOR LONGS ON
8.5 ACRES (CCM ZONED)

LOCATION TITLE:

THE TAYLOR GROUP ARCHITECTS,
GLEMENTS DEV. (PROP. OWNER)

APN TITLE:

SE CORNER OF CALDWELL &
DEMAREE

126-030-033, 034, 035, 036,

CHECK MARK INDICATES APPLICABILITY

☐

No comments.

☐

Same comments as _____

☒

Revisions required prior to submitting final plans. See comments below.

☐

Resubmittal required. See comments below.

☐

Individual can refuse service will be provided curbside.

☐Backyard fence gates must be a minimum of 32 inches in width for backyard
automation container storage.☒Commercial bin refuse service will be provided at bin enclosure built according
to city standards. All refuse enclosures must be a () T - 1 or () T - 2☐

Type of refuse service not indicated.

☐

Location of bin enclosure not acceptable. See comments below.

☐

Bin enclosure not to city standards double.

☐

Inadequate number of bins to provide sufficient service. See comments below.

☐

Drive approach too narrow for refuse trucks access. See comments below.

☐Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.☒

Paved areas should be engineered to withstand a 55,000 lb. refuse truck.

☒

Bin enclosure gates are () required (X) optional.

Hammerhead turnaround must be built per city standards.

☐

Cul - de - sac must be built per city standards.

☒Bin enclosures are for city refuse containers only. Grease drums or any other
items are not allowed to be stored inside bin enclosures.☒

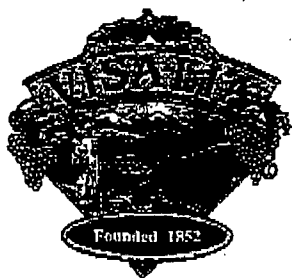
Area in front of refuse enclosure must be marked off indicating no parking

comments: Need to have concrete slab in front of enclosure. The width of the enclosure by
10 feet in front, by inches in depth.

Satisfied with angle of bin enclosure.

Name Frank Rodriguez

Phone # 713-4338



City of Visalia Fax

Community Development

Date: 5/1/2006

Number of Pages (Including this page): 19

To: Brian

Taylor Group

Phone: 437-5000

Fax: 437-5005

From: Sandra Clord

Support Services Assistant

Phone: (559) 713-4440

Fax: (559) 713-4814

Message: The fee schedule is as follows: CUP \$3,123.00, Specific Plan Amendment \$3,960.00 and if necessary Negative Dec. / Initial Study fee \$1,993.00. If you have any questions please call me. Thank you!

*If you do not receive all pages, please let us know. 315 East Acequia Avenue,
Visalia, CA 93291 • (559) 713-4440 • Fax (559) 713-4814*

CITY OF VISALIA
315 E. ACEQUIA AVENUE
VISALIA, CA 93291

**NOTICE OF A PROPOSED
NEGATIVE DECLARATION**

Project Title: Conditional Use Permit No. 2006-32, Specific Plan Amendment No. 2006-02

Project Description: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a drive-thru for a pharmacy. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

Specific Plan Amendment No. 2006-02 is a request to relocate an access drive on Caldwell Avenue, and to allow phased development of Sub-Area "B".

Contact Person: Andrew Chamberlain, Senior Planner. Phone: (559) 713-4003.

Time and Place of Public Hearing: A public hearing will be held before the Planning Commission on August 14, 2006, at 7:00 p.m. in the City Hall Council Chambers located at 707 W. Acequia, Visalia, CA.

Pursuant to City Ordinance No. 2388, the Environmental Coordinator of the City of Visalia has reviewed the proposed project described herein and has found that it will not result in any significant effect upon the environment because of the reasons listed below:

Reasons for Negative Declaration: Initial Study No. 2006-67 has not identified any significant, adverse environmental impacts that may occur because of the project. Copies of the initial study and other documents relating to the subject project may be examined by interested parties at the Planning Division in City Hall East, at 315 E. Acequia Ave., Visalia, CA.

Comments on this proposed Negative Declaration will be accepted until August 14, 2006.

Date: _____

2-13-06

Signed: _____

Paul Scheibel, AICP
Environmental Coordinator
City of Visalia

NEGATIVE DECLARATION

Project Title: Conditional Use Permit No. 2006-32

Project Description: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq. ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Sub area B of the Demaree/Caldwell Specific Plan area. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

Project Facts: Refer to Initial Study for project facts, plans and policies, discussion of environmental effects and mitigation measures, and determination of significant effect.

Attachments:

Initial Study	(X)
Environmental Checklist	(X)
Maps	()
Mitigation Measures	()
Letters	()

DECLARATION OF NO SIGNIFICANT EFFECT:

This project will not have a significant effect on the environment for the following reasons:

- (a) The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory.
- (b) The project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.
- (c) The project does not have environmental effects which are individually limited but cumulatively considerable. Cumulatively considerable means that the incremental effects of an individual project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.
- (d) The environmental effects of the project will not cause substantial adverse effects on human beings, either directly or indirectly.

This Negative Declaration has been prepared by the City of Visalia Planning Division in accordance with the California Environmental Quality Act of 1970, as amended. A copy may be obtained from the City of Visalia Planning Division Staff during normal business hours.

APPROVED
Paul Scheibel, AICP
Environmental Coordinator

By: 

Date Approved: _____

Review Period: 20 days

INITIAL STUDY

I. GENERAL

A. Description of the Project: Conditional Use Permit No. 2006-32 is a request by The Taylor Group to allow a retail center with seven buildings totaling 81,037 sq. ft., including three drive-thru facilities, to be built in multiple phases on 8.5 acres within Sub area B of the Demaree/Caldwell Specific Plan area. The site is located on the southeast corner of Caldwell Avenue. APN: 126-030-033/034/035/036.

B. Identification of the Environmental Setting: The site is currently vacant. The surrounding zoning and land uses are as follows:

North: CCM (Community Commercial)
South: OG (Office Garden)
East: CCM (Community Commercial)/ future community commercial center
West: CCM (Community Commercial)

Fire and police protection services, street maintenance of public streets, refuse collection, and wastewater treatment will be provided by the City of Visalia upon development of the area.

C. Plans and Policies: The General Plan Land Use Element (LUE) designates the site as Community Commercial. The site is zoned C-CM (Community Commercial). The proposed project is consistent with the Land Use Element of the General Plan.

II. ENVIRONMENTAL IMPACTS

No significant adverse environmental impacts have been identified for this project. The City of Visalia Land Use Element and Zoning Ordinance contain land use mitigation measures that are designed to reduce/eliminate impacts to a level of non-significance.

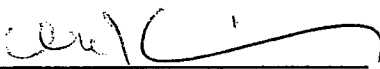
III. MITIGATION MEASURES


None. The City of Visalia Zoning Ordinance contains guidelines, criteria, and requirements for the mitigation of potential impacts related to light/glare, visibility screening, noise, and traffic/parking to eliminate and/or reduce potential impacts to a level of non-significance.

IV. PROJECT COMPATIBILITY WITH EXISTING ZONES AND PLANS

The project is compatible with the General Plan and Zoning Ordinance as the project relates to surrounding properties.

V. NAME OF PERSON WHO PREPARED INITIAL STUDY


Andy Chamberlain
Senior Planner


Paul Scheibel, AICP
Environmental Coordinator

INITIAL STUDY ENVIRONMENTAL CHECKLIST

Name of Proposal Conditional Use Permit No. 2006-32

NAME OF PROPONENT: Harold Clements
Address of Proponent: 891 S. McAuliff
Visalia, CA 93292
Telephone Number: (559)627-0537
Date of Review June 22, 2006

NAME OF AGENT: Russel F. Taylor
Address of Agent: 978 W. Alluvial Ave. #107
Telephone Number: (559)437-5000
Lead Agency: Taylor Group Architects

The following checklist is used to determine if the proposed project could potentially have a significant effect on the environment. Explanations and information regarding each question follow the checklist.

1 = No Impact 2 = Less Than Significant Impact
3 = Less Than Significant Impact with Mitigation Incorporated 4 = Potentially Significant Impact

I. AESTHETICS

Would the project:

- 1 a) Have a substantial adverse effect on a scenic vista?
- 1 b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?
- 1 c) Substantially degrade the existing visual character or quality of the site and its surroundings?
- 2 d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

II. AGRICULTURAL RESOURCES

Would the project:

- 1 a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency to non-agricultural use?
- 1 b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?
- 1 c) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to nonagricultural use?

III. AIR QUALITY

Would the project:

- 1 a) Conflict with or obstruct implementation of the applicable air quality plan?
- 1 b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?
- 2 c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?
- 1 d) Expose sensitive receptors to substantial pollutant concentrations?
- 1 e) Create objectionable odors affecting a substantial number of people?

IV. BIOLOGICAL RESOURCES

Would the project:

- 1 a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 1 b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 1 c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?
- 1 d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?
- 1 e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?
- 1 f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

V. CULTURAL RESOURCES

Would the project:

- 1 a) Cause a substantial adverse change in the significance of a historical resource as defined in Public Resources Code Section 15064.5?
- 1 b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Public Resources Code Section 15064.5?
- 1 c) Directly or indirectly destroy a unique paleontological resource or site, or unique geologic feature?

V. CULTURAL RESOURCES (continued)

Would the project:

- 1 d) Disturb any human remains, including those interred outside of formal cemeteries?

VI. GEOLOGY AND SOILS

Would the project:

- a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
- 1 i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?
- 1 ii) Strong seismic ground shaking?
- 1 iii) Seismic-related ground failure, including liquefaction?
- 1 iv) Landslides?
- 1 b) Result in substantial soil erosion or loss of topsoil?
- 1 c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?
- 1 d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

VII. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- 1 a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?
- 1 b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?
- 1 c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?
- 1 d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?
- 1 f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?
- 1 g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?
- 1 h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

VIII. HYDROLOGY AND WATER QUALITY

Would the project:

- 1 a) Violate any water quality standards of waste discharge requirements?

- 1 b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

- 1 c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?

- 1 d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?

- 1 e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

- 1 f) Otherwise substantially degrade water quality?

- 1 g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

- 1 h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?

- 1 i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

- 1 j) Inundation by seiche, tsunami, or mudflow?

IX. LAND USE AND PLANNING

Would the project:

- 1 a) Physically divide an established community?

- 1 b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?

- 1 c) Conflict with any applicable habitat conservation plan or natural community conservation plan?

X. MINERAL RESOURCES

Would the project:

- 1 a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

- 1 b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

XI. NOISE

Would the project:

- 2 a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

XI. NOISE (continued)

Would the project:

- 1 b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

- 2 c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?
- 2 d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?
- 1 f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

XII. POPULATION AND HOUSING

Would the project:

- 2 a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?
- 1 b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?
- 1 c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

XIII. PUBLIC SERVICES

Would the project:

- 2 a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:
 - 2 i) Fire protection?
 - 2 ii) Police protection?
 - 1 iii) Schools?
 - 1 iv) Parks?
 - 1 v) Other public facilities?

XIV. RECREATION

Would the project:

- 1 a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?
- 1 b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

XV. TRANSPORTATION / TRAFFIC

Would the project:

- 2 a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?
- 1 b) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways?

- 1 c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?
- 1 d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?
- 1 e) Result in inadequate emergency access?
- 1 f) Result in inadequate parking capacity?

XVI. UTILITIES AND SERVICE SYSTEMS

Would the project:

- 1 a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
- 2 b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 2 c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 1 d) Have sufficient water supplies available to service the project from existing entitlements and resources, or are new or expanded entitlements needed?
- 1 e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?
- 1 f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?
- 1 g) Comply with federal, state, and local statutes and regulations related to solid waste?

XVII. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- 1 a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?
- 1 b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?
- 1 c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly.

DISCUSSION OF ENVIRONMENTAL EVALUATION

I. AESTHETICS

- a. The project will not adversely affect the view of the Sierra Nevada mountain range, a scenic vista that can be seen from Visalia on clear days.
- b. There are no scenic resources on the site.
- c. Development of the site will change the visual character from a vacant piece of property to a commercial development. Although this loss of visual open space may be significant to some members of the community, the site has been planned for urban uses for several years. The City has development standards related to landscaping and other amenities to ensure that the visual character of the area is not degraded.
- d. The project will allow for new sources of light that is typical of urban development. The City has development standards that require that light be directed and/or shielded so it does not fall upon adjacent residential and commercial properties.

II. AGRICULTURAL RESOURCES

- a. The site is not zoned for agricultural use and will not involve the conversion of farmland. The project will not have an effect on any agricultural resources.
- b. The site is not under Williamson Act contract or not within an Agricultural Preserve.
- c. The project will not involve other changes in the existing environment which due to their location or nature, could result in conversion of Farmland to nonagricultural use.

III. AIR QUALITY

- a. The project in itself does not disrupt implementation of the San Joaquin Valley Unified Air Pollution Control District's air quality plan.
- b. The project will not violate an air quality standard or contribute substantially to an existing or projected air quality violation.
- c. The San Joaquin Valley is a region that is already at non-attainment status for air quality. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use.
- d. The project's proposed land uses generally do not generate substantial pollutant concentrations.
- e. The project's proposed land uses generally do not generate objectionable odors.

IV. BIOLOGICAL RESOURCES

- a. There are no identified candidate, sensitive, or special status species on the site.
- b. The project is not located within or adjacent to a sensitive riparian habitat or other natural community.

- c. The project is not located within or adjacent to federally protected wetlands as defined by Section 404 of the Clean Water Act.
- d. No animal movement will be affected by the proposed development.
- e. The City has a municipal ordinance in place to protect oak trees. All existing oak trees on the project site will be under the jurisdiction of this ordinance. Any Oak Trees to be removed from the site are subject to the jurisdiction of the municipal ordinance.
- f. There are no local or regional habitat conservation plans for the area.

V. CULTURAL RESOURCES

- a. There are no known historical resources located within the project area. If some potentially historical or cultural resource is unearthed during construction all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- b. There are no known archaeological resources located within the project area. If some archaeological resource is unearthed during construction all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- c. There are no known unique paleontological resources or geologic features located within the project area.
- d. There are no known human remains buried in the project vicinity. If human remains are unearthed during construction all work should cease until the proper authorities are notified and a qualified professional archaeologist can evaluate the finding and make any necessary mitigation recommendations.

VI. GEOLOGY AND SOILS

- a. The State Geologist has not issued an Alquist-Priolo Earthquake Fault Map for Tulare County. The project area is not located on or near any known earthquake fault lines. Therefore, the project will not expose people or structures to potential substantial adverse impacts involving earthquakes.
- b. The project will not result in substantial soil erosion or a loss of topsoil.
- c. The project area is relatively flat and the underlying soil is not known to be unstable. Soils in the Visalia area have few limitations with regard to development. Due to low clay content and limited topographic relief, soils in the Visalia area generally have low expansion characteristics.
- d. Due to low clay content, soils in the Visalia area have an expansion index of 0-20, which is defined as very low potential expansion.

VII. HAZARDS AND HAZARDOUS MATERIALS

- a. Hazardous materials such as gasoline and pesticides will most likely be found within the project area after the project is developed, but only in quantities typically used for residential or commercial use or application. This will not create a significant hazard.
- b. There is no reasonably foreseeable condition or incident involving the project that could result in release of hazardous materials into the environment.
- c. The project site is not located within one-quarter mile of a school. There are no reasonably foreseeable conditions or incidents involving the project that could affect this or proposed school sites or areas within one-quarter mile of school sites.
- d. The project area does not include any sites listed as hazardous materials sites pursuant to Government Code Section 65692.5.
- e. The project area is not located within any airport land use plan or within 2 miles of a public airport.
- f. The project area is not within the vicinity of any private airstrip.
- g. The project will not interfere with the implementation of any adopted emergency response plan or evacuation plan.
- h. There are no wildlands within or near the project area.

VIII. HYDROLOGY AND WATER QUALITY

- a. The project will not violate any water quality standards. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- b. The project will not substantially deplete groundwater supplies in the project vicinity.
- c. The project will not result in substantial erosion on- or off-site.
- d. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- e. Development standards are already in place to require that stormwater drainage be held on-site with a drainage basin or to connect to the City stormwater drainage system.
- f. There are no reasonably foreseeable reasons why the project would result in the degradation of water quality.
- g. The project area is located within Zones X500. Sites designated as being within X500 are areas of 500-year flood.
- h. The project area is located within Zones X500. Sites designated as being within X500 are areas of 500-year flood.

- i. The project would not expose people or structures to risks from failure of levee or dam.
- j. Seiche and tsunami impacts do not occur in the Visalia area. The site is relatively flat, so there will be no impacts related to mudflow.

IX. LAND USE AND PLANNING

- a. The project will not physically divide an established community.
- b. The site is within the current Urban Development Boundaries of the City of Visalia. The City of Visalia designates the area for urban development. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use.
- c. The project does not conflict with any applicable conservation plan.

X. MINERAL RESOURCES

- a. No mineral areas of regional or statewide importance exist within the Visalia area.
- b. There are no mineral resource recovery sites delineated in the Visalia area.

XI. NOISE

- a. The project will result in noise generation typical of urban development. There will be noise generated by traffic along designated arterial and collector streets. The City's standards for setbacks and/or construction of walls along major streets will reduce noise levels to a level that is less than significant.
- b. The project will not result in ground-borne vibration or ground-borne noise levels. There are no existing uses near the project area that create ground-borne vibration or ground-borne noise levels.
- c. Noise levels will increase beyond current levels as a result of the project. These levels will be typical of noise levels associated with urban development. Therefore, the increase is less than significant.
- d. Noise levels will increase during the construction of the project but shall remain within the limits defined by the City Noise Ordinance.
- e. The project area is not within an airport land use plan, nor is it within 2 miles of a public airport.
- f. There is no private airstrip near the project area.

XII. POPULATION AND HOUSING

- a. The requested project will facilitate for development of this site which will result in increased housing in the area. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for urban use.
- b. There are no houses on the site. This does not displace substantial numbers of housing units which would necessitate the construction of replacement housing elsewhere.

- c. There is a vacant house on the site; development of the site will not displace people living within existing residences.

XIII. PUBLIC SERVICES

- a.
 - i. Current fire protection facilities can adequately serve the site without a need for alteration.
 - ii. Current police protection facilities can adequately serve the site without a need for alteration.
 - iii. Current school facilities can adequately serve the site without a need for alteration.
 - iv. Current park and recreation facilities can adequately serve the site without a need for alteration.
 - v. Other public facilities can adequately serve the site without a need for alteration.

XIV. RECREATION

- a. The proposed project will not increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated.
- b. The proposed project does not include recreational facilities. It will not require the construction or expansion of recreational facilities within the area that might have an adverse physical effect on the environment.

XV. TRANSPORTATION AND TRAFFIC

- a. Development of the site will result in increased traffic in the area. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for urban use.
- b. The site is projected for urban development by the City and County General Plans. The project is not proposed to exceed what has already been planned for in this area.
- c. The project will not result in nor require a need to change air traffic patterns.
- d. Upon development of the site, roads will be designed and constructed to City standards so that there will be no increased hazards.
- e. Upon development of the site, roads will be constructed to City standards that will provide adequate emergency access.

- f. The project will be required to meet the City's parking requirements for shopping centers

XVI. UTILITIES AND SERVICE SYSTEMS

- a. The site is projected for urban development by the City General Plan. The project is not proposed to exceed what has already been planned for in this area.
- b. The project calls for the extension of sanitary sewer facilities. Expansion will not result in a significant effect. The trunk line expansion is consistent with the City Sewer Master Plan.
- c. The project calls for the extension of storm drain facilities. Expansion will not result in a significant effect. The trunk line expansion is consistent with the City Storm Water Master Plan.
- d. There are sufficient water supplies to support the project.
- e. The City has determined that there is adequate capacity existing to serve the site's projected wastewater treatment demands at the City wastewater treatment plant.
- f. Current solid waste disposal facilities can adequately serve the site without a need for alteration.
- g. The project should be able to meet the applicable regulations for solid waste.

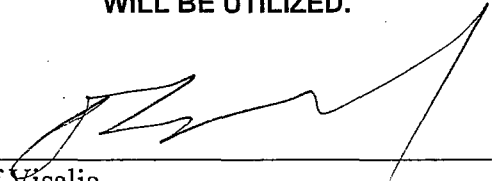
XVII. MANDATORY FINDINGS OF SIGNIFICANCE

- a. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- b. This site was evaluated in the EIR for the City of Visalia Land Use Element Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- c. The project will not create adverse environmental effects to a human population.

DETERMINATION OF REQUIRED ENVIRONMENTAL DOCUMENT

On the basis of this initial evaluation:

- ☒ I find that the proposed project **COULD NOT** have a significant effect on the environment. **A NEGATIVE DECLARATION WILL BE PREPARED.**
- ☐ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on the attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION WILL BE PREPARED.**
- ☐ I find the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required
- ☐ I find that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 90020160). The Environmental Impact Report prepared for the City of Visalia Land Use Element (Amendment No. 90-04) was certified by Resolution NO. 91-105 adopted on September 3, 1991. **THE PROGRAM ENVIRONMENTAL IMPACT REPORT WILL BE UTILIZED.**



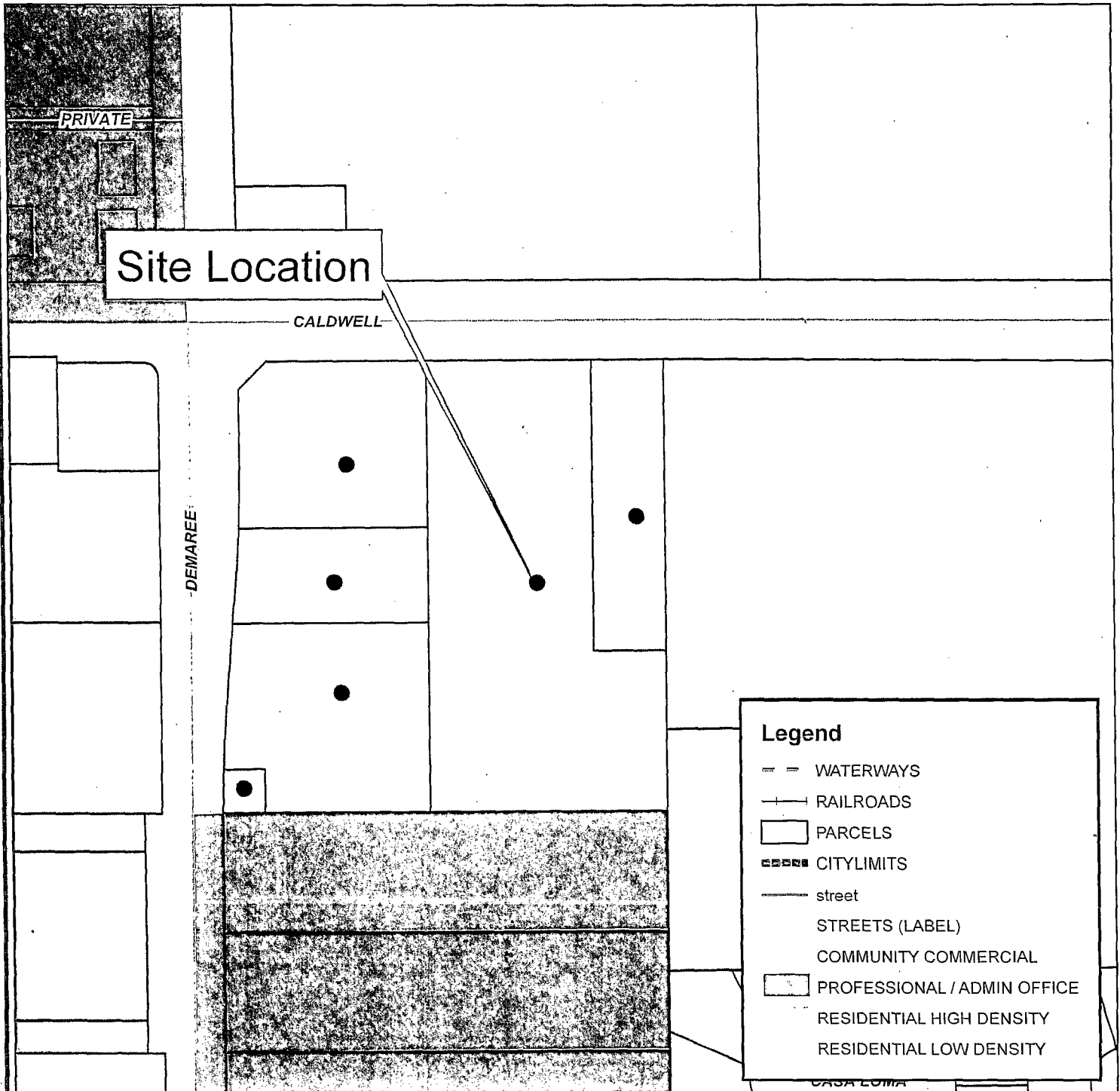
City of Visalia

7-13-06

Date

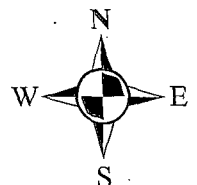
CUP 2006-32/ SPA 2006-02

APNs: 126-030-033, 034, 035, 036,
126-030-014, 015



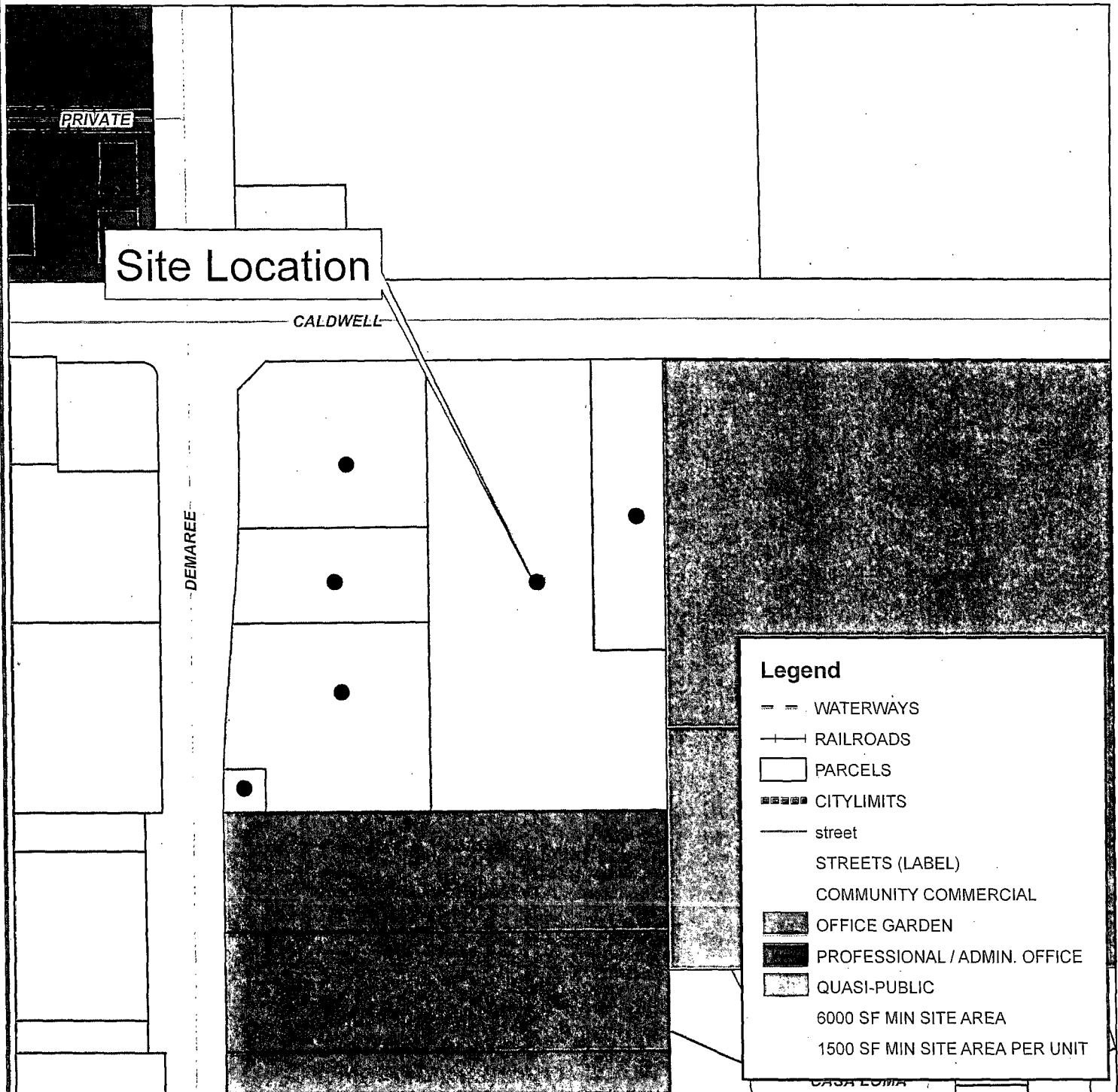
General Plan Land Use Map

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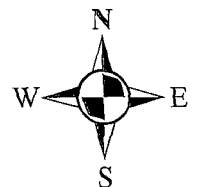
CUP 2006-32/ SPA 2006-32

APNs: 126-030-033, 034, 035, 036,
126-030-014, 015



Zoning Map

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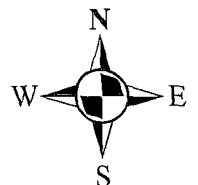
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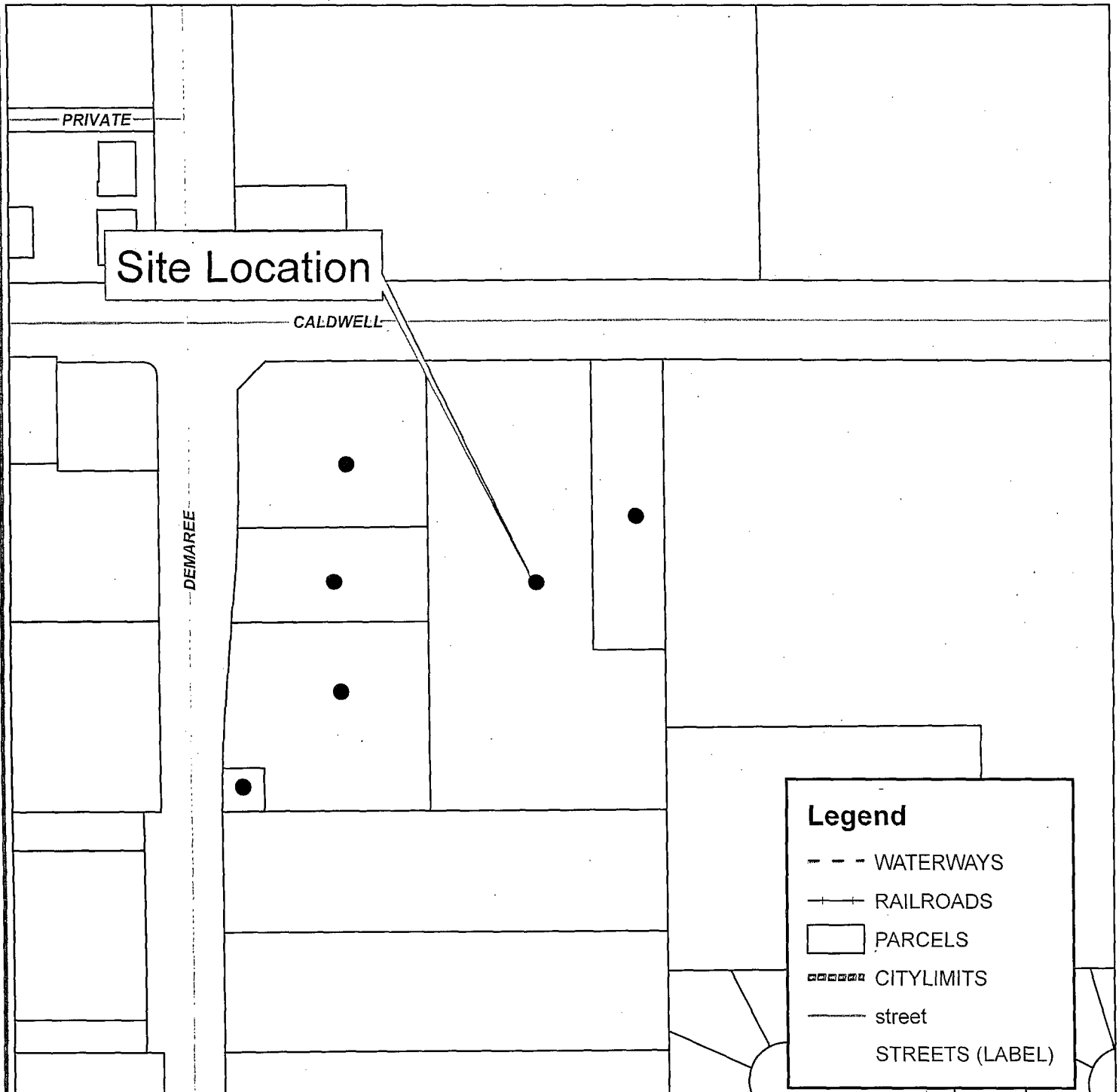
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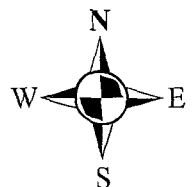


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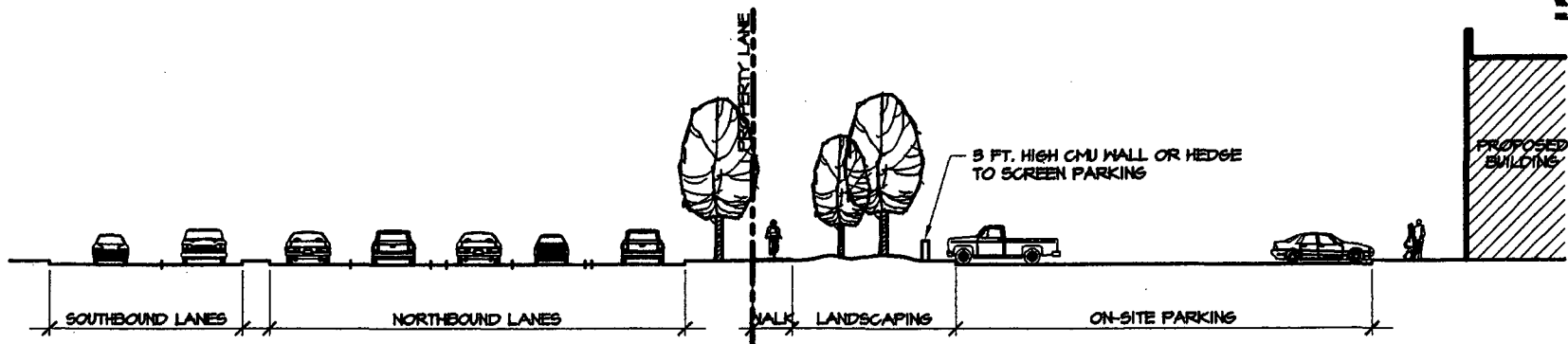
Location Map



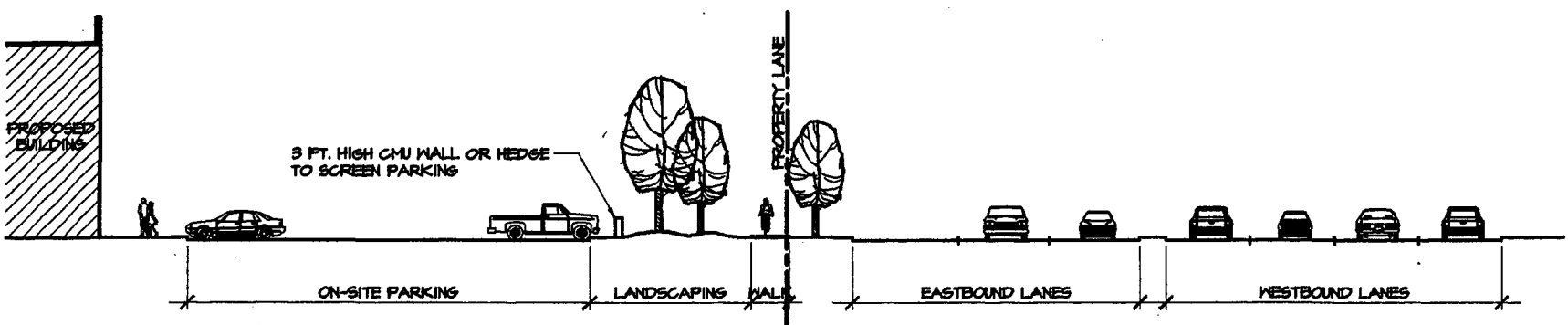
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PROJECT NOTICE

SITE PLAN & EXHIBITS



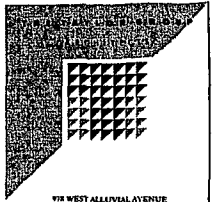
DEMAREE ROAD CROSS-SECTION



CALDWELL AVE. CROSS-SECTION

**PROPOSED STREET CROSS SECTION FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD**





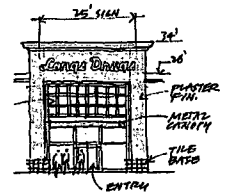
1718 WEST ALLUVIAL AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559.437.8808
FAX 559.437.5005



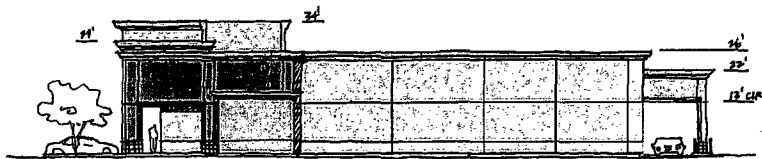
NORTH ELEVATION



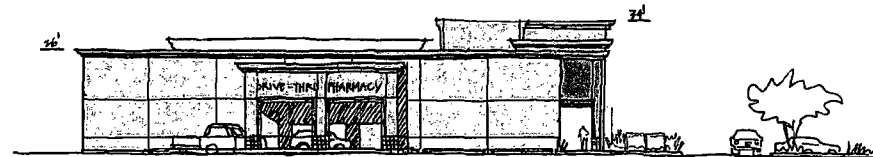
WEST ELEVATION



TOWER HEAD-ON ELEVATION



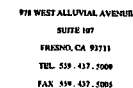
SOUTH ELEVATION



EAST ELEVATION

The owner hereby certifies that the information furnished to the architect is true and correct and that the architect is not aware of any material omissions or misstatements in the information furnished.

DESIGNED BY _____
CHECKED BY _____
DATE _____
PROJECT NO. _____
OF _____ SHEETS

[illegible]

DRAWN BY _____
DATE _____
REVISIONS _____

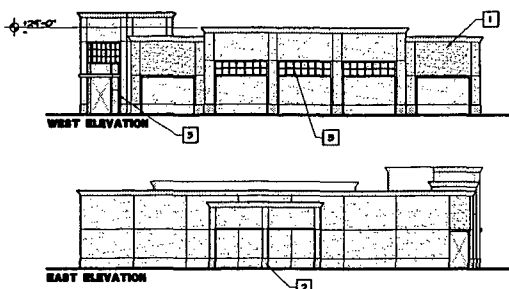
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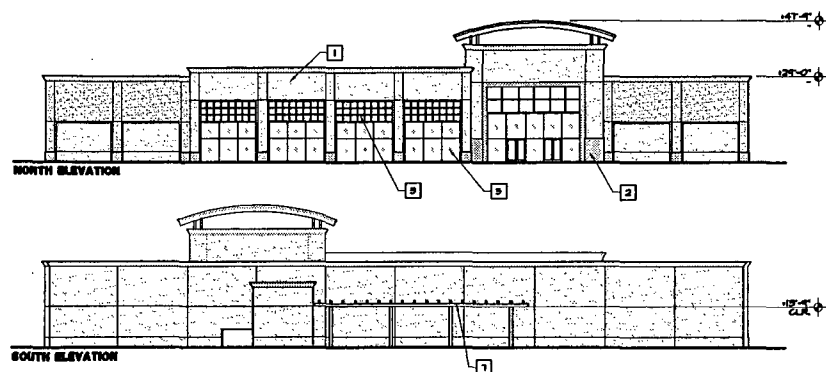
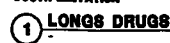


**PROPOSED MASTER SITE PLAN FOR
LONGS DRUG ST
CALDWELL AVE. & DEMAREE ROAD**

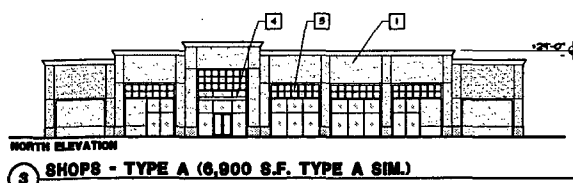
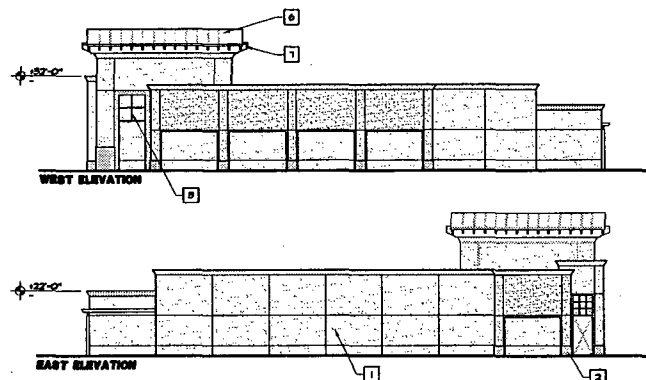
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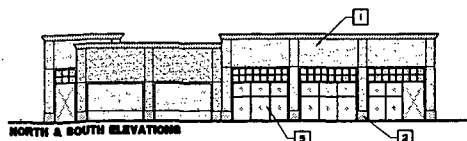
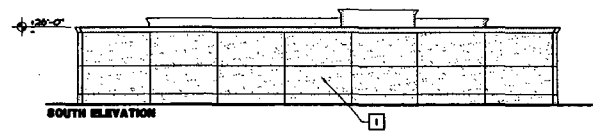
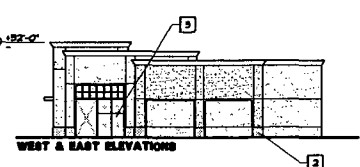
1. EXTERIOR GEMENT PLASTER; SEE COLORED ELEVATIONS 9-1/2" = 1" PAGE OF SUBMITTAL FOR TYP. COLOR SCHEME.
2. TILE BASE
3. DECORATIVE METAL GRILL
4. METAL CANOPY
5. STOREFRONT SYSTEM
6. METAL STANDING SEAM ROOFING
7. METAL TRELLIS / SUPPORT STRUCTURE



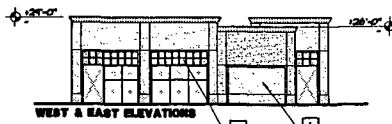
2 MAJOR RETAIL



3 SHOPS - TYPE A (6,900 S.F. TYPE A SIM.)



④ SHOPS - TYPE B



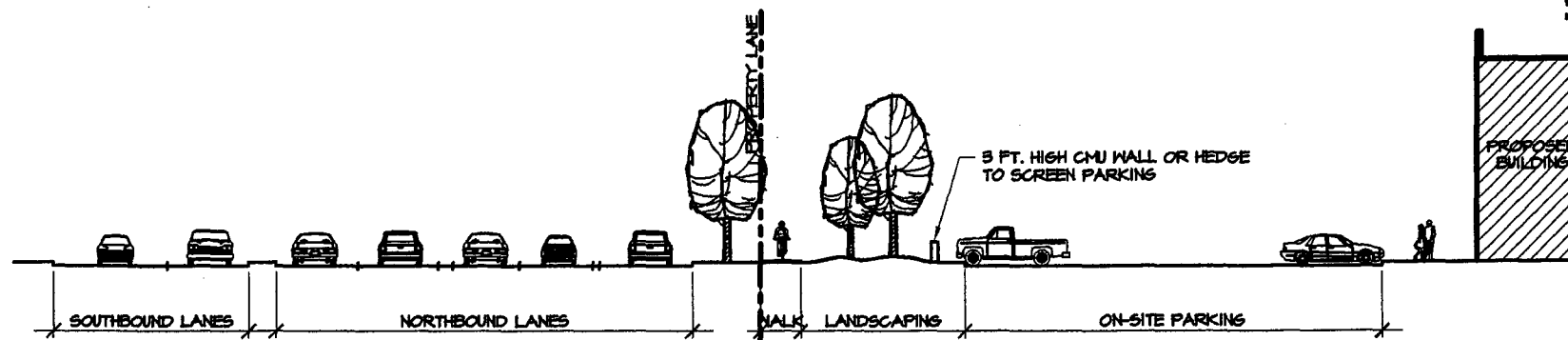
WEST & EAST ELEVATIONS

ELEVATIONS

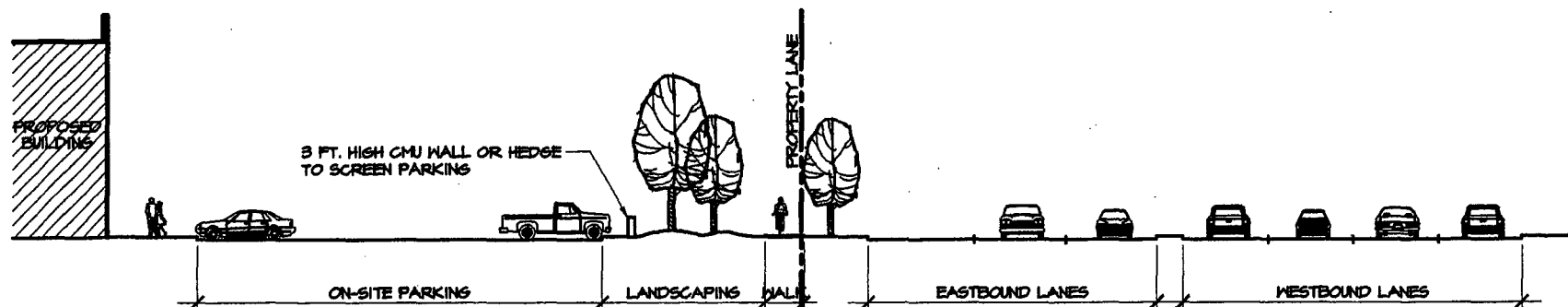
SCALE: 1"=20'-0"



THE PAVES GROUP
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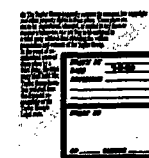


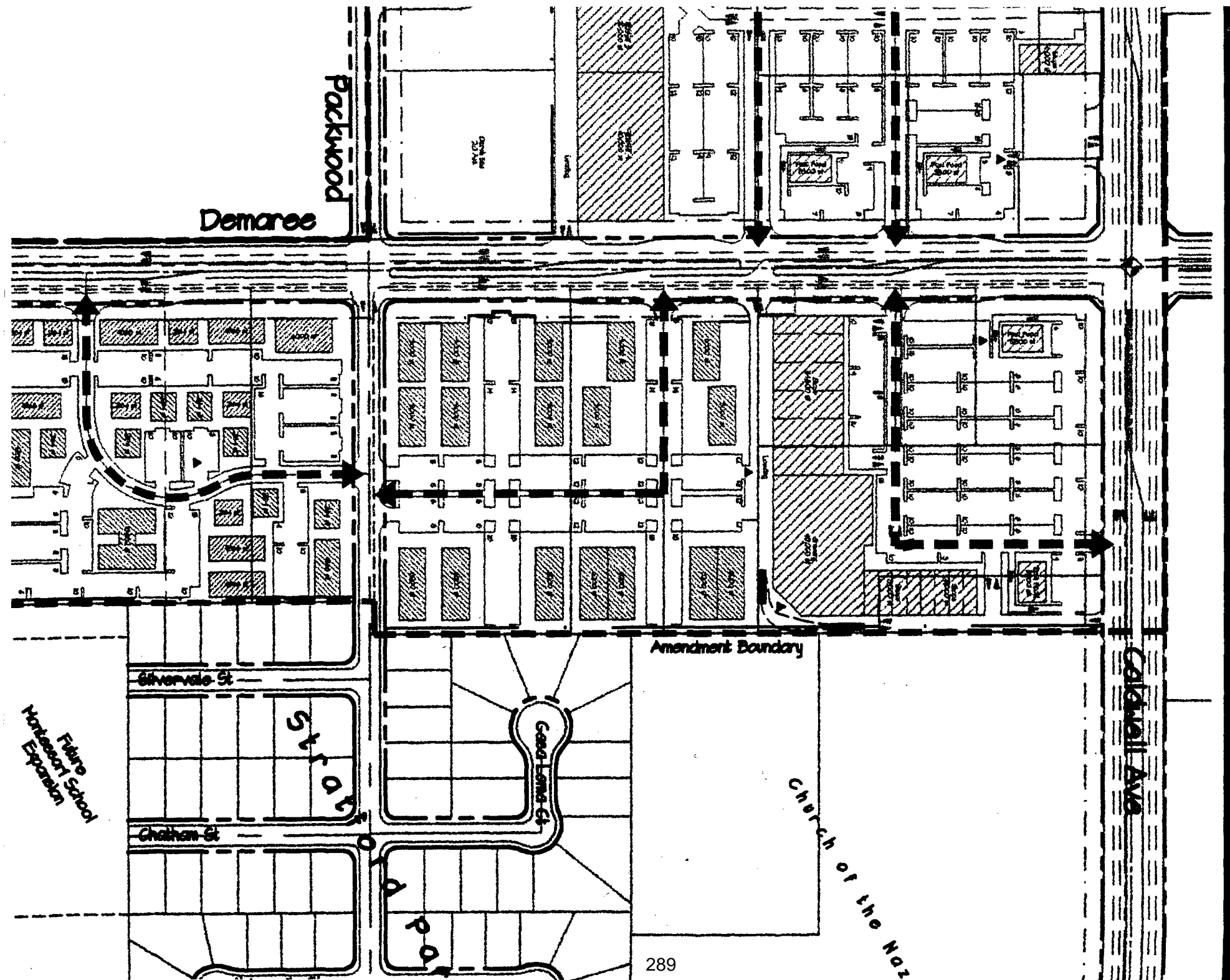
DEMAREE ROAD CROSS-SECTION



CALDWELL AVE. CROSS-SECTION

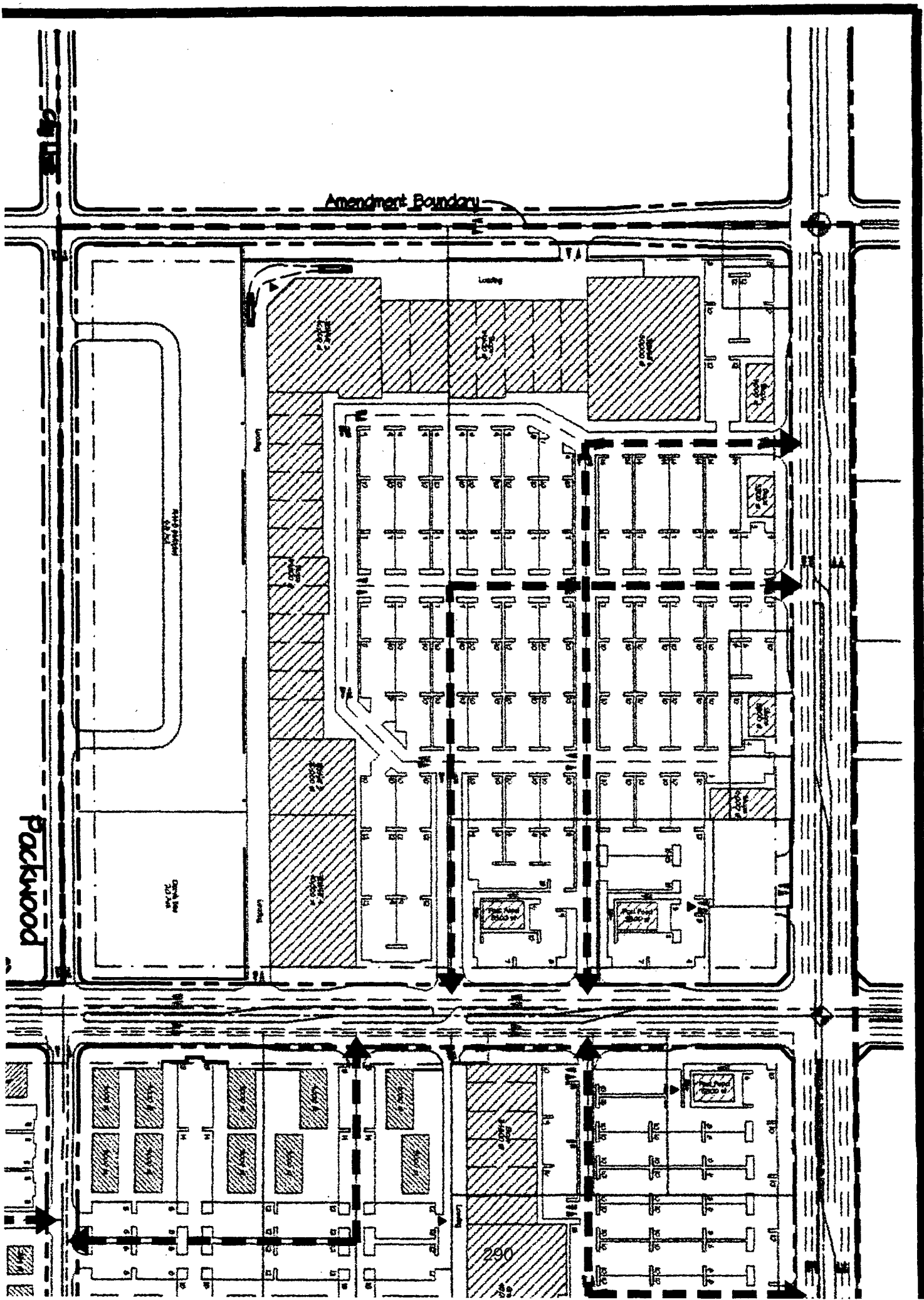
**PROPOSED STREET CROSS SECTION FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD**



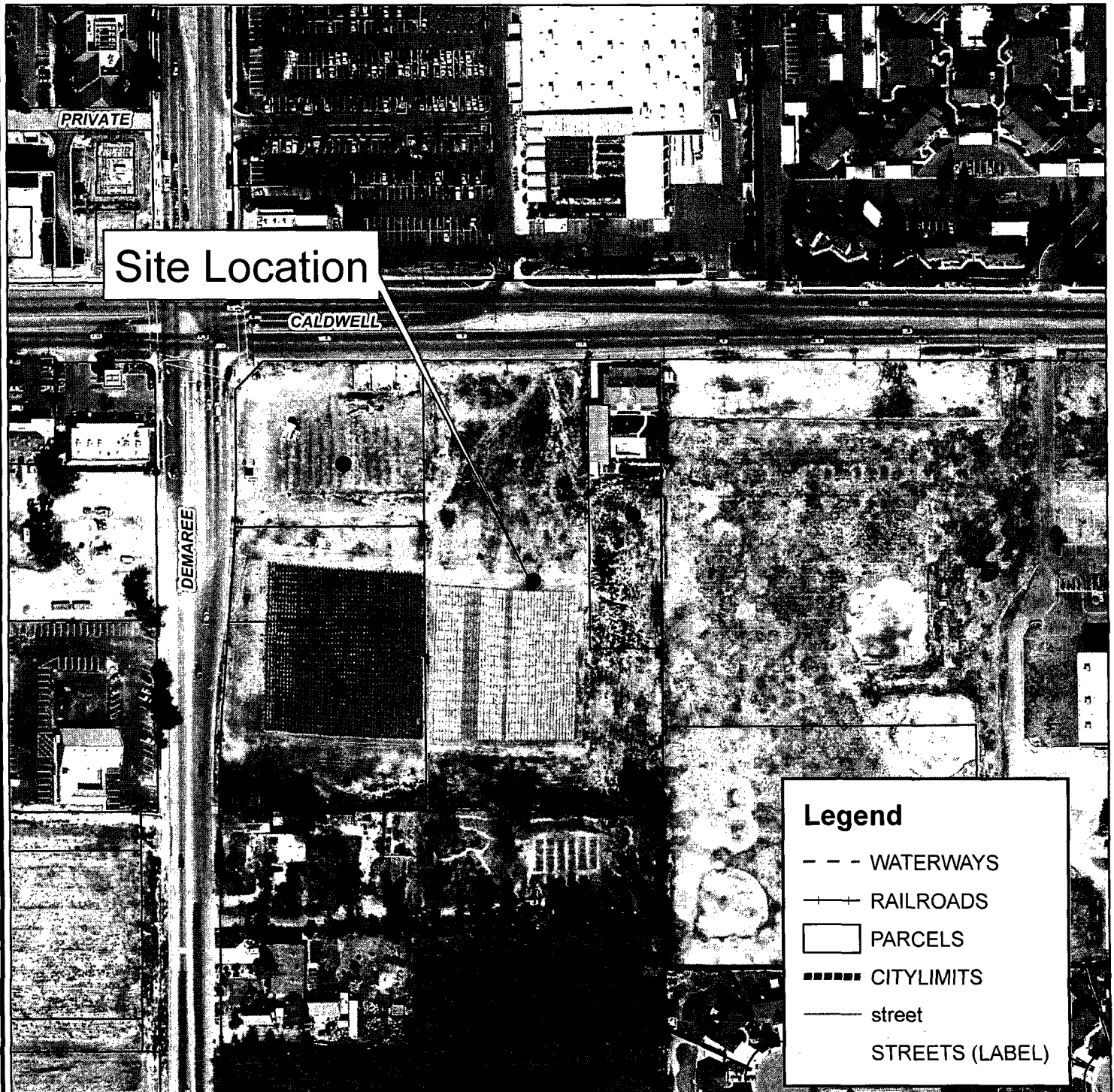


Amendment Boundary

Packwood



CUP 2006-32/ SPA 2006-02
APNs: 126-030-033, 034, 035, 036,
126-030-014, 015



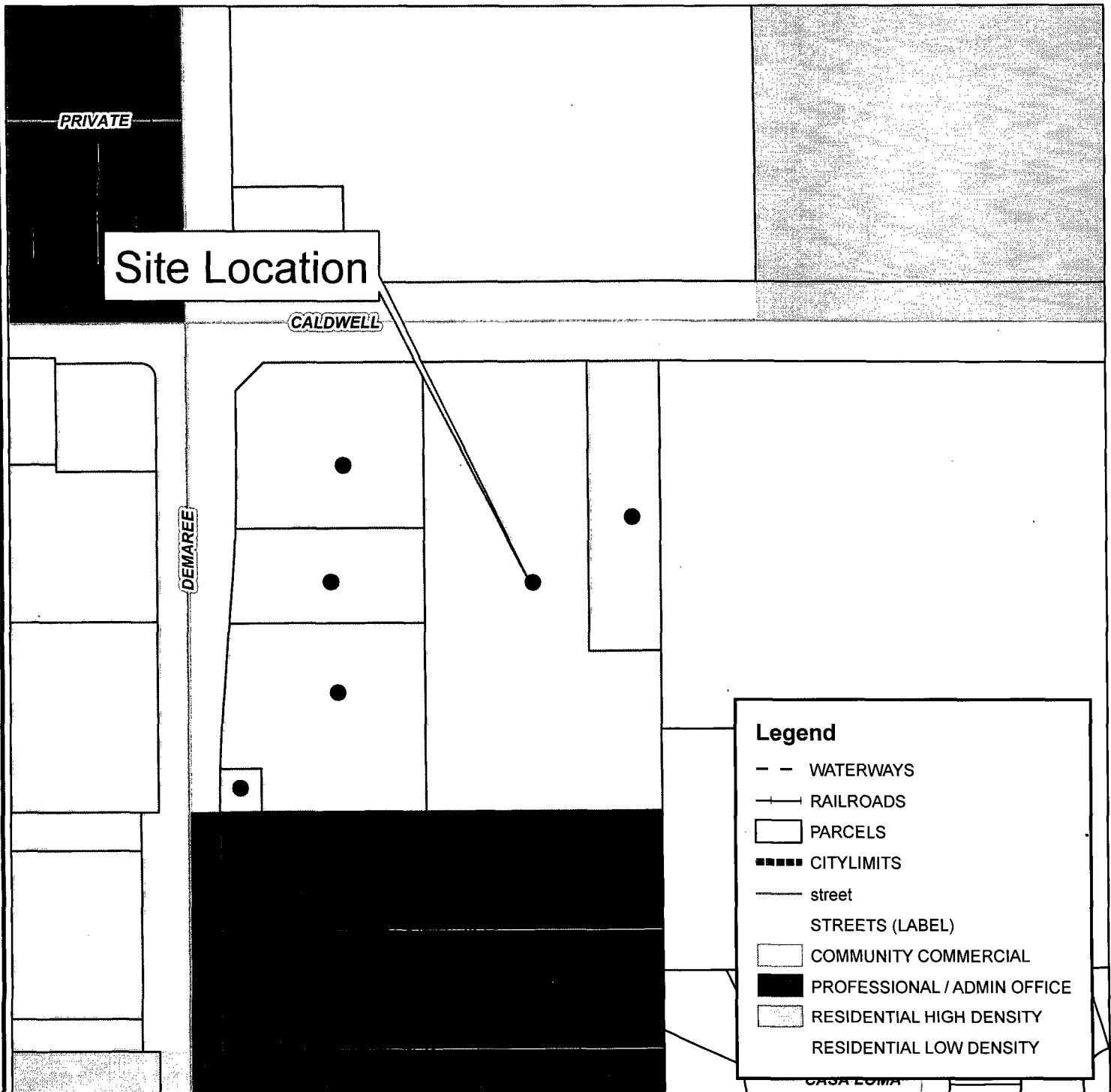
Aerial Photo

200 100 0 200 400 Feet



CUP 2006-32/ SPA 2006-02

APNs: 126-030-033, 034, 035, 036,
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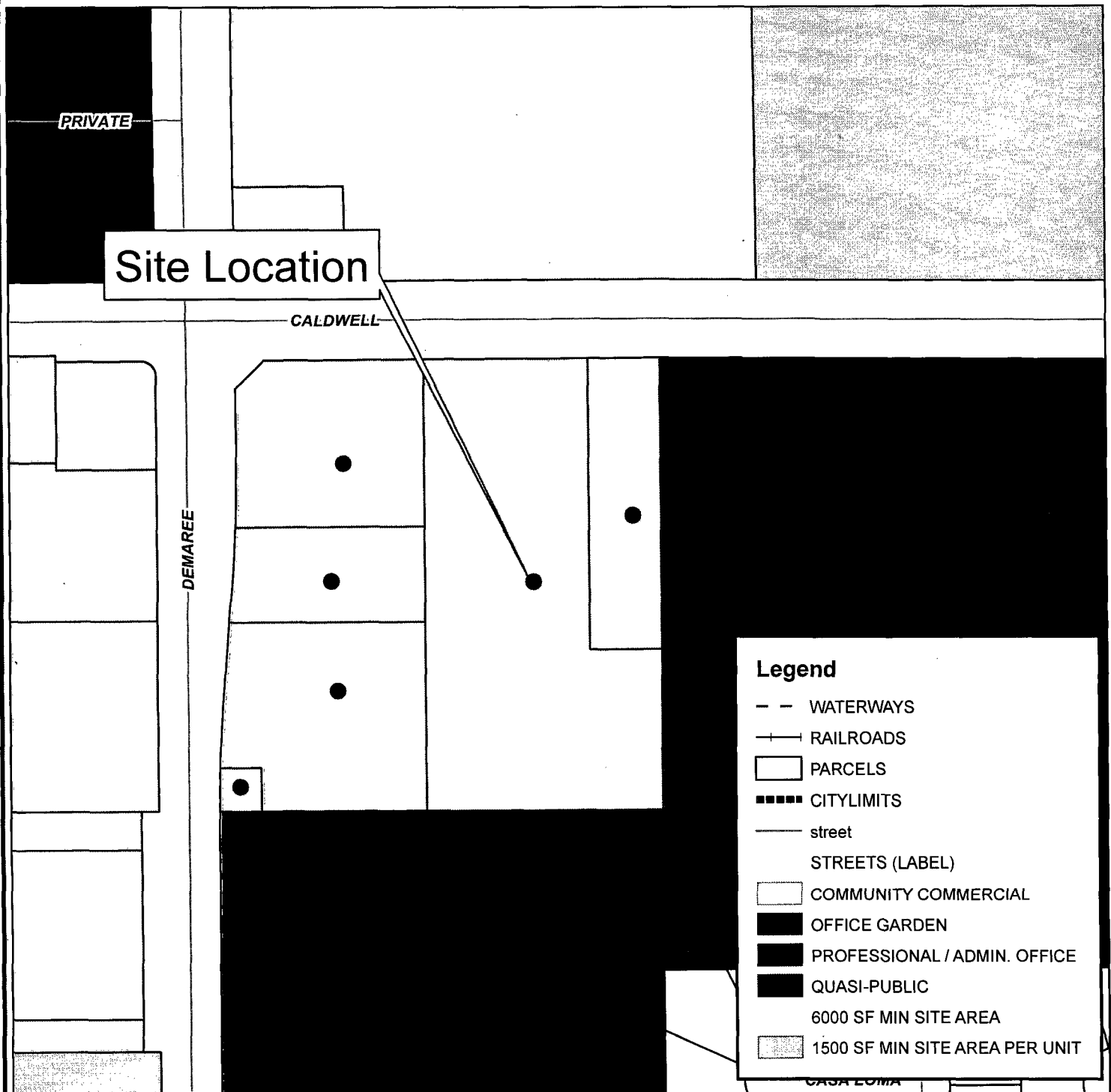
General Plan Land Use Map

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CUP 2006-32/ SPA 2006-02

APNs: 126-030-033, 034, 035, 036,
126-030-014, 015



Zoning Map

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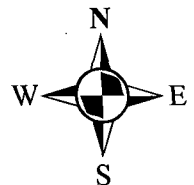
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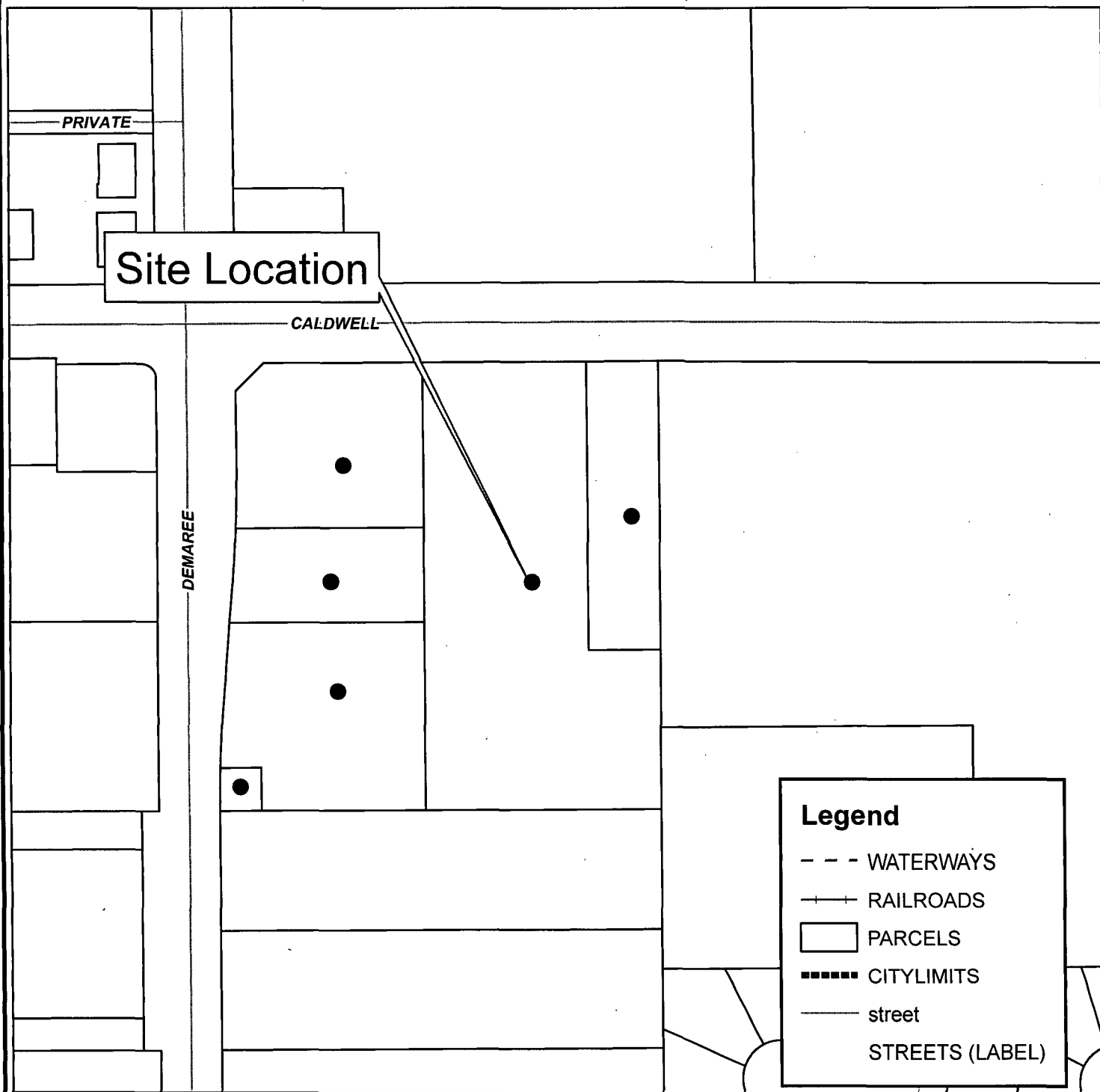
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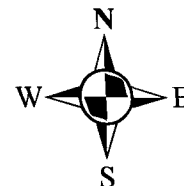
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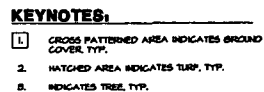
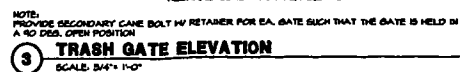
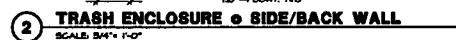
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126-030-014, 015



Location Map

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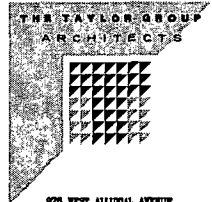




SCALE: 1"=40'-0"



OF _____ SHEETS _____



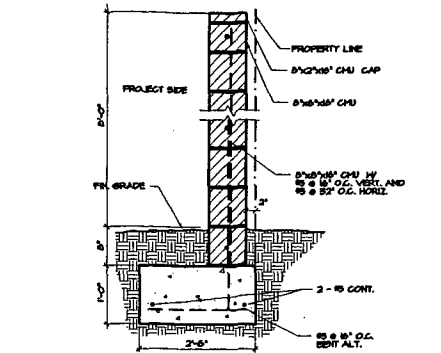
976 WEST ALLIUM AVENUE
SUITE 107
PERRIS, CA 92471
TEL. 660 . 437 . 5000
FAX. 660 . 437 . 5000

PROPOSED MASTER SITE PLAN FOR **LONGS DRUG STORES** CALDWELL AVE. & DEMAREE ROAD

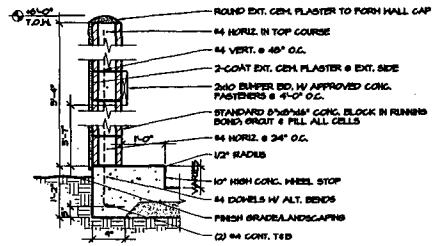
By The Taylor Group, Inc. is hereby acknowledged by copyright and other property rights in this plan. These plans are not to be reproduced, stored, or used in any form or by any means, without the prior written consent of The Taylor Group, Inc. The use of any portion of these plans without the prior written consent of The Taylor Group, Inc. is prohibited. The use of any portion of these plans without the prior written consent of The Taylor Group, Inc. is prohibited. The use of any portion of these plans without the prior written consent of The Taylor Group, Inc. is prohibited.

DRAWN BY
DATE 5/1/06
REVISED BY
DATE
REVISED BY
DATE

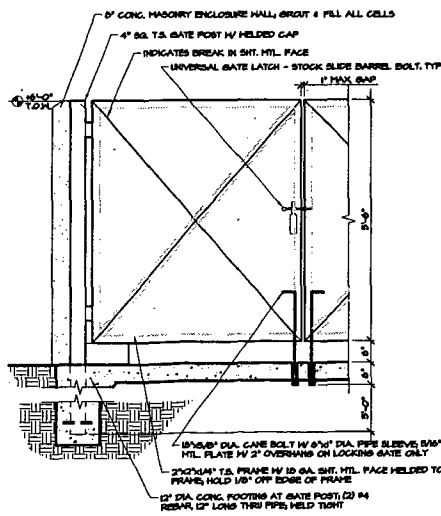
SHEET NO
SPR2
OF 2 SHEETS



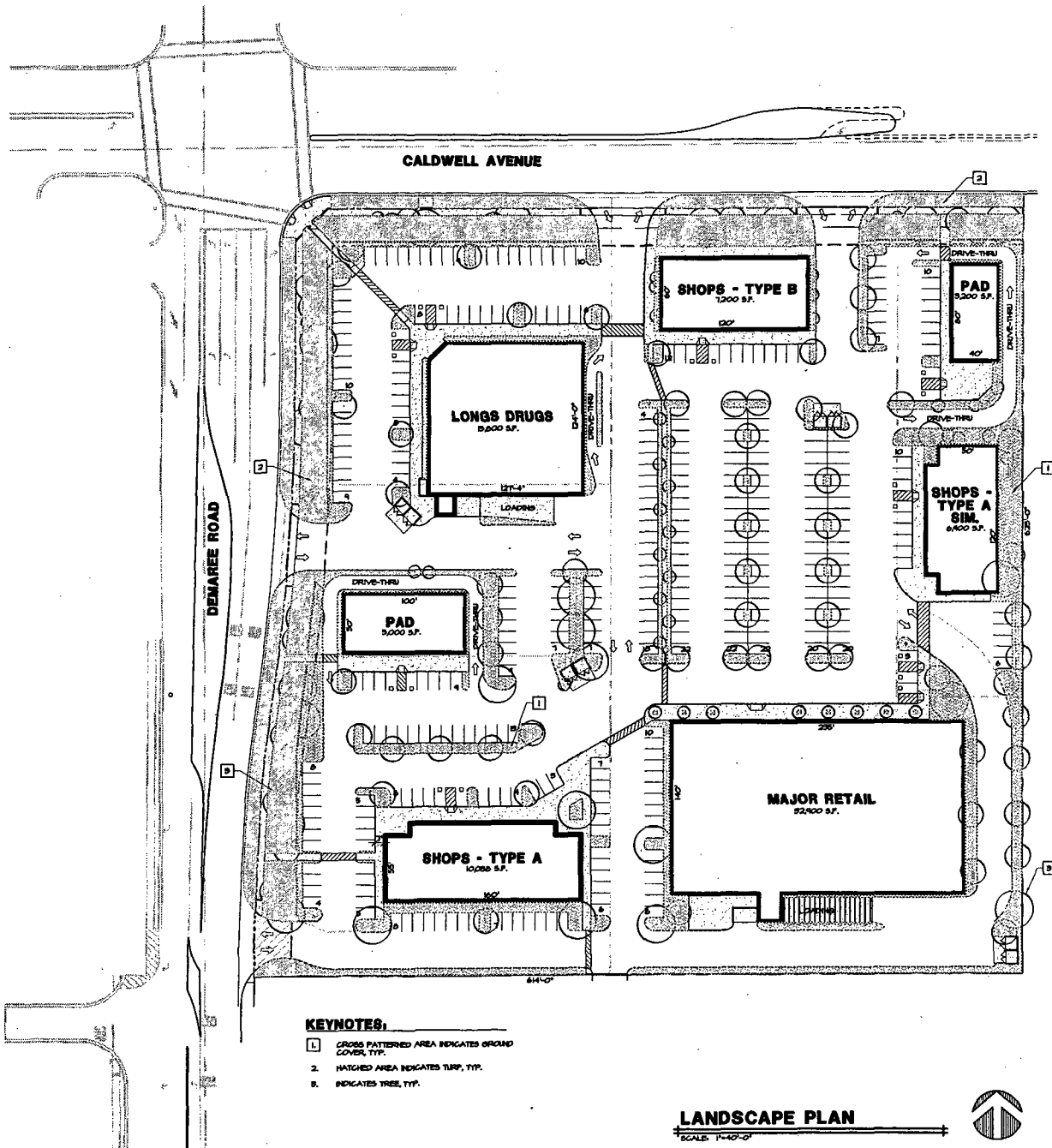
1 CMU WALL - PROPERTY LINE - TYP.
SCALE: 1/4" = 1'-0"



2 TRASH ENCLOSURE - SIDE/BACK WALL
SCALE: 3/4" = 1'-0"

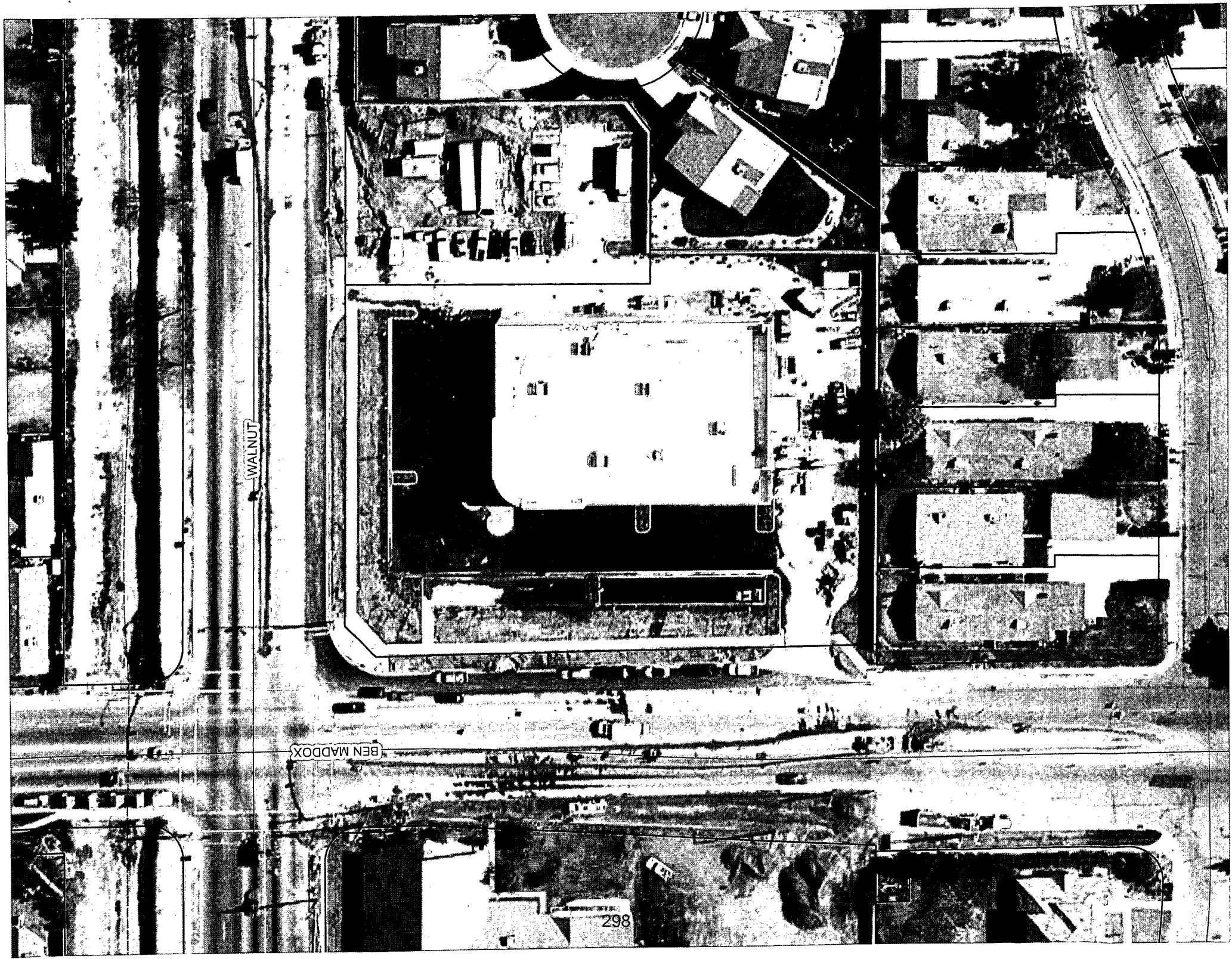


3 TRASH GATE ELEVATION
SCALE: 3/4" = 1'-0"



- KEYNOTES:**
1. CROSS HATCHED AREA INDICATES GROUND COVER, TYP.
 2. HATCHED AREA INDICATES TURF, TYP.
 3. INDICATES TREE, TYP.

LANDSCAPE PLAN
SCALE: 1/4" = 1'-0"

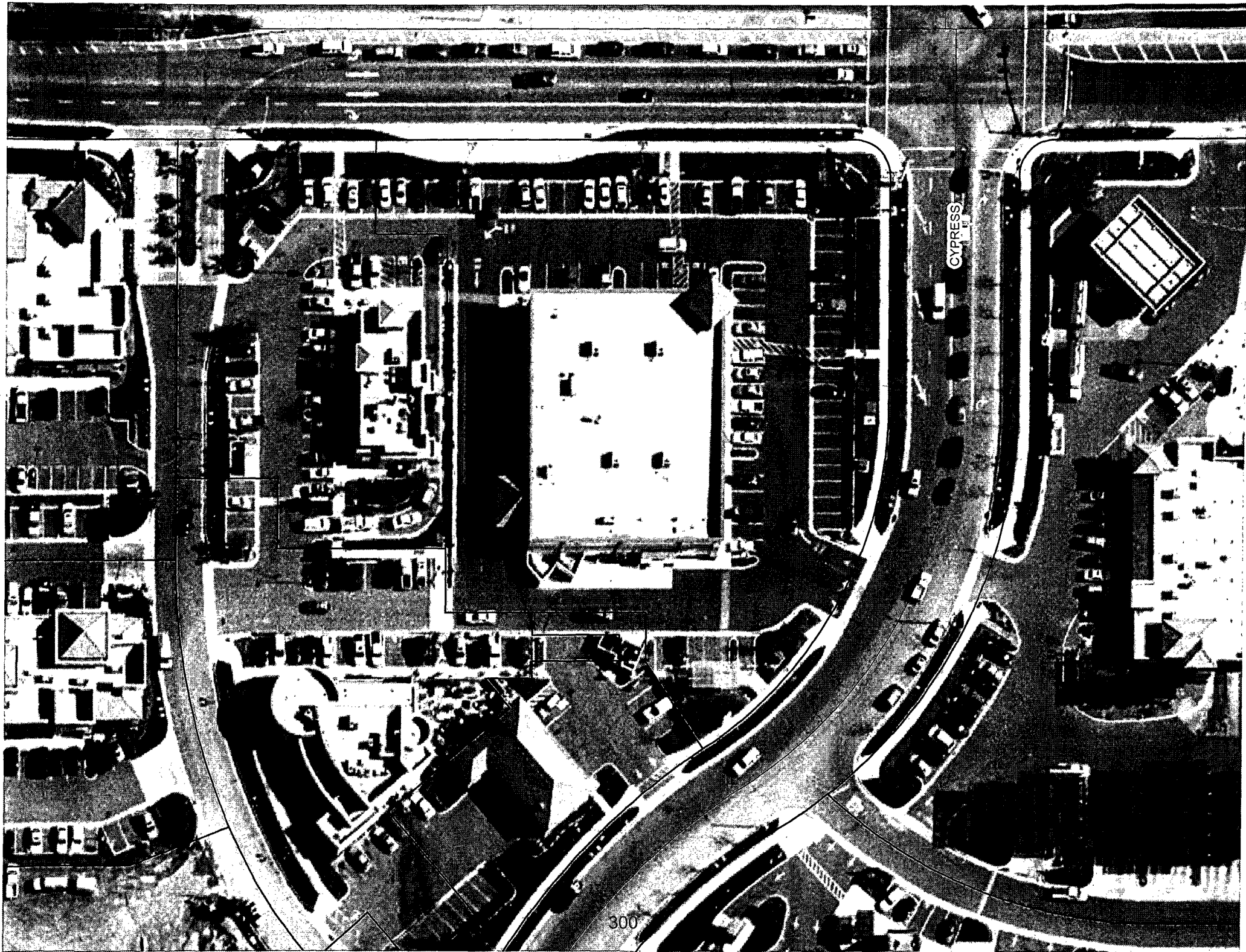


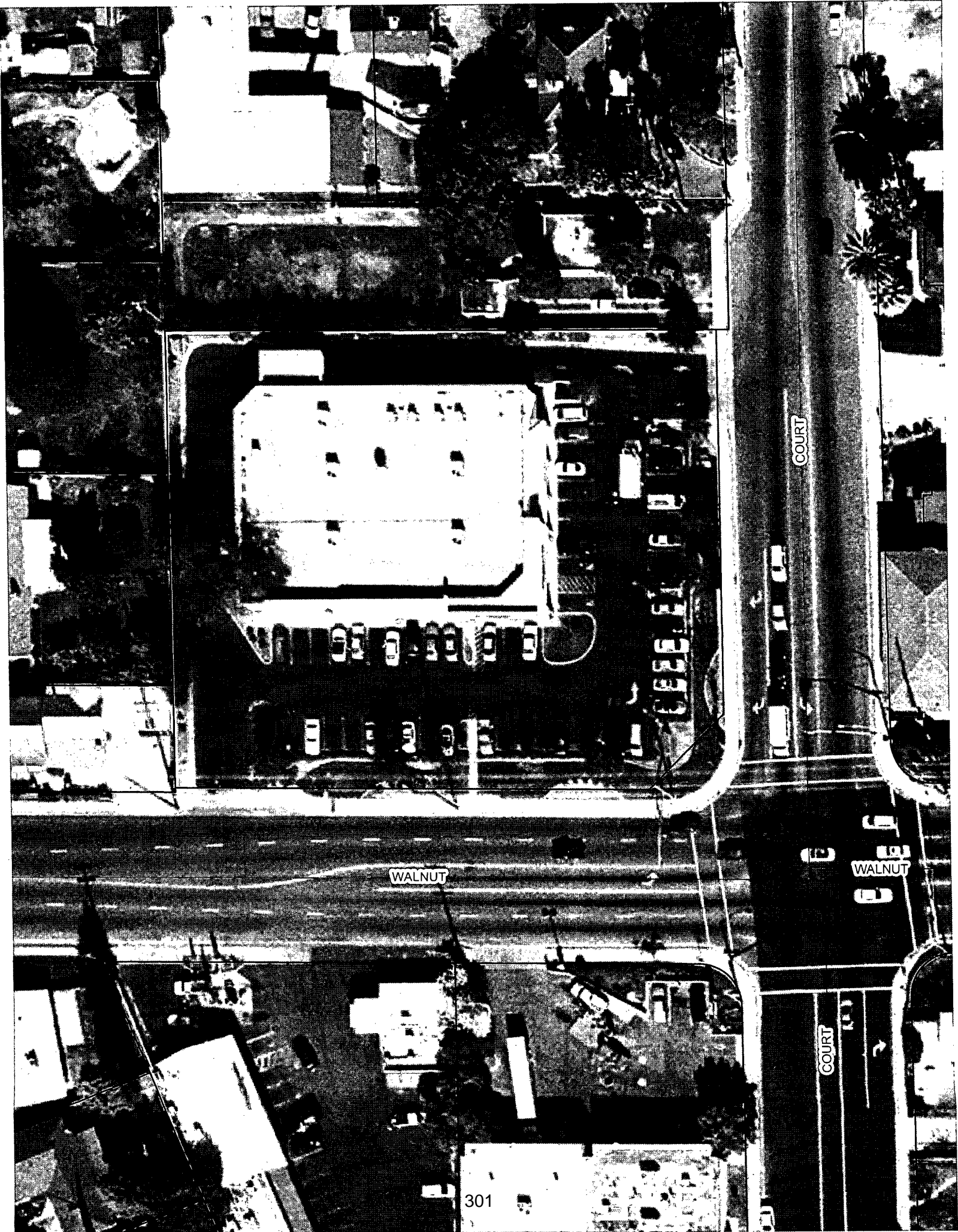
WALNUT

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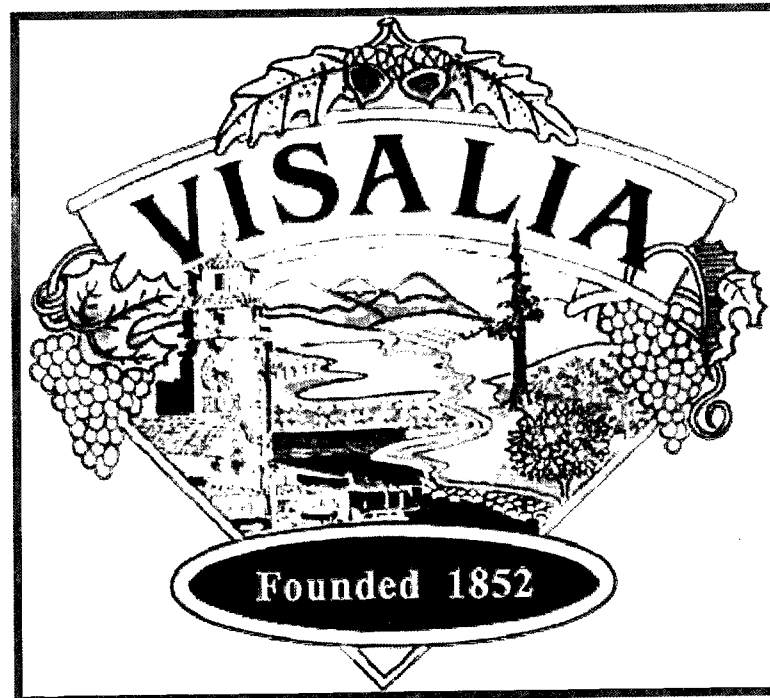
COURT

301

CITY OF VISALIA

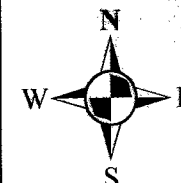
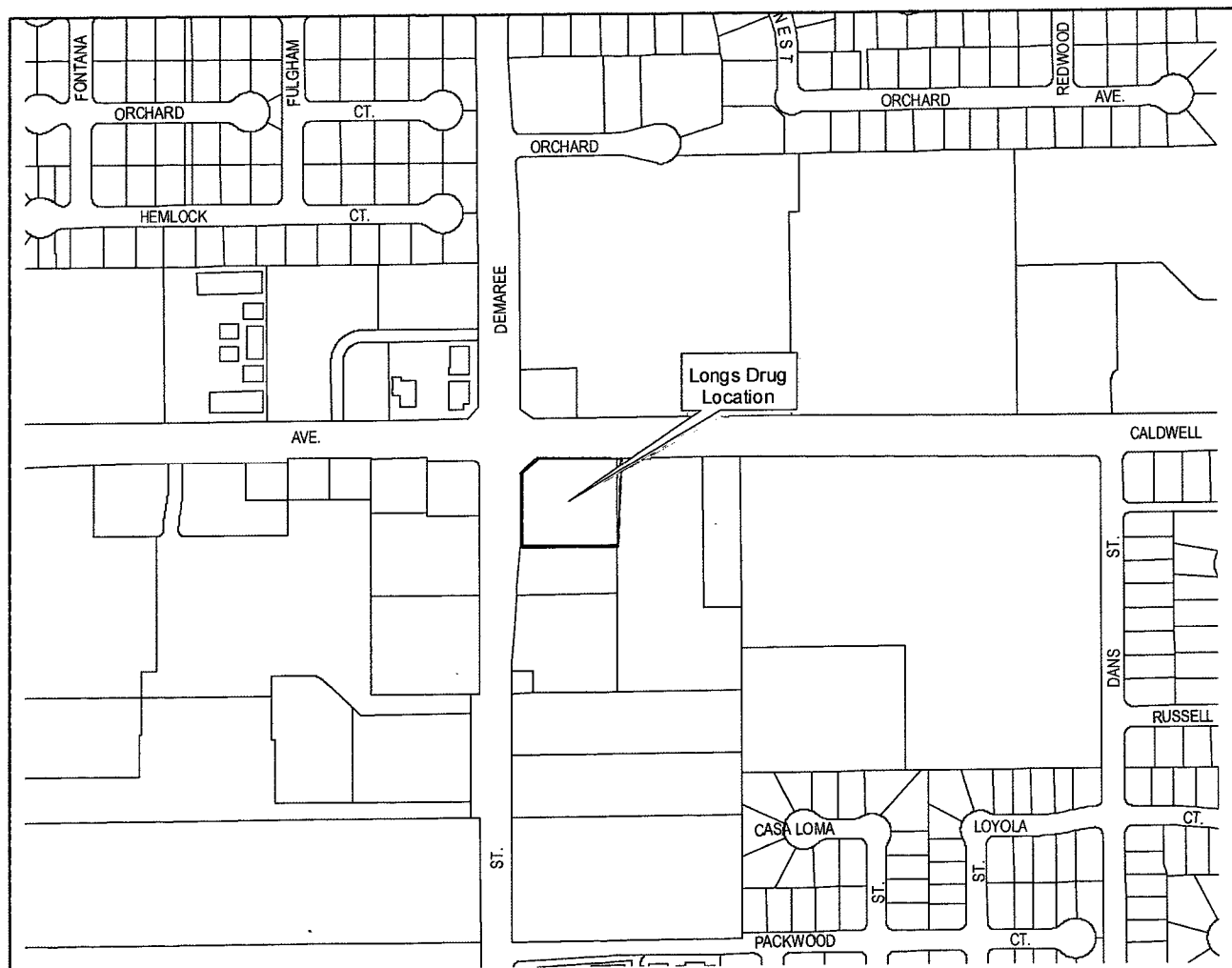
PLANNING COMMISSION

October 9, 2006



The Taylor Group

Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32



LOCATION MAP

The Taylor Group

Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32



Legend

- WATERWAYS
- +— RAILROADS
- PARCELS
- COMMUNITY COMMERCIAL
- PROFESSIONAL / ADMIN OFFICE
- RESIDENTIAL HIGH DENSITY
- RESIDENTIAL LOW DENSITY

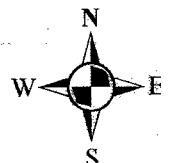
**GENERAL PLAN
LAND USE**



Legend

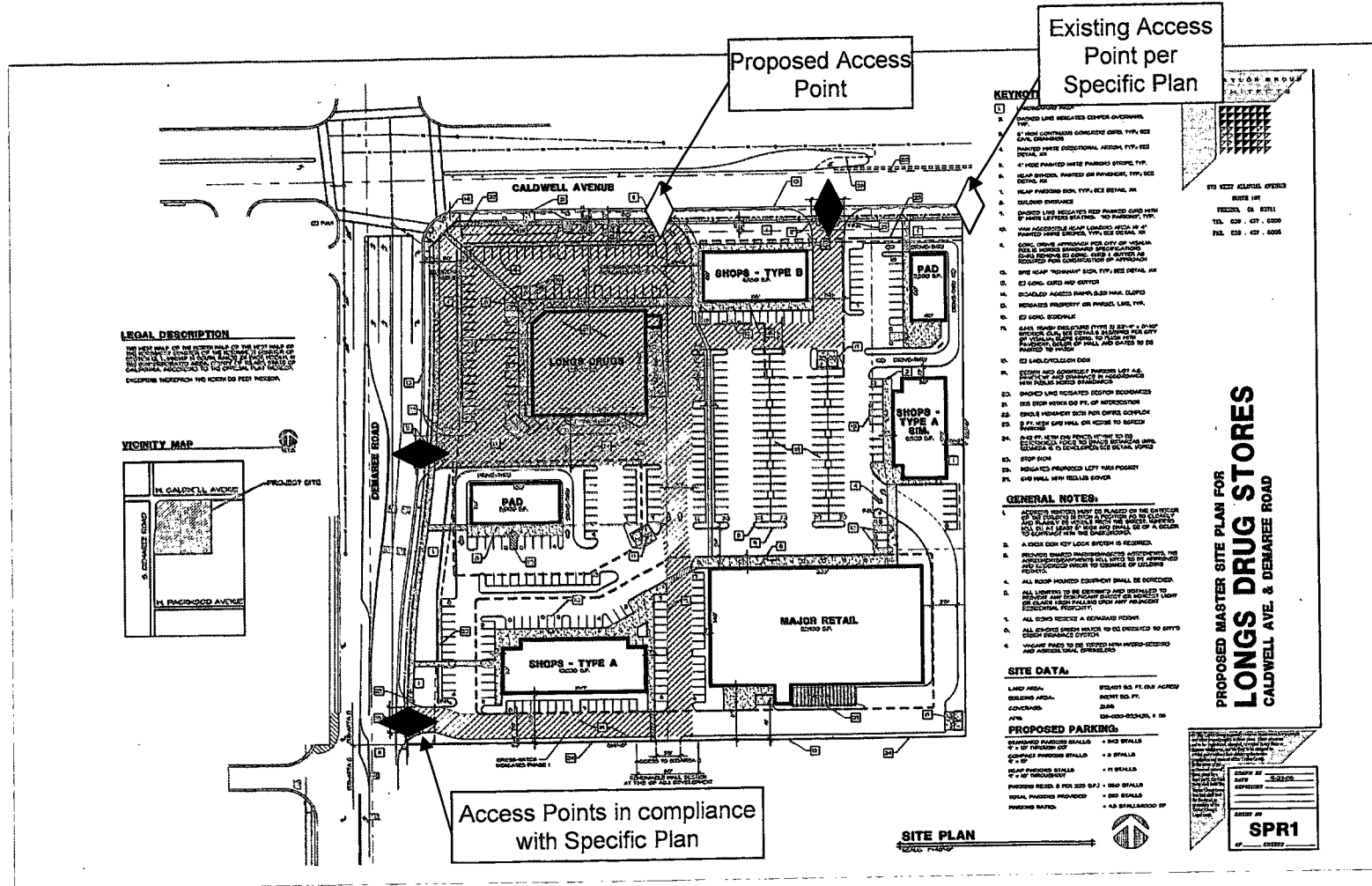
- WATERWAYS
- +— RAILROADS
- PARCELS
- COMMUNITY COMMERCIAL
- OFFICE GARDEN
- PROFESSIONAL / ADMIN. OFFICE
- QUASI-PUBLIC
- R-1-2 (SINGLE-FAMILY RES.)
- R-M-2 (MULTI-FAMILY RES.)

ZONING



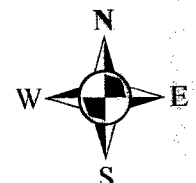
The Taylor Group

Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32

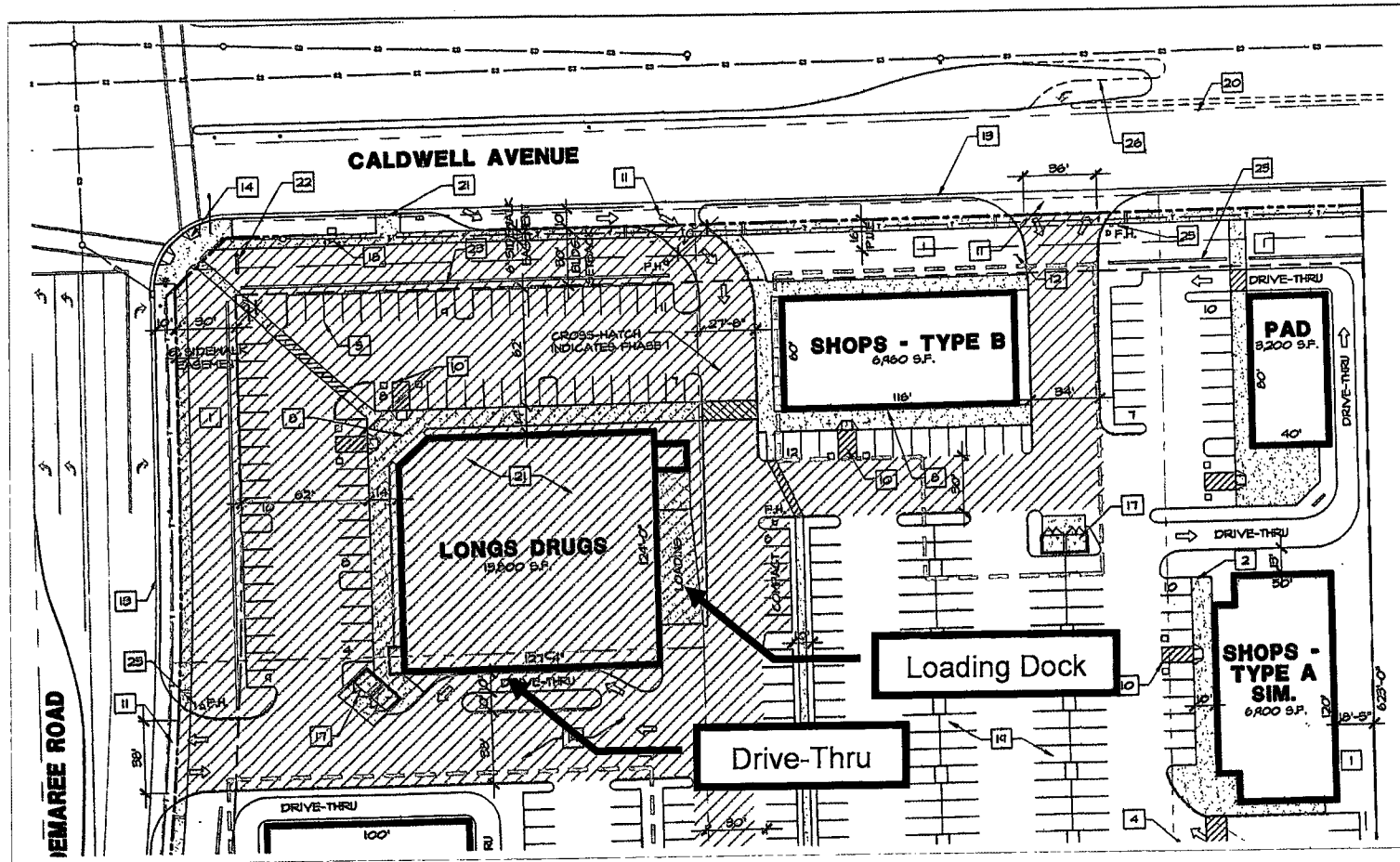


Specific Plan Amendment

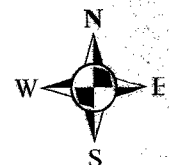
- Relocation of Caldwell Avenue Access Point
- Shaded area as Phase One for phased development of this sub-area



The Taylor Group
Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32



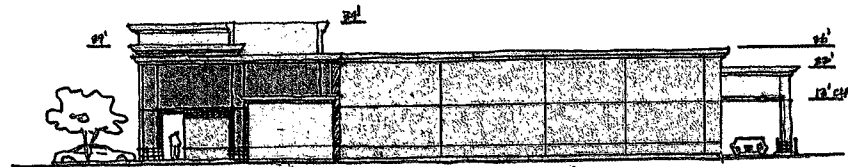
CUP Site Plan



The Taylor Group
Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32



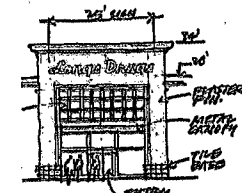
NORTH ELEVATION



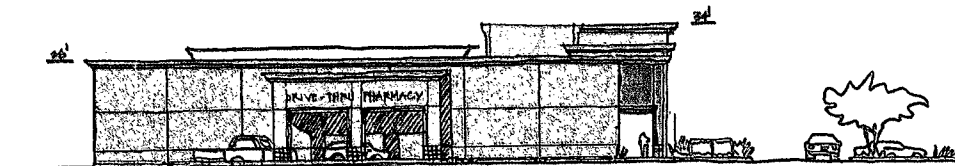
SOUTH ELEVATION



WEST ELEVATION

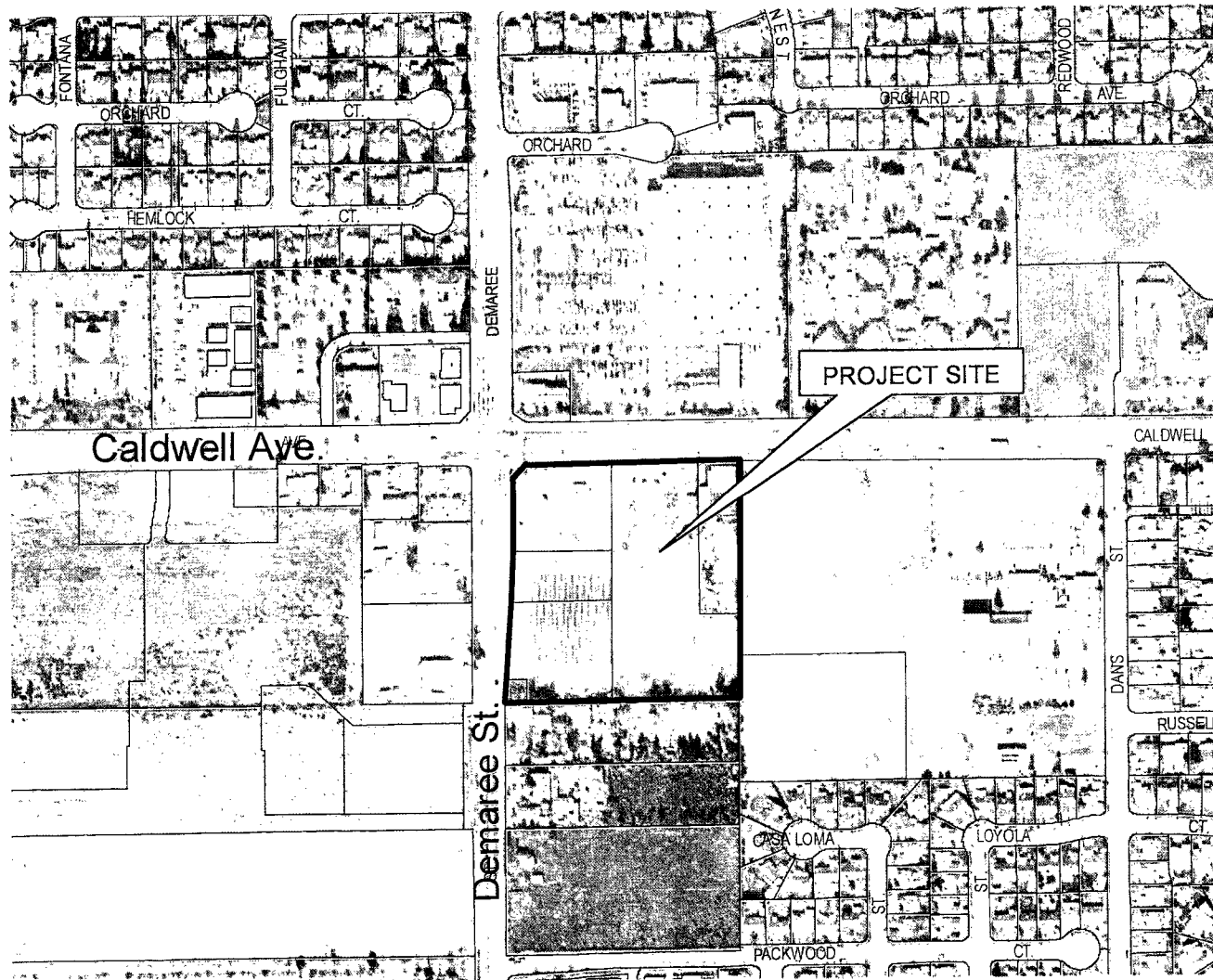


TOWER HEAD-ON ELEVATION



EAST ELEVATION

The Taylor Group
Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32



AERIAL MAP

The Taylor Group

Specific Plan Amend. No. 2006-02 & Conditional Use Permit No. 2006-32

Staff Recommendation

- Adopt Resolution No. 2006-90
- Adopt Resolution No. 2006-91

Basis for Findings

- Request for Specific Plan Amendment and Conditional Use Permit are consistent with the Demaree-Caldwell Specific Plan, General Plan, and Zoning Ordinance

THE TAYLOR GROUP ARCHITECTS

978 W. ALLUVIAL AVE. • STE. 107 • FRESNO, CA 93711 • PH 559.437.5000 • FAX 559.437.5005

FACSIMILE TRANSMITTAL

Date: 9/11/06No. of pages including cover sheet: 1To: Andrew ChamberlainCompany: City of Visalia

Subject: _____

Phone: _____

Fax Phone: 713-4814CC: 650-1321 Tom AndersonFrom: Brian RichmondProject: Long's Visalia

Project #: _____

Phone: (559) 437-5000

Fax Phone: (559) 437-5005

Time: 10:15

REMARKS:

☐ As Requested☐ Urgent☐ F.Y.I.☒ Please Respond☐ For Your Review

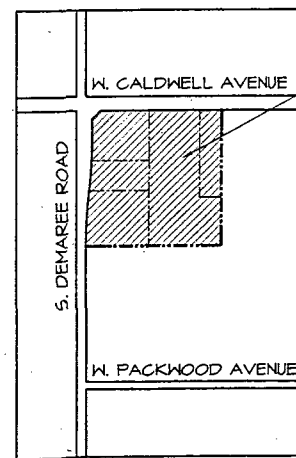
Andrew — We would like to request a continuance on the CUP and amendment to the specific plan for the Long's project at the southeast corner of Demaree and Caldwell. We would like it to be continued to Sept. 25 Planning Commission meeting.

Thank you

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MDB4M, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

VICINITY MAP



PROJECT SITE



NTS

DEMAREE ROAD

CALDWELL AVENUE

SUB-AREA B
SUB-AREA C

HEAD

SIGNAL

CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION AT TIME OF ADJ. DEVELOPMENT

LONGS DRUGS
15,800 S.F.

SHOPS - TYPE A
10,038 S.F.

SHOPS - TYPE B
6,960 S.F.

SHOPS - TYPE A SIM.
6,900 S.F.

MAJOR RETAIL
32,400 S.F.

PAD
3,200 S.F.

PAD
5,000 S.F.

SITE PLAN

SCALE: 1"=40'-0"

KEYNOTES:

- LANDSCAPING AREA
- DASHED LINE INDICATES BUMPER OVERHANG, TYP.
- 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
- PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
- 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
- HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
- HCAP PARKING SIGN, TYP.; SEE DETAIL XX
- BUILDING ENTRANCE
- DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING: "NO PARKING", TYP.
- VAN ACCESSIBLE HCAP LOADING AREA W/ 4" PAINTED WHITE STRIPES, TYP.; SEE DETAIL XX
- CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
- SITE HCAP "TOWAWAY" SIGN, TYP.; SEE DETAIL XX
- (E) CONC. CURB AND GUTTER
- DISABLED ACCESS RAMP; (1:20 MAX. SLOPE)
- INDICATES PROPERTY OR PARCEL LINE, TYP.
- (E) CONC. SIDEWALK
- C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CLR.; SEE DETAILS 243/SPR2 PER CITY OF VISALIA; SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
- (E) CABLE/TELECOM BOX
- DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
- DASHED LINE INDICATES SECTION BOUNDARIES
- BUS STOP WITHIN 150 FT. OF INTERSECTION
- SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
- 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
- 8'-12 FT. HIGH CMU FENCE, HEIGHT TO BE DETERMINED; FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL USPR2
- STOP SIGN
- INDICATES PROPOSED LEFT TURN POCKET
- CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

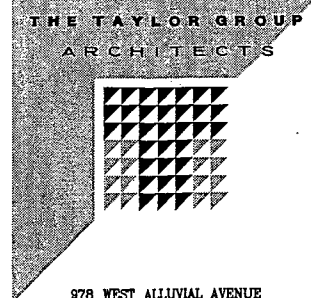
- ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO BE CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
- A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
- PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
- ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
- ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
- ALL SIGNS REQUIRE A SEPARATE PERMIT.
- ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
- VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	572,487 SQ. FT. (13.0 ACRES)
BUILDING AREA:	80,747 SQ. FT.
COVERAGE:	21.8%
APN:	126-030-333435, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGHOUT	= 342 STALLS
COMPACT PARKING STALLS 4' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGHOUT	= 17 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 365 STALLS
PARKING RATIO:	= 45 STALLS/1000 SF



978 WEST ALLUVIAL AVENUE
SUITE 107
FRESNO, CA 93711
TEL 559 . 437 . 5000
FAX 559 . 437 . 5005

Phase 1
- 72 parking stalls

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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OF SHEETS	

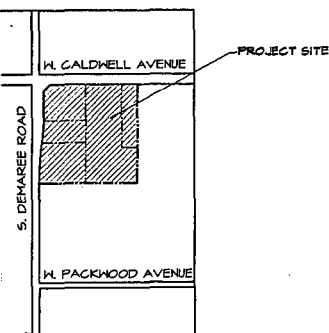
Oct. 4, 2006

Off Agenda SPR 06-02

LEGAL DESCRIPTION

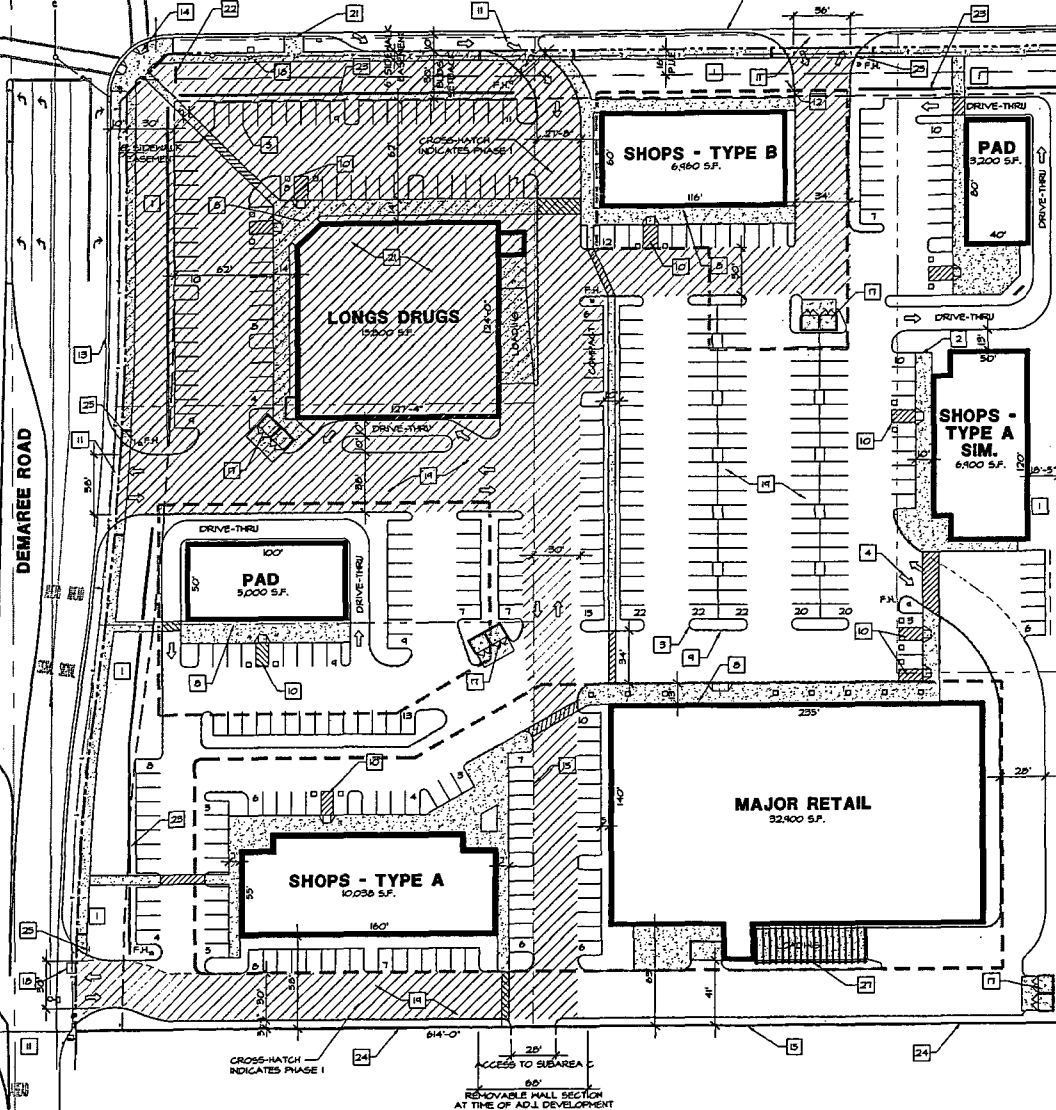
THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 18 SOUTH, RANGE 24 EAST, MOORE IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, ACCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

CITY MAP



DEMAREE ROAD

CALDWELL AVENUE



KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 5" WHITE LETTERS STATING "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4" PAINTED WHITE STRIPE, TYP.; SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-10), REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP "TOWHAY" SIGN, TYP.; SEE DETAIL XX
13. (E) CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP (1:20 MAX. SLOPE)
15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (E) CONC. SIDEWALK
17. C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CURB; SEE DETAILS 2A, 2B, 2C PER CITY OF VISALIA SLOPE CONC. TO FLUSH WITH PAVEMENT, COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
18. (E) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CMU FENCE HEIGHT TO BE DETERMINED, FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED SEE DETAIL USPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO BE CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS, THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
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5. ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
6. ALL SIGNS REQUIRE A SEPARATE PERMIT.
7. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
8. VACANT PADS TO BE TIERED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

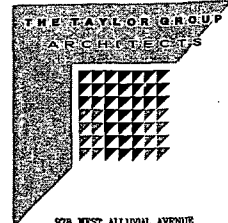
LAND AREA:	571,481 SQ. FT. (13.0 ACRES)
BUILDING AREA:	80,747 SQ. FT.
COVERAGE:	21.0%
APN:	126-090-352420, ± 36

PROPOSED PARKING:

STANDARD PARKING STALLS 4' x 12' THROUGH OUT	• 342 STALLS
COMPACT PARKING STALLS 4' x 15'	• 6 STALLS
HCAP PARKING STALLS 4' x 12' THROUGH OUT	• 17 STALLS
PARKING REGD. (1 PER 225 S.F.)	• 360 STALLS
TOTAL PARKING PROVIDED	• 565 STALLS
PARKING RATIO	• 4.3 STALLS/1000 S.F.

SITE PLAN

SCALE: 1"=40'-0"



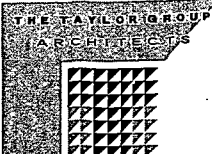
978 WEST ALLUVIAL AVENUE
SUITE 107
PESNO, CA 93711
TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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SPR1	
OF SHEETS	



978 WEST ALLIANCE AVENUE
SUITE 107
PESANO, CA 93711
TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 5" WHITE LETTERS STATING "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4' PAINTED WHITE STRIPES, TYP.; SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (B) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP "TOWAWAY" SIGN, TYP.; SEE DETAIL XX
13. (B) CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP; (1:20 MAX. SLOPE)
15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (B) CONC. SIDEWALK
17. CHU TRASH ENCLOSURE (TYPE 2) 22'-4" x 5'-10" INTERIOR CURB; SEE DETAILS 2.13/SPR2 PER CITY OF VISALIA SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF HALL AND GATES TO BE PAINTED TO MATCH
18. (B) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CHU HALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CHU FENCE HEIGHT TO BE DETERMINED; FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL 15/SPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CHU HALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO BE CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
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6. ALL SIGNS REQUIRE A SEPARATE PERMIT.
7. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
8. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	372,481 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,771 SQ. FT.
COVERAGE:	21.6%
APN:	126-C20-355,420, ± 56

PROPOSED PARKING:

STANDARD PARKING STALLS 4' x 10' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 4' x 10'	= 6 STALLS
HCAP PARKING STALLS 4' x 10' THROUGHOUT	= 11 STALLS
PARKING REGD. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 965 STALLS
PARKING RATIO:	= 4.5 STALLS/1000 S.F.

SITE PLAN

SCALE: 1"=40'-0"

PROPOSED MASTER SITE PLAN FOR LONGS DRUG STORES CALDWELL AVE. & DEMAREE ROAD

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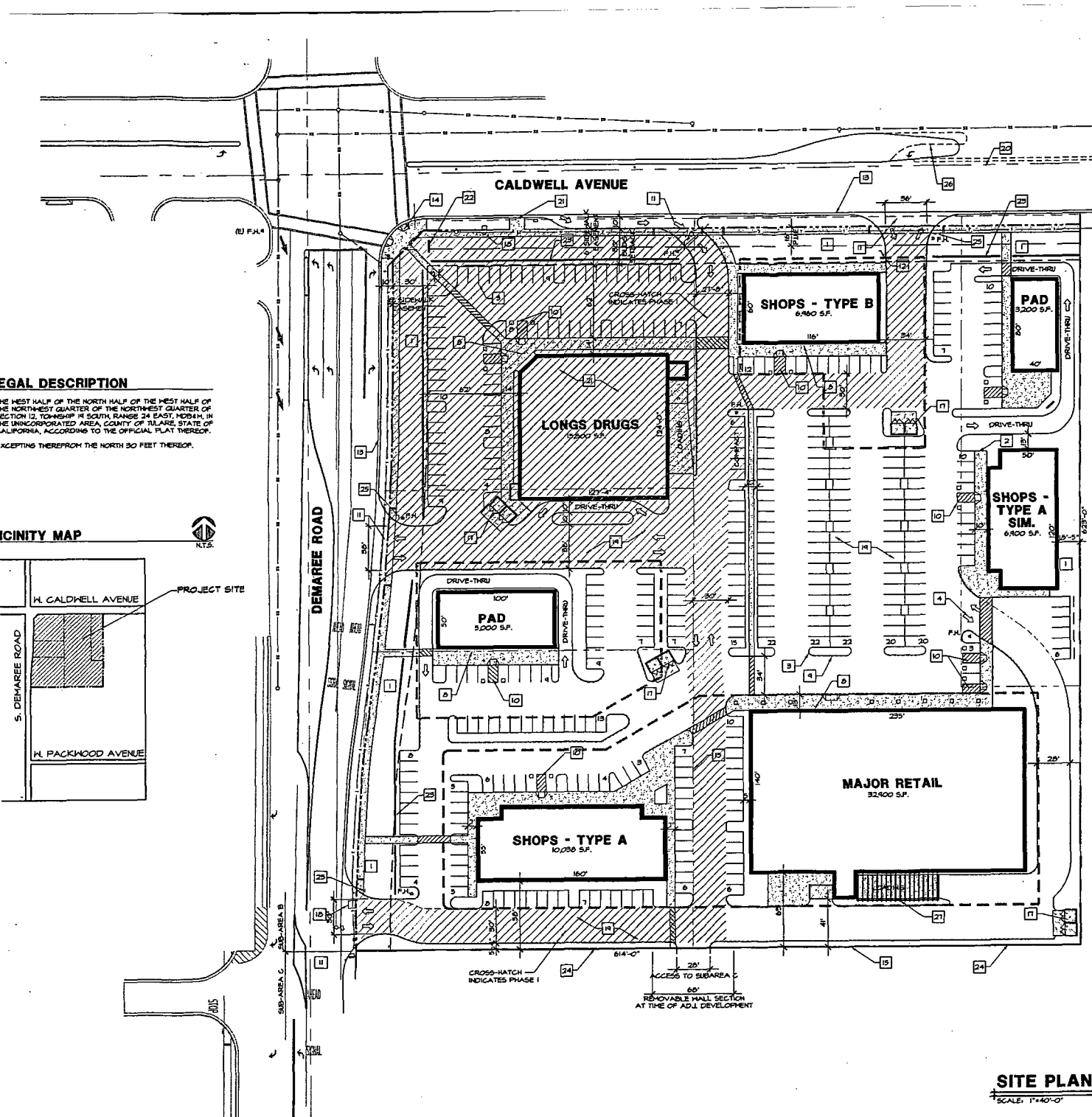
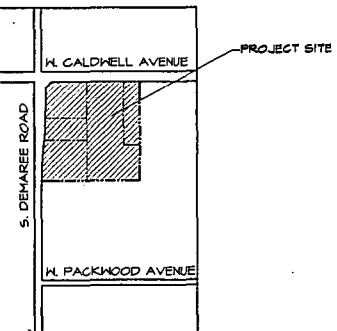
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DATE
REVISIONS

SHEET NO
SPR1
OF SHEETS

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MONSIEUR, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

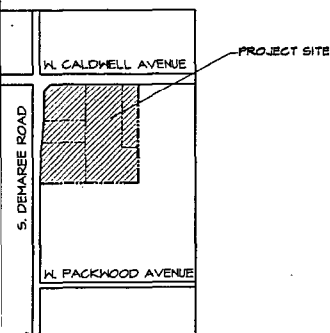
NEIGHBORHOOD MAP



LEGAL DESCRIPTION

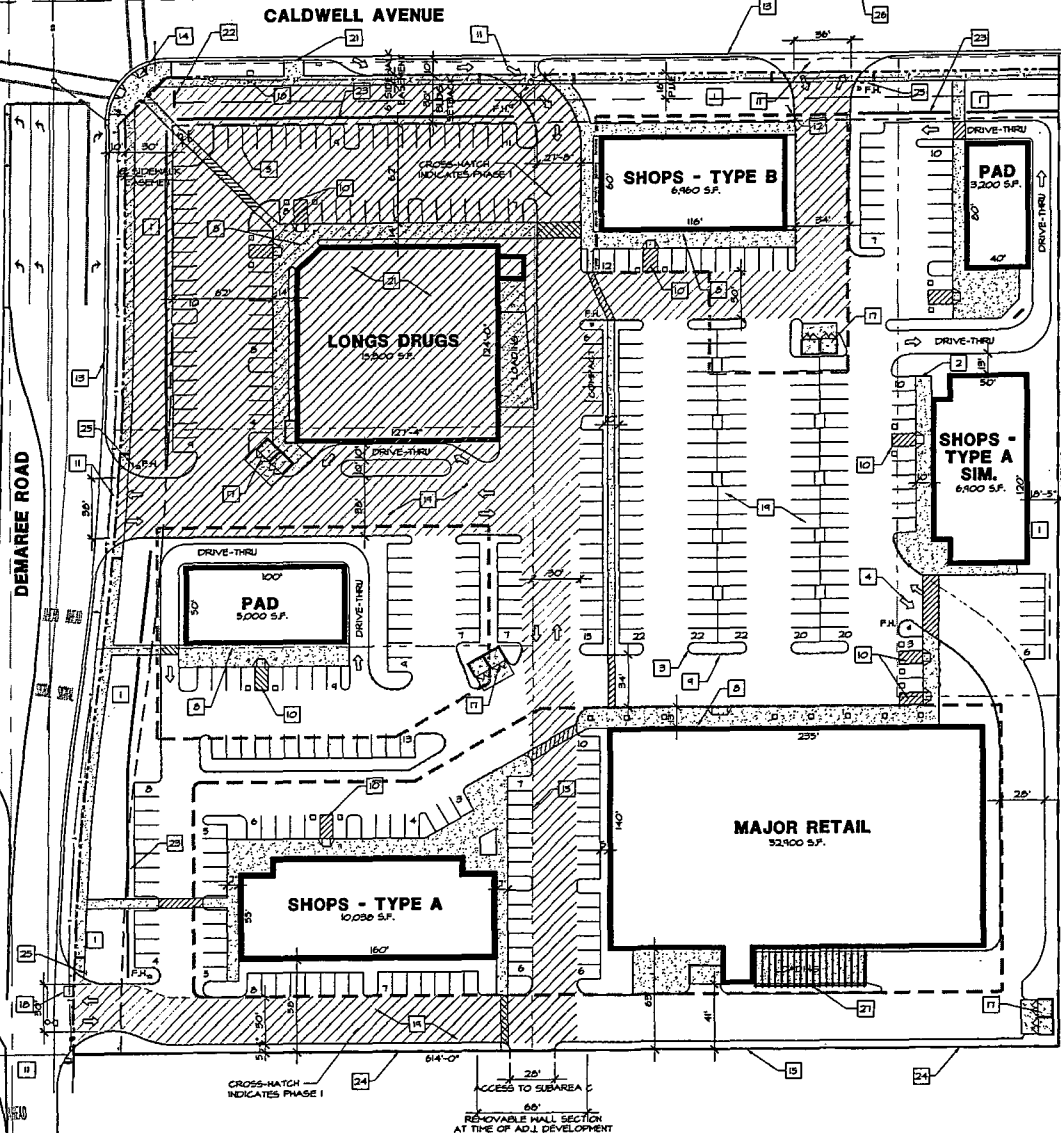
THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 11 SOUTH, RANGE 24 EAST, M084N, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

CITY MAP



DEMAREE ROAD

CALDWELL AVENUE



KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUFFER OVERHANGS, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP., SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP., SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP., SEE DETAIL XX
7. HCAP PARKING SIGN, TYP., SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4' PAINTED WHITE STRIPES, TYP., SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-H) REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP "TOWAWAY" SIGN, TYP., SEE DETAIL XX
13. (E) CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP, (0.20 MAX. SLOPE)
15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (E) CONC. SIDEWALK
17. C.H.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CLR.; SEE DETAILS 21.3/5/12 PER CITY OF VISALIA. SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
18. (E) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CHU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CHU FENCE, HEIGHT TO BE DETERMINED; FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL 15/12
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CHU WALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO BE CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS, THE AGREEMENTS/AGREEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
4. ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
5. ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
6. ALL SIGNS REQUIRE A SEPARATE PERMIT.
7. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
8. VACANT PADS TO BE TURPED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

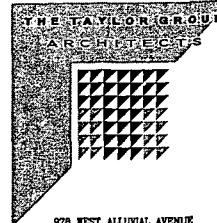
LAND AREA:	872,481 SQ. FT. (19.5 ACRES)
BUILDING AREA:	80,771 SQ. FT.
COVERAGE:	21.2%
APN:	026-050-25,5435, 1 56

PROPOSED PARKING:

STANDARD PARKING STALLS 8' x 18' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 8' x 15'	= 6 STALLS
HCAP PARKING STALLS 8' x 18' THROUGHOUT	= 11 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 359 STALLS
PARKING RATIO	= 4.5 STALLS/1000 SF

SITE PLAN

SCALE: 1"=40'-0"

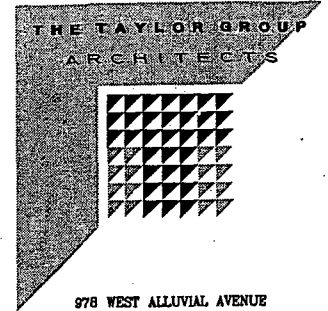


978 WEST ALLIANT AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559. 437. 5000
FAX. 559. 437. 5005

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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DRAWN BY
DATE 4-27-06
REVISED BY
DATE
SHEET NO.
SPR1
OF SHEETS



978 WEST ALLUVIAL AVENUE
SUITE 107
PRESNO, CA 93711
TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANGS, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING: "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4" PAINTED WHITE STRIPES, TYP.; SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP "TOWAWAY" SIGN, TYP.; SEE DETAIL XX
13. (E) CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP; (1:20 MAX. SLOPE)
15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (E) CONC. SIDEWALK
17. C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CLR.; SEE DETAILS 243/SPR2 PER CITY OF VISALIA; SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
18. (E) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CMU FENCE; HEIGHT TO BE DETERMINED; FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL 1/SPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
4. ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
5. ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
6. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
7. ALL SIGNS REQUIRE A SEPARATE PERMIT.
8. ALL VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	372,487 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,791 SQ. FT.
COVERAGE:	21.8%
APN:	126-030-33,34,35, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGHOUT	= 11 STALLS
PARKING REGD. (1 PER 225 SF.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 363 STALLS
PARKING RATIO:	= 45 STALLS/1000 SF

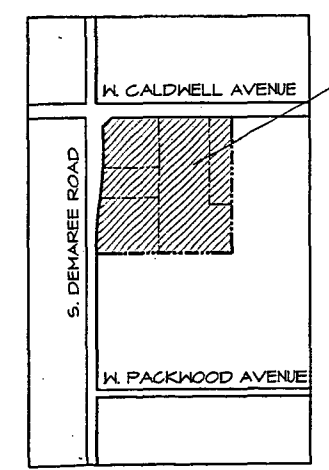
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MDB4M, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

VICINITY MAP

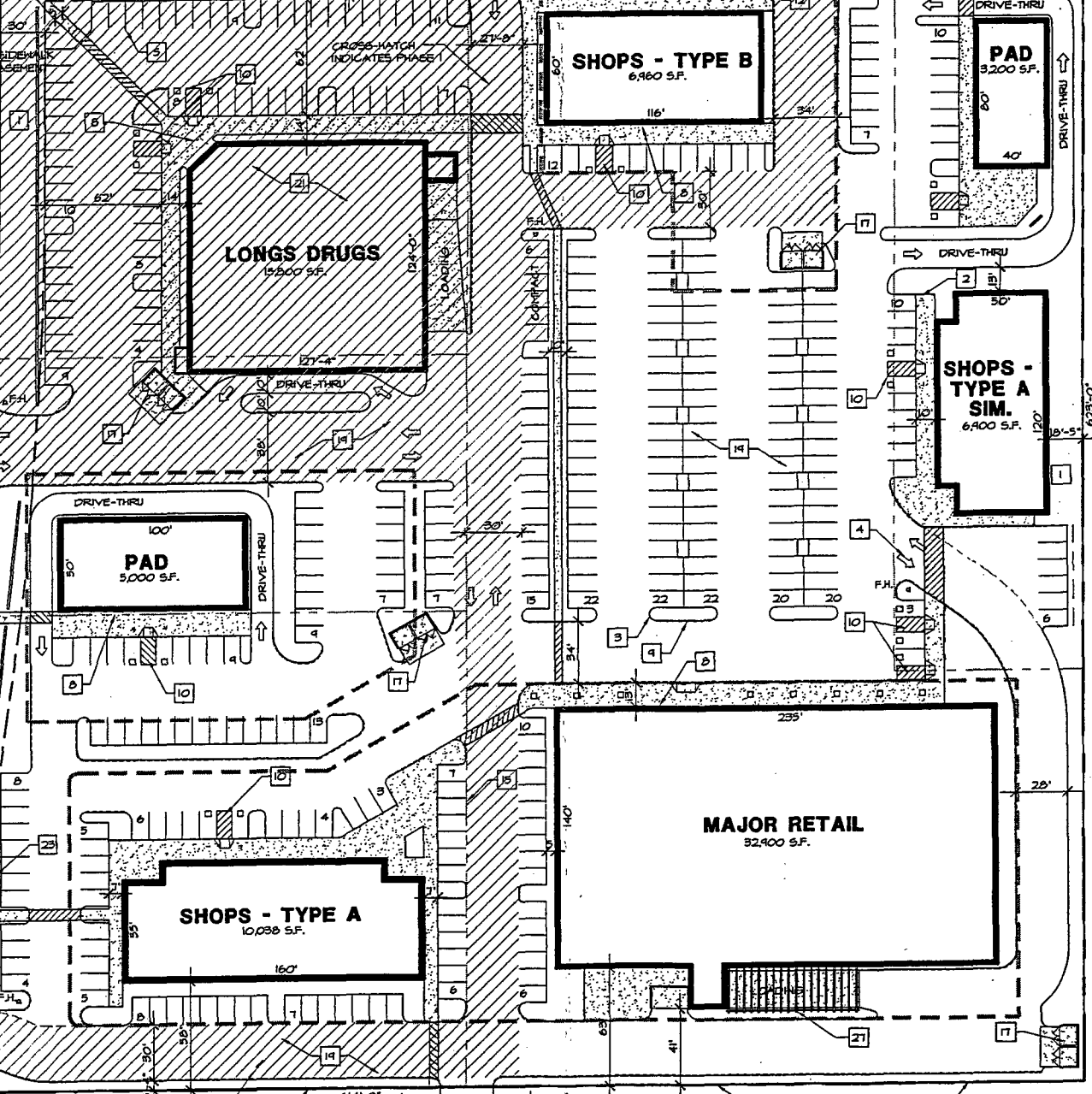


PROJECT SITE

DEMAREE ROAD

CALDWELL AVENUE

SUB-AREA B
SUB-AREA C



CROSS-HATCH INDICATES PHASE I
ACCESS TO SUBAREA C
REMOVABLE WALL SECTION AT TIME OF ADJ. DEVELOPMENT

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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Drawn by: [Signature]
Date: 9-21-06
Revisions: [Blank]
SHEET NO: SPR1
OF SHEETS: [Blank]

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING: "NO PARKING", TYP.
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11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
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14. DISABLED ACCESS RAMP; (1:20 MAX. SLOPE)
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17. C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CLR.; SEE DETAILS 243/SPR2 PER CITY OF VISALIA; SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
18. (E) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CMU FENCE HEIGHT TO BE DETERMINED; FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL VSPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
4. ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
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7. ALL SIGNS REQUIRE A SEPARATE PERMIT.
8. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
9. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	512,481 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,741 SQ. FT.
COVERAGE:	21.6%
APN:	126-030-3334.35, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGHOUT	= 17 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 365 STALLS
PARKING RATIO:	= 4.5 STALLS/1000 SF

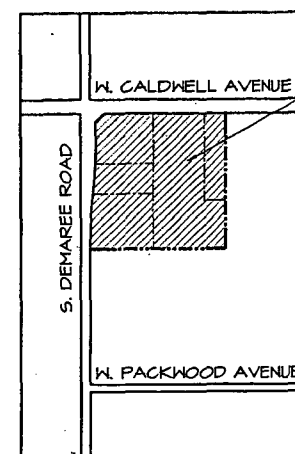
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MDB4M, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 50 FEET THEREOF.

VICINITY MAP



PROJECT SITE

DEMAREE ROAD

CALDWELL AVENUE

LONGS DRUGS
5,500 S.F.

SHOPS - TYPE B
6,960 S.F.

PAD
3,200 S.F.

SHOPS - TYPE A SIM.
6,400 S.F.

PAD
5,000 S.F.

MAJOR RETAIL
32,400 S.F.

SHOPS - TYPE A
10,038 S.F.

CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION AT TIME OF ADJ. DEVELOPMENT

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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DRAWN BY
DATE 9-27-06
REVISIONS
SHEET NO
SPR1
OF SHEETS

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
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6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 5" WHITE LETTERS STATING "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4" PAINTED WHITE STRIPES, TYP.; SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
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20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CMU FENCE; HEIGHT TO BE DETERMINED. FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL 15/SPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDINGS IN SUCH A POSITION AS TO CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
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9. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	972,487 SQ. FT. (2.5 ACRES)
BUILDING AREA:	80,747 SQ. FT.
COVERAGE:	21.8%
APN:	126-030-333435, & 56

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGHOUT	= 17 STALLS
PARKING REQ. (1 PER 225 SF.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 365 STALLS
PARKING RATIO:	= 4.5 STALLS/1000 SF

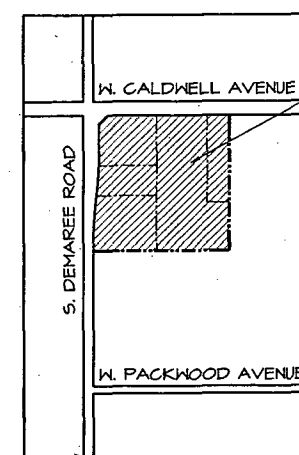
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MDB4M, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF.
EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

VICINITY MAP



PROJECT SITE

DEMAREE ROAD

CALDWELL AVENUE

LONGS DRUGS
15,500 S.F.

SHOPS - TYPE B
6,960 S.F.

PAD
3,200 S.F.

SHOPS - TYPE A SIM.
6,400 S.F.

PAD
5,000 S.F.

MAJOR RETAIL
32,400 S.F.

SHOPS - TYPE A
10,038 S.F.

CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION
AT TIME OF ADJ. DEVELOPMENT

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

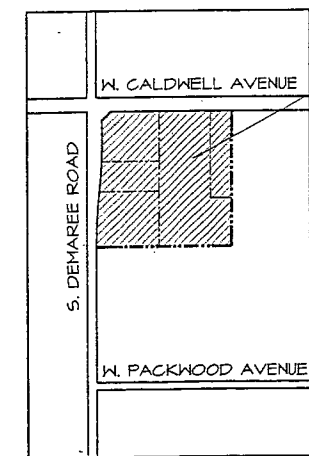
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DRAWN BY
DATE 4-27-06
REVISIONS
SHEET NO.
SPR1
OF SHEETS

LEGAL DESCRIPTION

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VICINITY MAP



PROJECT SITE



N.T.S.

DEMAREE ROAD

CALDWELL AVENUE

SUB-AREA C

SUB-AREA B

SUB-AREA A

SUB-AREA D

SUB-AREA E

SUB-AREA F

SUB-AREA G

SUB-AREA H

SUB-AREA I

SUB-AREA J

SUB-AREA K

SUB-AREA L

SUB-AREA M

SUB-AREA N

SUB-AREA O

SUB-AREA P

SUB-AREA Q

SUB-AREA R

SUB-AREA S

SUB-AREA T

SUB-AREA U

SUB-AREA V

SUB-AREA W

SUB-AREA X

SUB-AREA Y

SUB-AREA Z

SUB-AREA AA

SUB-AREA AB

SUB-AREA AC

SUB-AREA AD

SUB-AREA AE

SUB-AREA AF

SUB-AREA AG

SUB-AREA AH

SUB-AREA AI

SUB-AREA AJ

SUB-AREA AK

SUB-AREA AL

SUB-AREA AM

SUB-AREA AN

SUB-AREA AO

SUB-AREA AP

SUB-AREA AQ

SUB-AREA AR

SUB-AREA AS

SUB-AREA AT

SUB-AREA AU

SUB-AREA AV

SUB-AREA AW

SUB-AREA AX

SUB-AREA AY

SUB-AREA AZ

SUB-AREA BA

SUB-AREA BB

SUB-AREA BC

SUB-AREA BD

SUB-AREA BE

SUB-AREA BF

SUB-AREA BG

SUB-AREA BH

SUB-AREA BI

SUB-AREA BJ

SUB-AREA BK

SUB-AREA BL

SUB-AREA BM

SUB-AREA BN

SUB-AREA BO

SUB-AREA BP

SUB-AREA BQ

SUB-AREA BR

SUB-AREA BS

SUB-AREA BT

SUB-AREA BU

SUB-AREA BV

SUB-AREA BW

SUB-AREA BX

SUB-AREA BY

SUB-AREA BZ

SUB-AREA CA

SUB-AREA CB

SUB-AREA CC

SUB-AREA CD

SUB-AREA CE

SUB-AREA CF

SUB-AREA CG

SUB-AREA CH

SUB-AREA CI

SUB-AREA CJ

SUB-AREA CK

SUB-AREA CL

SUB-AREA CM

SUB-AREA CN

SUB-AREA CO

SUB-AREA CP

SUB-AREA CQ

SUB-AREA CR

SUB-AREA CS

SUB-AREA CT

SUB-AREA CU

SUB-AREA CV

SUB-AREA CW

SUB-AREA CX

SUB-AREA CY

SUB-AREA CZ

SUB-AREA DA

SUB-AREA DB

SUB-AREA DC

SUB-AREA DD

SUB-AREA DE

SUB-AREA DF

SUB-AREA DG

SUB-AREA DH

SUB-AREA DI

SUB-AREA DJ

SUB-AREA DK

SUB-AREA DL

SUB-AREA DM

SUB-AREA DN

SUB-AREA DO

SUB-AREA DP

SUB-AREA DQ

SUB-AREA DR

SUB-AREA DS

SUB-AREA DT

SUB-AREA DU

SUB-AREA DV

SUB-AREA DW

SUB-AREA DX

SUB-AREA DY

SUB-AREA DZ

SUB-AREA EA

SUB-AREA EB

SUB-AREA EC

SUB-AREA ED

SUB-AREA EE

SUB-AREA EF

SUB-AREA EG

SUB-AREA EH

SUB-AREA EI

SUB-AREA EJ

SUB-AREA EK

SUB-AREA EL

SUB-AREA EM

SUB-AREA EN

SUB-AREA EO

SUB-AREA EP

SUB-AREA EQ

SUB-AREA ER

SUB-AREA ES

SUB-AREA ET

SUB-AREA EU

SUB-AREA EV

SUB-AREA EW

SUB-AREA EX

SUB-AREA EY

SUB-AREA EZ

SUB-AREA FA

SUB-AREA FB

SUB-AREA FC

SUB-AREA FD

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SUB-AREA FF

SUB-AREA FG

SUB-AREA FH

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SUB-AREA GZ

SUB-AREA HA

SUB-AREA HB

SUB-AREA HC

SUB-AREA HD

SUB-AREA HE

SUB-AREA HF

SUB-AREA HG

SUB-AREA HH

SUB-AREA HI

SUB-AREA HJ

SUB-AREA HK

SUB-AREA HL

SUB-AREA HM

SUB-AREA HN

SUB-AREA HO

SUB-AREA HP

SUB-AREA HQ

SUB-AREA HR

SUB-AREA HS

SUB-AREA HT

SUB-AREA HU

SUB-AREA HV

SUB-AREA HW

SUB-AREA HX

SUB-AREA HY

SUB-AREA HZ

SUB-AREA IA

SUB-AREA IB

SUB-AREA IC

SUB-AREA ID

SUB-AREA IE

SUB-AREA IF

SUB-AREA IG

SUB-AREA IH

SUB-AREA II

SUB-AREA IJ

SUB-AREA IK

SUB-AREA IL

SUB-AREA IM

SUB-AREA IN

SUB-AREA IO

SUB-AREA IP

SUB-AREA IQ

SUB-AREA IR

SUB-AREA IS

SUB-AREA IT

SUB-AREA IU

SUB-AREA IV

SUB-AREA IW

SUB-AREA IX

SUB-AREA IY

SUB-AREA IZ

SUB-AREA JA

SUB-AREA JB

SUB-AREA JC

SUB-AREA JD

SUB-AREA JE

SUB-AREA JF

SUB-AREA JG

SUB-AREA JH

SUB-AREA JI

SUB-AREA JJ

SUB-AREA JK

SUB-AREA JL

SUB-AREA JM

SUB-AREA JN

SUB-AREA JO

SUB-AREA JP

SUB-AREA JQ

SUB-AREA JR

SUB-AREA JS

SUB-AREA JT

SUB-AREA JU

SUB-AREA JV

SUB-AREA JW

SUB-AREA JX

SUB-AREA JY

SUB-AREA JZ

SUB-AREA KA

SUB-AREA KB

SUB-AREA KC

SUB-AREA KD

JEMAREE ROAD

CALDWELL AVENUE

SHOPS - TYPE B

6,960 S.F.

LONGS DRUGS

15,800 S.F.

PAD

3,200 S.F.

**SHOPS -
TYPE A
SIM.**

6,900 S.F.

CROSS-HATCH
INDICATES PHASE I

LOADING

COMPACT

DRIVE-THRU

DRIVE-THRU

DRIVE-THRU

6' SIDEWALK
EASEMENT

6' SIDEWALK
EASEMENT

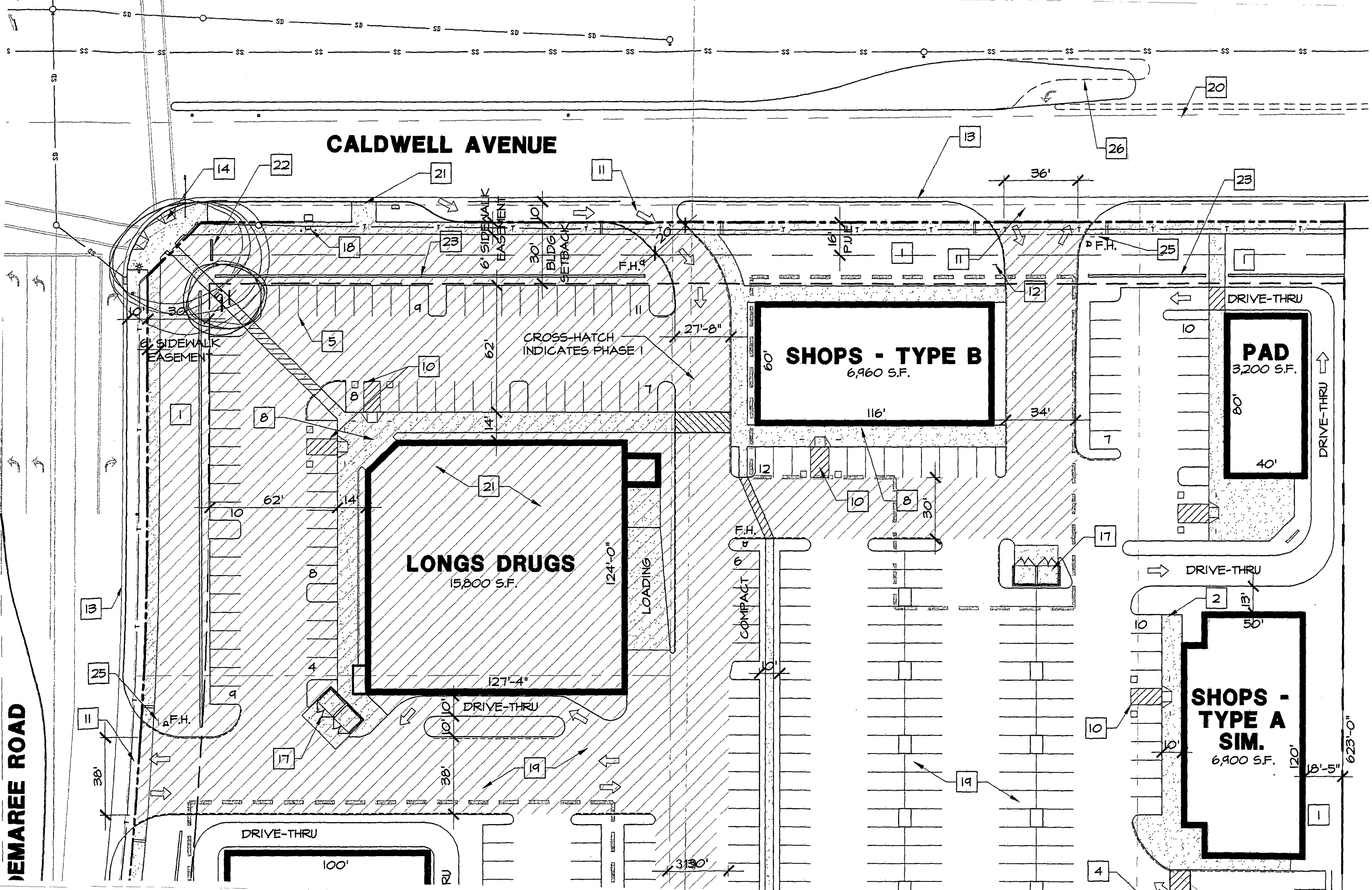
BLDG
SETBACK

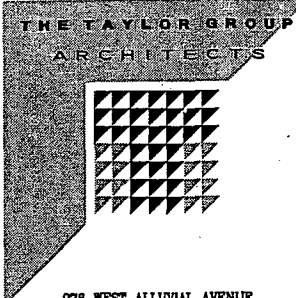
P.U.E.

P.F.H.

P.F.H.

P.F.H.





978 WEST ALLUVIAL AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559.437.5000
FAX. 559.437.5005

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
3. 6" HIGH CONTINUOUS CONCRETE CURB, TYP.; SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING "NO PARKING", TYP.
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11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP TOWAWAY SIGN, TYP.; SEE DETAIL XX
13. (E) CONC. CURB AND GUTTER
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15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (E) CONC. SIDEWALK
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19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
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21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8-12 FT. HIGH CMU FENCE; HEIGHT TO BE DETERMINED. FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL VSPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

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6. ALL SIGNS REQUIRE A SEPARATE PERMIT.
7. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
8. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	312,487 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,747 SQ. FT.
COVERAGE:	21.5%
APN:	126-030-3334.35, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH CUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGH CUT	= 17 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 365 STALLS
PARKING RATIO:	= 4.5 STALLS/1000 SF

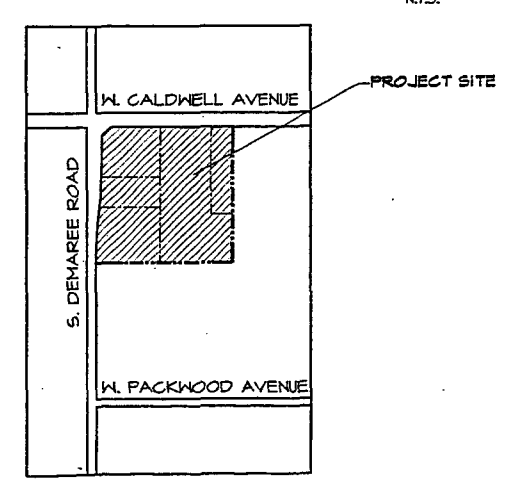
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

THE WEST HALF OF THE NORTH HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 14 SOUTH, RANGE 24 EAST, MDB&M, IN THE UNINCORPORATED AREA, COUNTY OF TULARE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 30 FEET THEREOF.

VICINITY MAP



CALDWELL AVENUE

DEMAREE ROAD

LONGS DRUGS
12,500 S.F.

SHOPS - TYPE B
6,400 S.F.

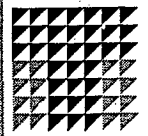
SHOPS - TYPE A SIM.
6,400 S.F.

MAJOR RETAIL
32,400 S.F.

SHOPS - TYPE A
10,030 S.F.

PAD
5,000 S.F.

PAD
3,200 S.F.



978 WEST ALLUVIAL AVENUE
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TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

KEYNOTES:

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25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES:

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9. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA:

LAND AREA:	372,481 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,791 SQ. FT.
COVERAGE:	21.8%
APN:	126-030-33,34,35, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH OUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGHOUT	= 17 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 365 STALLS
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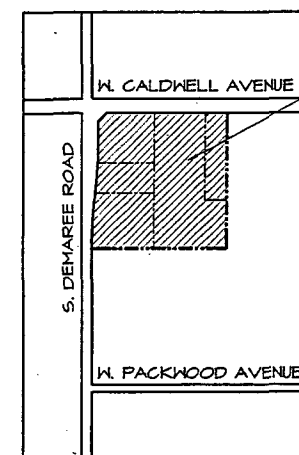
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

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VICINITY MAP



DEMAREE ROAD

CALDWELL AVENUE

LONGS DRUGS
15,500 S.F.

SHOPS - TYPE B
6,980 S.F.

SHOPS - TYPE A SIM.
6,900 S.F.

SHOPS - TYPE A
10,030 S.F.

MAJOR RETAIL
32,400 S.F.

CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION AT TIME OF ADJ. DEVELOPMENT

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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DRAWN BY: 9-27-06
DATE: 9-27-06
REVISIONS:
SHEET NO: SPR1
OF SHEETS:

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUMPER OVERHANG, TYP.
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SITE DATA:

LAND AREA:	372,481 SQ. FT. (8.5 ACRES)
BUILDING AREA:	80,741 SQ. FT.
COVERAGE:	21.6%
APN:	126-030-333435, & 36

PROPOSED PARKING:

STANDARD PARKING STALLS 9' x 18' THROUGH CUT	= 342 STALLS
COMPACT PARKING STALLS 9' x 15'	= 6 STALLS
HCAP PARKING STALLS 9' x 18' THROUGH CUT	= 17 STALLS
PARKING REQ. (1 PER 225 S.F.)	= 360 STALLS
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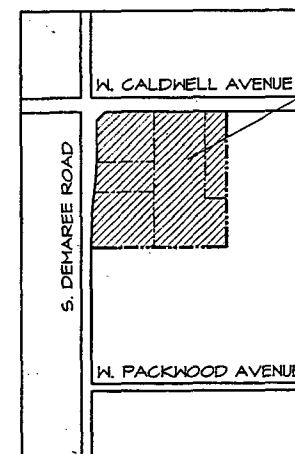
SITE PLAN

SCALE: 1"=40'-0"

LEGAL DESCRIPTION

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VICINITY MAP



DEMAREE ROAD

CALDWELL AVENUE

LONGS DRUGS
15,500 S.F.

SHOPS - TYPE B
6,960 S.F.

SHOPS - TYPE A SIM.
6,400 S.F.

MAJOR RETAIL
32,900 S.F.

SHOPS - TYPE A
10,038 S.F.

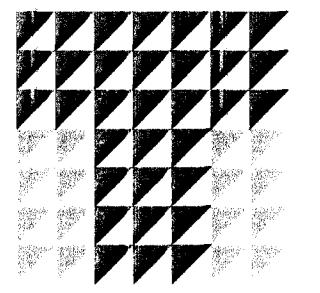
CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION AT TIME OF ADJ. DEVELOPMENT

PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

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DRAWN BY
DATE 9-21-06
REVISIONS
SHEET NO
SPR1
OF SHEETS



978 WEST ALLUVIAL AVENUE

SUITE 107

FRESNO, CA 93711

TEL. 559 . 437 . 5000

FAX. 559 . 437 . 5005

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SITE DATA:

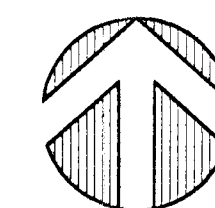
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SITE PLAN

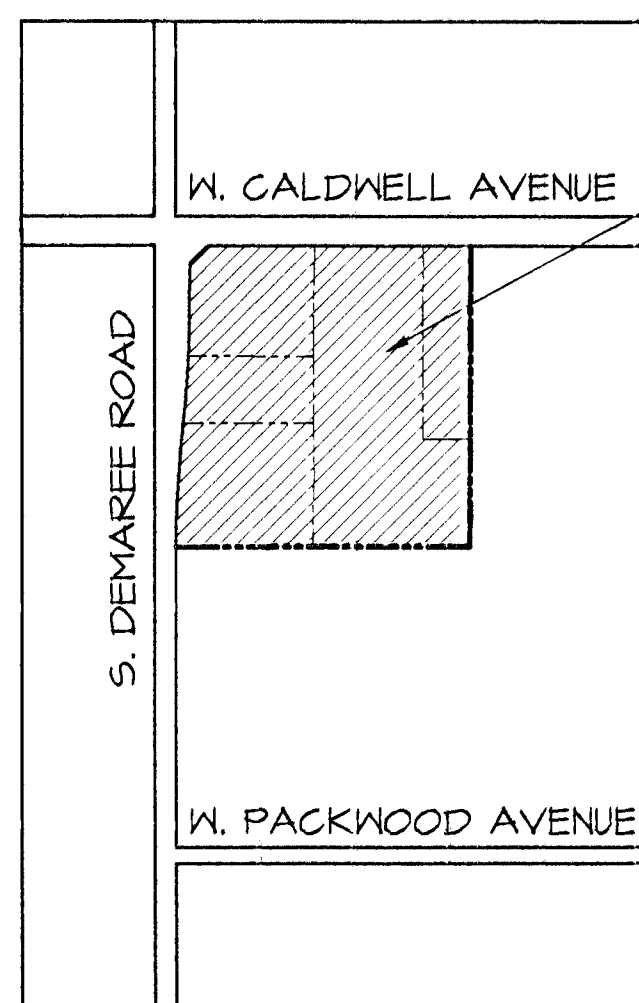
SCALE: 1"=40'-0"



LEGAL DESCRIPTION

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VICINITY MAP



PROJECT SITE

DEMAREE ROAD

CALDWELL AVENUE

LONGS DRUGS
15,800 S.F.

SHOPS - TYPE B
6,960 S.F.

SHOPS - TYPE A SIM.
6,900 S.F.

MAJOR RETAIL
32,900 S.F.

SHOPS - TYPE A
10,038 S.F.

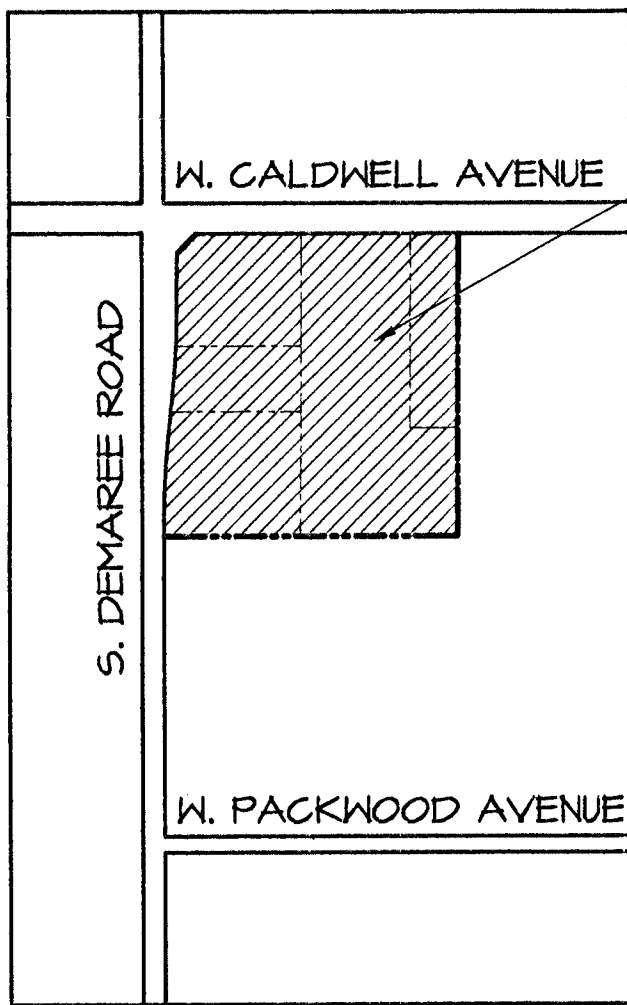
CROSS-HATCH INDICATES PHASE I

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VICINITY MAP



DEMAREE ROAD

CALDWELL AVENUE

(E) F.H.4

LONGS DRUGS
15,800 S.F.

SHOPS - TYPE B
12,000 S.F.

PAD
3,200 S.F.

SHOPS - TYPE A SIM.
6,900 S.F.

SHOPS - TYPE A
10,038 S.F.

MAJOR RETAIL
32,400 S.F.

CROSS-HATCH INDICATES PHASE I

ACCESS TO SUBAREA C
REMOVABLE WALL SECTION
AT TIME OF ADJ. DEVELOPMENT

SITE PLAN

SCALE: 1"=40'-0"

KEYNOTES.

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4. PAINTED WHITE DIRECTIONAL ARROW, TYP.; SEE DETAIL XX
5. 4" WIDE PAINTED WHITE PARKING STRIPE, TYP.
6. HCAP SYMBOL PAINTED ON PAVEMENT, TYP.; SEE DETAIL XX
7. HCAP PARKING SIGN, TYP.; SEE DETAIL XX
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH 3" WHITE LETTERS STATING: "NO PARKING", TYP.
10. VAN ACCESSIBLE HCAP LOADING AREA W/ 4" PAINTED WHITE STRIPES, TYP.; SEE DETAIL XX
11. CONC. DRIVE APPROACH PER CITY OF VISALIA PUBLIC WORKS STANDARD SPECIFICATIONS (C-14); REMOVE (E) CONC. CURB & GUTTER AS REQUIRED FOR CONSTRUCTION OF APPROACH
12. SITE HCAP "TOWAWAY" SIGN, TYP.; SEE DETAIL XX
13. (E) CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP; (1:20 MAX. SLOPE)
15. INDICATES PROPERTY OR PARCEL LINE, TYP.
16. (E) CONC. SIDEWALK
17. C.M.U. TRASH ENCLOSURE (TYPE 2) 22'-4" x 8'-10" INTERIOR CLR.; SEE DETAILS 243/SPR2 PER CITY OF VISALIA; SLOPE CONC. TO FLUSH WITH PAVEMENT; COLOR OF WALL AND GATES TO BE PAINTED TO MATCH
18. (E) CABLE/TELECOM BOX
19. DESIGN AND CONSTRUCT PARKING LOT A.C. PAVEMENT AND DRAINAGE IN ACCORDANCE WITH PUBLIC WORKS STANDARDS
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. BUS STOP WITHIN 150 FT. OF INTERSECTION
22. SINGLE MONUMENT SIGN FOR ENTIRE COMPLEX
23. 3 FT. HIGH CMU WALL OR HEDGE TO SCREEN PARKING
24. 8'-12 FT. HIGH CMU FENCE; HEIGHT TO BE DETERMINED. FENCE TO DIVIDE SUBAREAS UNTIL SUBAREA C IS DEVELOPED; SEE DETAIL 1/SPR2
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CMU WALL WITH TRELLIS COVER

GENERAL NOTES.

1. ADDRESS NUMBERS MUST BE PLACED ON THE EXTERIOR OF THE BUILDING IN SUCH A POSITION AS TO CLEARLY AND PLAINLY BE VISIBLE FROM THE STREET. NUMBERS WILL BE AT LEAST 6" HIGH AND SHALL BE OF A COLOR TO CONTRAST WITH THE BACKGROUND.
2. A KNOX BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS/EASEMENTS WILL NEED TO BE APPROVED AND RECORDED PRIOR TO ISSUANCE OF BUILDING PERMITS.
4. ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED.
5. ALL LIGHTING TO BE DESIGNED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT OR GLARE FROM FALLING UPON ANY ADJACENT RESIDENTIAL PROPERTY.
7. ALL SIGNS REQUIRE A SEPARATE PERMIT.
8. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY'S STORM DRAINAGE SYSTEM.
9. VACANT PADS TO BE TURFED WITH HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS

SITE DATA.

LAND AREA:	312,481 SQ. FT. (8.5 ACRES)
BUILDING AREA:	81,031 SQ. FT.
COVERAGE:	21.8%
APN:	126-030-333435, & 36

PROPOSED PARKING.

STANDARD PARKING STALLS 4' x 10' THROUGH OUT	= 341 STALLS
HCAP PARKING STALLS 4' x 10' THROUGH OUT	= 17 STALLS
PARKING REGD. (1 PER 225 S.F.)	= 360 STALLS
TOTAL PARKING PROVIDED	= 364 STALLS
PARKING RATIO:	= 4.5 STALLS/1000 SF





THE TAYLOR GROUP
ARCHITECTS

978 WEST ALLUVIAL AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559 . 437 . 5000
FAX. 559 . 437 . 5005

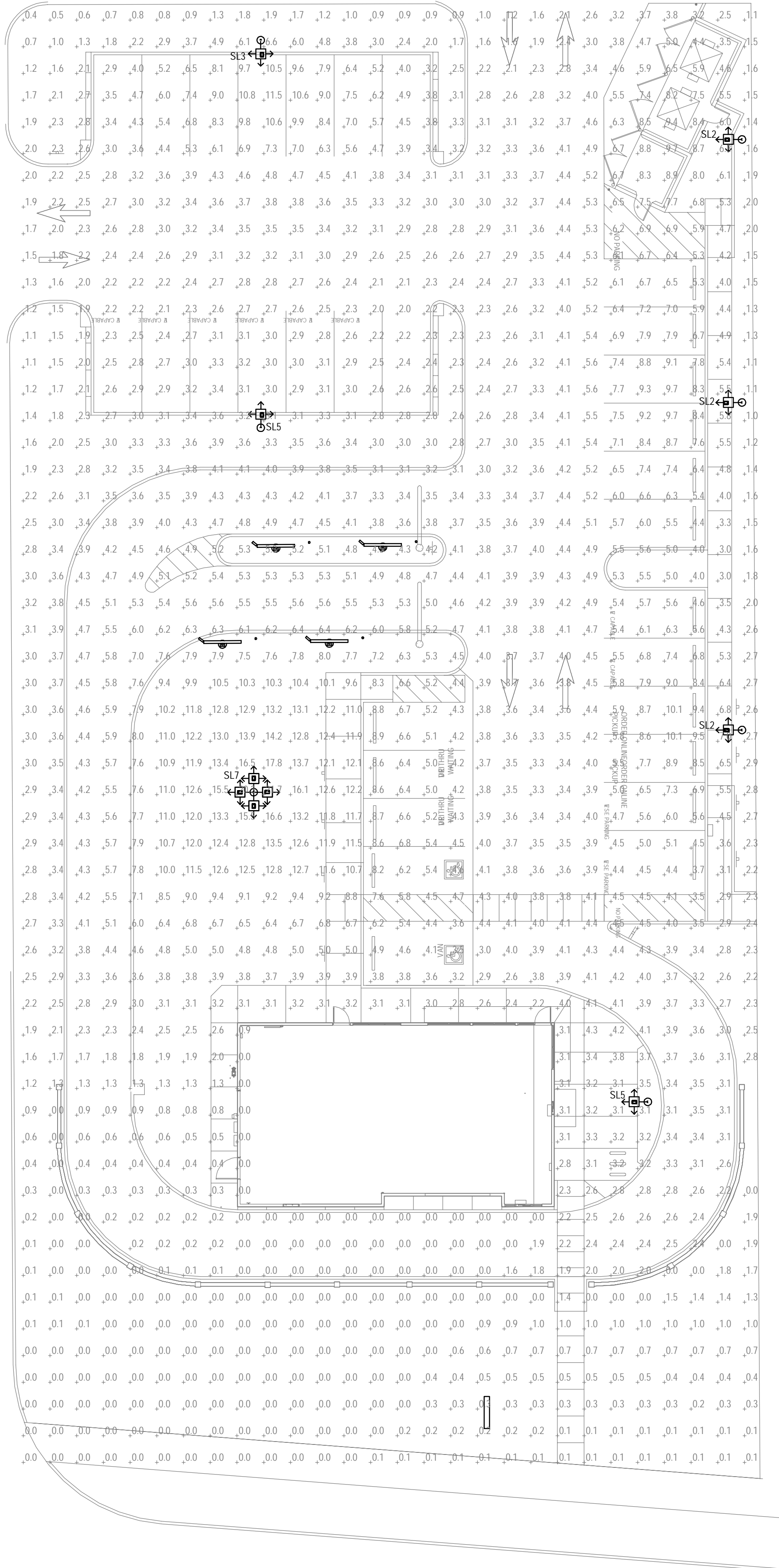
PROPOSED MASTER SITE PLAN FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD

DRAWN BY
DATE 5/17/06
REVISIONS

SHEET NO
SPR1
OF SHEETS

SCHEDULE						
SYMBOL	LABEL	MANUFACTURER	CATALOG NUMBER	LAMP	NUMBER LAMPS	LUMENS PER LAMP
	SL2	SOLAIS	GL3-17L-2RM-740-STD-10-1PS-00-SGY-ES-HSS	LED	1	ABSOLUTE
	SL3	SOLAIS	GL3-17L-3RM-740-STD-10-1PS-00-SGY-ES-HSS	LED	1	ABSOLUTE
	SL4	SOLAIS	GL3-17L-4WS-740-STD-10-1PS-00-SGY-ES	LED	1	ABSOLUTE
	SL7	SOLAIS	GL3-17L-4WS-740-STD-10-1PS-00-SGY-ES	LED	4	ABSOLUTE

STATISTICS						
DESCRIPTION	SYMBOL	AVG	MAX	MIN	MAX/MIN	AVG/MIN
ZONE 1 - SITE	XX	3.7 fc	21.7 fc	0.0 fc	N/A	N/A



 SITE PHOTOMETRIC PLAN
1" = 15' - 0"



Thorson-Baker + Associates
CONSULTING ENGINEERS

3030 West Streetsboro Road
Richfield, Ohio 44286

(330) 659-6688 Ph.
(330) 659-6675 Fax



in the box®

9357 SPECTRUM CENTER BLVD.
SAN DIEGO, CA 92123

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DATES

RELEASE: JULY 07, 2022

P.M. UPDATES: _____

SUBMITTAL DATE: _____

1: _____

2: _____

3: _____

BID: _____

CONSTRUCTION: _____

REVISIONS

△	_____
△	_____
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△	_____
△	_____



Architectural Solutions Group

6930 DESTINY DRIVE
SUITE 100
ROCKLIN, CA 95677

CONTACT: GREG BORCHARDT
PHONE: 916.303.4512
EMAIL: gborchart@pmdginc.com

KEN MCKRACKEN, ARCHITECT



REGISTERED PROFESSIONAL ENGINEER
JUSTIN HOSSEINI
24452
ELECTRICAL
STATE OF CALIFORNIA
EXPIRES: 3/31/2025

SITE INFORMATION

MK TYPE: MK12B_SM

JIB #: 5221

ADDRESS:
S. DEMAREE ST. &
W. CALDWELL AVE
VISALIA, CA 93277

DRAWN BY: TBA

PROJECT #: MFT24001.0

SCALE: _____

SITE PHOTOMETRIC PLAN ES1.1



Cercidium x 'Desert Museum' Thornless Palo Verde



Platanus racemosa 'Roberts' Roberts California Sycamore



Quercus englemannii Engelmann Oak



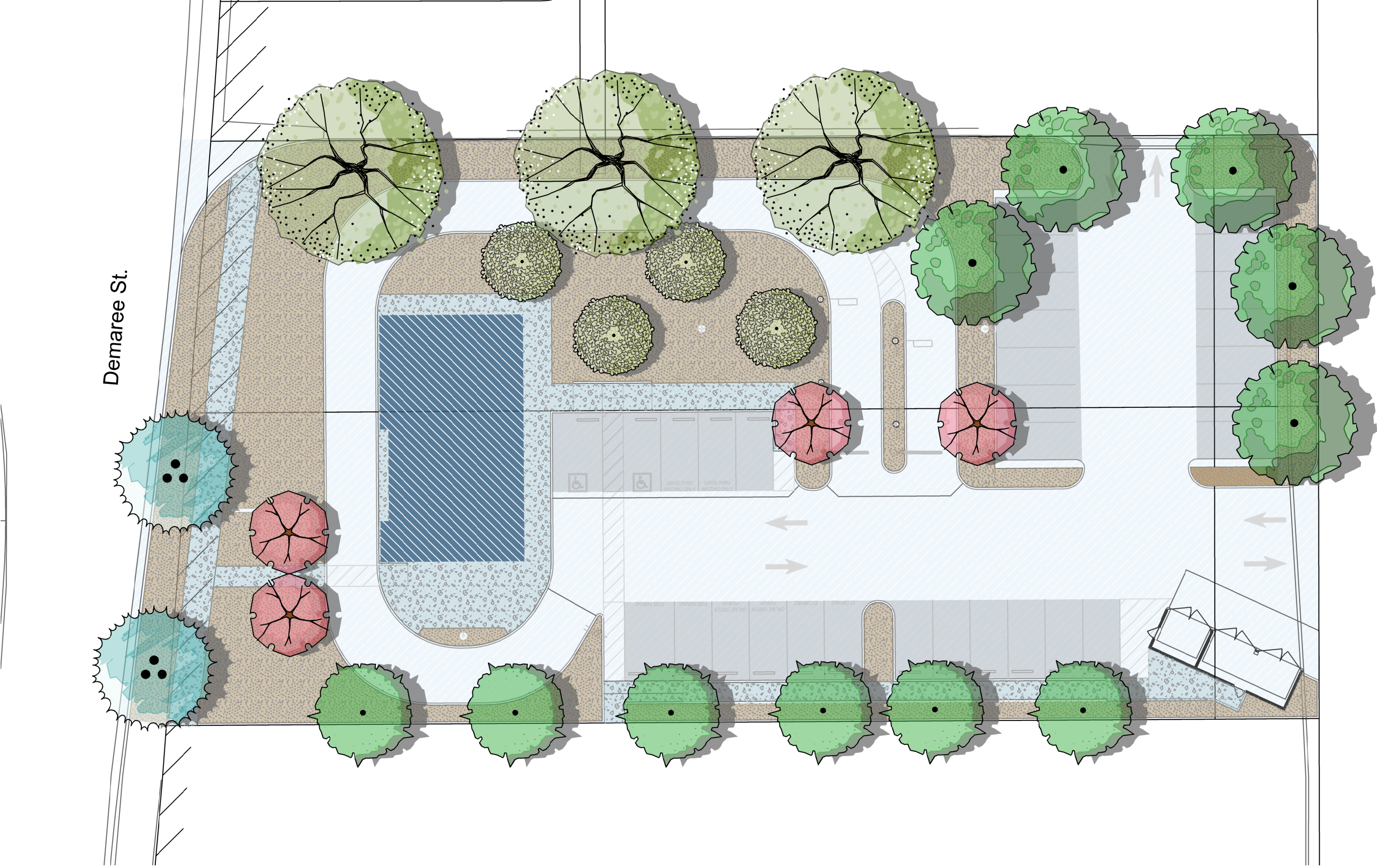
Acer buergerianum Trident Maple



Laurus x 'Saratoga' Saratoga Hybrid Laurel



Pistacia chinensis 'Keith Davey' Keith Davey Chinese Pistache



Tree Legend

SYMBOL	CODE	BOTANICAL / COMMON NAME	SIZE	TYPE	USE	WUCOLS	STYLE	CA NATIVE	CANOPY	QTY
TREES										
	AB	Acer buergerianum Trident Maple	15 gal	Deciduous	Accent Tree	Medium	Standard	No	20'	4
	CD2	Cercidium x 'Desert Museum' Thornless Palo Verde	15 gal	Deciduous	Specimen Tree	Very Low	Standard	Yes	20'	4
	LX	Laurus x 'Saratoga' Saratoga Hybrid Laurel	15 gal	Evergreen	Parking Lot Tree	Low	Standard	No	25'	6
	PK	Pistacia chinensis 'Keith Davey' Keith Davey Chinese Pistache	15 gal	Deciduous	Parking Lot Tree	Medium	Standard	No	30'	5
	PM	Platanus racemosa 'Roberts' Roberts California Sycamore	15 gal	Deciduous	Screen Tree	Medium	Standard	Yes	45'	3
	QE	Quercus englemannii Engelmann Oak	15 gal	Evergreen	Parking Lot Tree	Low	Multi	Yes	30'	2

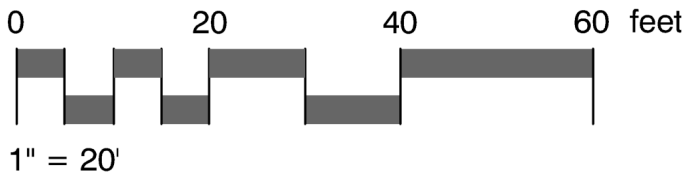
Materials Legend

	SIDEWALK	3,607 sf
	PLANTING AREA 4 - 8" washed river cobble.	12,614 sf
	SITE	39,358 sf
	PARKING LOT	5,584 sf
	BUILDING	1,995 sf

Notes:
1 - Graphic quantities take precedence over written quantities
2 - All plant material shall be of quality as prescribed in the details and specifications.
Any tree not meeting such requirements shall be removed from the site and replaced at no cost to the owner.
3 - Trees shall not be planted within:
a. 6'-0" of drive approaches
b. 6'-0" of sewer lines
c. 6'-0" of water lines
d. 10'-0" of fire hydrants
e. 20'-0" of light standards
4 - 10% of the parking lot shall be landscape per condition #8 of Site Plan Review #2025-002.
a. Total sq. footage of parking= 5,584 sq. ft.
b. 10% of the parking lot landscape= 558.4 sq. ft.
c. Total sq. footage of parking lot landscape= 2,168 sq. ft.
d. Total percentage of parking lot landscape= 38.8%
5 - All quantities and amounts shown on the plans are best estimates for the benefit of the contractor. In field conditions may vary compared to what is shown on the plans. Therefore, it is the Contractor's responsibility to verify all lengths, square footages, and amounts prior to bidding the project.

Schematic Plant Palette

Shrubs and Groundcover	
Achillea spp.	Yarrow
Agave spp.	Agave
Aloe spp.	Aloe
Arctostaphylos spp.	Manzanita
Artemisia sp. + cvs.	Coastal Sage
Baccharis sp. + cvs.	Coyote Bush
Bouteloua gracilis + cvs.	Blue Gamma Grass
Calamagrostis sp. + cvs.	Reed Grass
Callistemon 'Little John'	Dwarf Bottlebrush
Carex sp.	Sedge
Carpenteria californica	Bush Anemone
Ceanothus sp. + cvs.	Wild Lilac
Chondropetalum tectorum	Cape Rush
Cistus sp. + cvs.	Rockrose
Cotoneaster 'Coral Beauty'	Prostrate Cotoneaster
Correa x 'Ivory Bells'	Ivory Bells Australian Fuchsia
Dasylium spp.	Desert Spoon
Dianella sp. + cvs.	Blue Flax Lilly
Echium candicans	Pride of Madera
Eriogonum sp. + cvs.	Buckwheat
Hesperaloe parvifolia	Red Yucca
Heteromeles arbutifolia cvs.	Toyon
Juniperus sp. + cvs.	Juniper
Lantana x 'New Gold'	New Gold Lantana
Lavandula sp. + cvs.	Lavender
Leucophyllum sp. + cvs.	Texas Ranger
Leymus 'Canyon Prince'	Wild Rye
Mahonia repens	Creeping Mahonia
Muhlenbergia sp.	Muhly Grass
Nepeta x faassennii 'Walkers Low'	Catmint
Olea 'Lil Ole'	Dwarf Olive
Pennisetum sp. + cvs.	Feather Grass
Penstemon sp. + cvs.	Beard Tongue
Rhamnus californica cvs.	Coffeeberry
Rhamnus alaternus	Italian Buckthorn
Rhus integrifolia	Lemonadeberry
Rosemarinus sp. + cvs.	Rosemary
Salvia sp. + cvs.	Sage
Teucrium sp. + cvs.	Germander
Verbena illicina 'De La Mina'	De La Mina Verbena
Westringia fruticosa + cvs.	Coast Rosemary
Yucca sp. + cvs.	Yucca
Zauschneria sp. + cvs.	California Fuchsia



PRELIMINARY LANDSCAPE PLAN
JIB, Demaree
VISALIA, CA



REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: September 11, 2006

PROJECT PLANNER: Andrew Chamberlain, A.I.C.P., 713-4003

SUBJECT: Specific Plan Amendment No. 2006-02: A request by The Taylor Group to amend the Demaree/Caldwell Specific Plan to allow the relocation of an access point on Caldwell Avenue, and to allow a phased development of Sub-area B.

Conditional Use Permit No. 2006-32: A request by The Taylor Group to have a Pharmacy with a drive-thru lane.

The site is located on the southeast corner of Caldwell Avenue and Demaree Street (APNs: 126-030-033/034/035/036/014/015).

STAFF RECOMMENDATION *Item # 8*

Staff recommends approval of Conditional Use Permit No. 2006-29 based upon the findings and conditions in Resolution No 2006-90; and

Staff recommends approval of the phased development of Sub-area "B" but denial of the relocation of the access drive for Specific Plan Amendment No 2006-02 based on the findings in Resolution No. 2006-91.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No 2006-32 by adoption of Resolution No. 2006- 90

I move to approve Specific Plan Amendment No 2006-02, to approve the phased development of Sub-area "B", and to deny relocating the second Caldwell Avenue drive access from the east property line of the sub-area, by adoption of Resolution No. 2006-91

PROJECT DESCRIPTION

Specific Plan Amendment No. 2006-02 is a request to amend the Demaree/Caldwell Specific Plan to allow the relocation of an access point on Caldwell Avenue, and to allow a phased development of Sub-area B as shown in Exhibit "A". The Master Development Plan, as shown in Exhibit "C", contains two access drives onto Caldwell Avenue. The applicant is requesting to relocate the easternmost drive from the east property line by approximately 300 feet to the west. The second part of the specific plan request is to allow the phasing of Sub-area "B" to allow the construction of the proposed pharmacy as the first phase.

Conditional Use Permit No. 2006-32 is a request for a drive-thru window for a pharmacy as illustrated in Exhibit "A". Pharmacies are a permitted use in the underlying Community Commercial Zone and the Specific Plan, but the drive-thru component requires a Conditional Use Permit approval .

BACKGROUND INFORMATION

General Plan Land Use Designation: CCM
Zoning: C-CM (Community Comm.)
Surrounding Land Use and Zoning: North: C-CM / Commercial
South: C-CM / Commercial
East: C-CM / Commercial
West: C-CM / Commercial
Environmental Review: Negative Declaration No. 2006-67
Special Districts: Demaree/Caldwell Specific Plan, Sub-area "B"
Site Plan: Site Plan Review No. 2006-02

RELATED PLANS & POLICIES

Please refer to Chapter 3 of the Demaree/Caldwell Specific Plan regarding the development plan for Sub-area "B". This document has been placed on the City web-site and may be found at, www.ci.visalia.ca.us under Community Development/Publications.

RELATED PROJECTS

The Demaree/Caldwell Specific Plan was adopted in 1996, and encompasses approximately 66 acres on the south side of Caldwell Avenue on the east and west sides of Demaree Street. This location is one of the four Community Commercial locations in the City which are designated in the General Plan Land Use Element to serve a quadrant of the community through a Specific Plan.

PROJECT EVALUATION

Condition Use Permit No.2006-32

The proposed drive-thru is similar to those approved for the pharmacies at the southeast corner of Walnut Avenue and Ben Maddox Way, and the northwest corner of Akers Street and Cypress Avenue.

Staff raised the issue of on-site circulation at the southeast corner of the proposed building with the applicants during the Site Plan Review process. The site layout has the two main access/drive aisles converging at the southeast corner of the pharmacy building where the loading dock and drive-thru access are also located. The number of potentially conflicting vehicle maneuvers in the future, when other tenants are established on the site, is a concern to staff. The Planning Commission may determine that added conditions to redesign this portion of the site should be included in this use permit. There are several options to enhance the function of this portion of the site. They would include, but not be limited to, reversing the vehicle direction for the drive-thru, which would reduce the need for vehicles entering and exiting the drive-thru from having to cross on-coming traffic in the main drive aisle. The addition of pavement striping and landscaping to further separate the loading dock and drive-thru from the main aisles would also reduce the potential for vehicle conflicts in this area. There is sufficient land on the site to move the building north up to five feet, or the east/west access drive south by

the same amount to allow more room for the added landscaping and/or striping. The addition of a larger landscape island along the south edge of the loading dock would also significantly enhance the visual appearance of the dock area.

It should be noted that one of the issues which has caused staff to recommend denial of the requested relocation of the drive aisle on Caldwell Avenue is related to the fact that the drive-thru would substantially contribute to potential peak hour traffic conflicts on the site due to vehicles exiting the drive-thru and having to cross oncoming traffic is that is entering the site from Caldwell Avenue.

The applicant has included proposed building elevations in Exhibit "D", along with tentative elevations of the other shops and buildings in the sub-area in Exhibit "E". Staff has included a requirement for consistency with the elevations in Exhibit "D" in Condition No. 2. The shops and other building elevations are not conditioned by this action since they are on separate properties. Their future development is required to be compatible with the first approved building in the sub-area, as specified in Policy B-5 of the Specific Plan.

Specific Plan Amendment No 2006 -02

Phased Development of Sub-Area "B"

Staff supports the phased development of Sub-area "B". The site plan provided in Exhibit "A" demonstrates that there are opportunities to develop portions of the site independently based upon the common access drives which would serve any of the phases.

The applicant intends to install the cross-hatched area as Phase One for the proposed pharmacy. The Phase One improvements include all four access drives, with approximately 30 feet of paving width for the drive aisles to the south and east. Staff supports providing full vehicle access on the site with Phase One. The southerly extension of the proposed main north/south access drive all the way to the southern property line of the sub-area will create a large pocket of un-improved frontage along Demaree Street.

As a part of Phase One, staff has included a condition requiring a seven foot high block wall along a portion of the southern property line of the Sub-area, adjacent to the existing single family residence. The wall will provide view and noise protection to the house, and should extend approximately 25 feet east of the house. The remaining 460 feet to the west (approximate) will be addressed at the time of a development proposal for the "Major Retail A", the interim wall is being proposed as needed.

Relocation of Caldwell Avenue Access Drive

Staff is recommending denial of the relocation of the Caldwell Avenue access drive from the east property line to the proposed location to provide direct access to the pharmacy. As the access drive exists now in the Specific Plan, it is a secondary access drive along the eastern property line which would typically be for service and delivery vehicles.

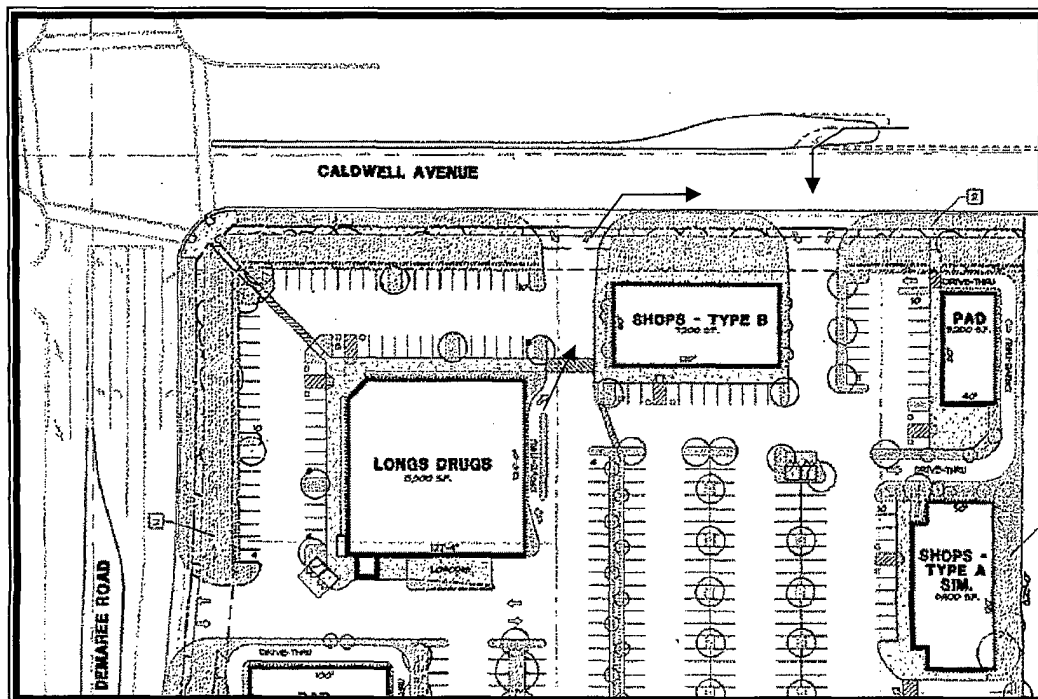
The applicant has provided an Access Analysis, Exhibit "G", which indicates that the benefits of greater safety and convenience will result, along with increased speeds of traffic on Caldwell Avenue. The Analysis concludes that the relocation of the second access drive from a typical service vehicle access to a major access point, thereby creating two major access points on Caldwell Avenue, will give drivers quicker access and reduce frustration. The analysis also recommends that a "decel lane" be added to afford additional protection. The addition of a "decel lane" would typically require that the building be moved back further to accommodate the loss of landscaping along the frontage.

The recommendation to deny the relocation of the access drive is based on the issues listed below:

1. Potential conflicting turning maneuvers from vehicles exiting the site east-bound on Caldwell Avenue while west-bound vehicles are trying to cross the east-bound lanes at the median break 150 feet away.
2. The drive-thru would substantially contribute to potential peak hour traffic conflicts on/off the site due to vehicles exiting the drive-thru having to cross oncoming traffic is that is entering the site from Caldwell Avenue, which would typically be most problematic at the peak hours. The access analysis does not cover the potential of on-site conflicts from the proposed drive-thru.
3. The proposed two access drives have approximately 80 feet of separation between the radius returns which does not meet City Standards for 500 feet between the access points. It should be noted that the existing Specific Plan does not have the required separation for the approved access points on Caldwell Avenue in Sub-area "B".
4. Two close access points gives bicycles and pedestrians two areas of conflict with vehicles utilizing the site.

The City Public Works Department has reviewed the applicant's Access Analysis and has also noted that the access point should not be relocated based upon the lack of separation between the two Caldwell Avenue access points. Engineering recognizes that the existing plan does not meet the separation standards, and that the proposed relocation of the access drive does meet the minimum 200 foot distance from the arterial intersection.

The City Engineer has included a condition in the Use Permit that the medians in Caldwell and Demaree shall be installed with Phase One to control conflicting turning maneuvers entering/exiting the site. Staff has included Condition No. 7 requiring the medians and striping as a part of the building permit for the pharmacy/Phase One. At the southern access point on Demaree, there may not be enough existing right-of-way to accommodate the median and striping, wherein the Public Works Department would work with the applicant to create an interim solution.



Landscaping

Landscaping plans will be submitted with the project's building permit package for review by the Planning Division. Staff is further recommending a condition of approval that requires placing a permanent landscape screen or mounding that achieves a height of three-foot to visually screens parking stalls and drive-thru lanes from any adjacent public street. The applicant has provided Caldwell and Demaree cross sections, Exhibit "F", which show a three-foot high wall or evergreen hedge to screen the parking areas adjacent to the street. The provision for a three-foot high wall, mound or evergreen hedge is one of the landscaping standards in the Specific Plan.

Environmental Review

An Initial Study was prepared for this project, consistent with the California Environmental Quality Act (CEQA). The Initial Study disclosed that environmental impacts are determined to be not significant. Therefore, staff recommends that Negative Declaration No. 2006-67 be adopted for this project.

RECOMMENDED FINDINGS

Conditional Use Permit No. 2006-32

1. That the proposed drive-thru is similar to other pharmacy drive-thru approvals.
2. That the proposed drive-thru will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
3. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located.
 - The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity.
3. That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
4. That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

Specific Plan Amendment No 2006 -02

1. That the phased development of the sub-area can be done in an orderly fashion as shown in Exhibit "A".
2. That the proposed Specific Plan Amendments, as amended by staff, are consistent with the purpose and intent of the Demaree Caldwell Specific Plan, Zoning Ordinance , and the Land Use Element.
3. That the proposed relocation of the access drive from the eastern property line to the proposed location is not supported based upon the following:

- Potential conflicting turning maneuvers from vehicles exiting the site east-bound on Caldwell Avenue while west-bound vehicles are trying to cross the east-bound lanes at the median break 150 feet away.
 - The drive-thru would substantially contribute to potential peak hour traffic conflicts on/off the site due to vehicles exiting the drive-thru having to cross oncoming traffic is that is entering the site from Caldwell Avenue, which would typically be most problematic at the peak hours. The access analysis does not cover the potential of on-site conflicts from the proposed drive-thru.
 - The proposed two access drives have approximately 80 feet of separation between the radius returns which does not meet City Standards for 500 feet between the access points. It should be noted that the existing Specific Plan does not have the required separation for the approved access points on Caldwell Avenue in Sub-area "B".
 - Two close access points gives bicycles and pedestrians two areas of conflict with vehicles utilizing the site.
4. That an Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant, and Negative Declaration No. 2006-67 is hereby adopted.
 5. That there is no evidence before the Planning Commission that the proposed project will have any potential for adverse effects on wildlife resources, as defined in Section 711.2 of the Department of Fish and Game Code.

RECOMMENDED CONDITIONS OF APPROVAL

Conditional Use Permit No. 2006-32

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2006-002.
2. That the site be developed in substantial compliance with the site plan shown in Exhibits "A" and "B", and the building elevations in Exhibit "D", attached herein, and with modifications to match the Sub-area access points in the Specific Plan,.
3. That landscaping and irrigation plans be submitted for review by Planning Division staff with building permits.
4. That an evergreen hedge, mound, or wall approximately 3 foot-high be installed along drive-thru lanes and parking lots which are directly adjacent to Caldwell Avenue or Demaree Street, consistent with the Specific Plan requirements.
5. That a seven foot high block wall be constructed along the southern property line of Sub-area "B" from approximately 25 feet behind the right-of-way line on Demaree to approximately 25 feet past the east end of the house on the adjacent property. An additional wood fence, minimum height six foot, will be required to define the property line and reduce headlight glare across the balance of the property to the south where a fence of this description may not already be in place.
6. That this conditional use permit shall not be deemed approved if Specific Plan Amendment No. 2006-02 is not approved by the City Council.
7. That the Caldwell and Demaree medians and striping shall be installed by the developer as a part of the pharmacy/Phase One building permit (such medians to control/eliminate left turns

into the subject site). This condition shall be met prior to the first building occupancy in Sub-area "B".

8. That all other city codes and ordinances be met.
9. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2006-32, prior to the issuance of any building permits for this project.

Specific Plan Amendment No 2006-02

1. That the phasing of the proposed project be substantially consistent with Exhibit "A".
2. That the adopted vehicle circulation and access plan (figure 9, Caldwell /Demaree Specific Plan) be met.
3. That all other requirements of the Caldwell /Demaree Specific Plan be met.

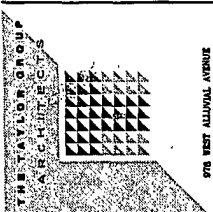
Attachments:

- Exhibit "A" – Site Plan
- Exhibit "B" – Site Plan - Pharmacy
- Exhibit "C" – Sub-area "B" site plan from adopted Specific Plan
- Exhibit "D" – Pharmacy Elevations
- Exhibit "E" – Tentative Elevations for Shops and Other Buildings
- Exhibit "F" – Cross Sections for Caldwell Avenue and Demaree Street
- Exhibit "G" – Access Analysis
- General Plan Land Use Map
- Zoning Map
- Aerial Photo
- Location Map

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.38.120, an appeal to the City Council may be submitted within five working days following the date of a decision by the Planning Commission on a conditional use permit application. An appeal shall be in writing and shall be filed with the City Clerk at 707 W. Acequia Ave., Visalia, CA 93291. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record.

The Specific Plan Amendment goes on to the City Council for final action, any appeal of the Conditional Use Permit would typically be heard at the same time. If the conditional use permit is approved and not appealed, the City Council would not review the use permit, and only the specific plan amendment would be reviewed.



THE TAYLOR GROUP
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PESQUERO, CA 94051
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FAX 650.437.0000

PROPOSED MASTER SITE PLAN FOR **LONGS DRUG STORES** CALDWELL AVE. & DEMAREE ROAD

SPR1
SHEET 01
DATE: 5/17/08
REVISIONS:
BY: JTB
DATE: 5/17/08

KEYNOTES:

1. LANDSCAPING AREA
2. DASHED LINE INDICATES BUFFER OVERLAP
3. 6" H&V CONCRETE CURB TYP. SEE CIVIL DRAWINGS
4. PAINTED WHITE DIRECTIONAL ARROWS TYP. SEE DETAIL 1X
5. 4" H&V PAINTED WHITE PAVEMENT STRIPE TYP. SEE DETAIL 1X
6. H&V STRIPE, PAINTED ON PAVEMENT, TYP. SEE DETAIL 1X
7. H&V PAVEMENT SIGN TYP. SEE DETAIL 1X
8. BUILDING ENTRANCE
9. DASHED LINE INDICATES RED PAINTED CURB WITH WHITE LETTERS SIGNING, NO PARKING - TYP.
10. PAINTED WHITE STRIPES TYP. SEE DETAIL 1X
11. CONC. DRIVE APPROACH PER CITY OF VESALIA PUBLIC WORKS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH
12. SITE H&V TOWARD SIGN TYP. SEE DETAIL 1X
13. CONC. CURB AND GUTTER
14. DISABLED ACCESS RAMP (D) MAX. SLOPED
15. INDICATES PROPERTY OR PARCEL LINE TYP.
16. (R) CONC. ASPHALT
17. C&G TRAILER ENCLOSURE (TYPE 2) 22'-4" x 8'-0" INTERIOR CLEAR SEE DETAILS 2A, 2B PER CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH
18. (R) CONC. ASPHALT
19. (R) CONC. ASPHALT
20. DASHED LINE INDICATES SECTION BOUNDARIES
21. RED STOP WITHIN 80 FT. OF INTERSECTION
22. SIGHT TRIANGLE SIGN FOR ENTER COMPLEX
23. 8 FT. HIGH CONC. H&V OR H&V TO GREEN
24. 8 FT. HIGH CONC. H&V FENCE, HEIGHT TO BE DETERMINED PER CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH
25. STOP SIGN
26. INDICATES PROPOSED LEFT TURN POCKET
27. CONC. H&V TRUSS COVER

GENERAL NOTES:

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH AND SHALL BE VISIBLE FROM THE STREET. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH.
2. A POOL BOX KEY LOCK SYSTEM IS REQUIRED.
3. PROVIDE SHARED PARKING/ACCESS AGREEMENTS. THE AGREEMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH.
4. ALL ROOF HIGHTS SHALL BE IN ACCORDANCE WITH THE CITY OF VESALIA STANDARD SPECIFICATIONS FOR CONSTRUCTION OF APPROACH.
5. ALL LIGHTING TO BE INSTALLED AND INSTALLED TO PREVENT ANY SIGNIFICANT DIRECT OR INDIRECT LIGHT INTO ADJACENT RESIDENTIAL PROPERTY.
6. ALL SIGNS REQUIRE A SEPARATE PERMIT.
7. ALL ON-SITE STORM WATER TO BE DIRECTED TO CITY STORM DRAINAGE SYSTEM.
8. VACANT PLOTS TO BE TURNED INTO HYDRO-SEEDING AND AGRICULTURAL SPRINKLERS.

SITE DATA:

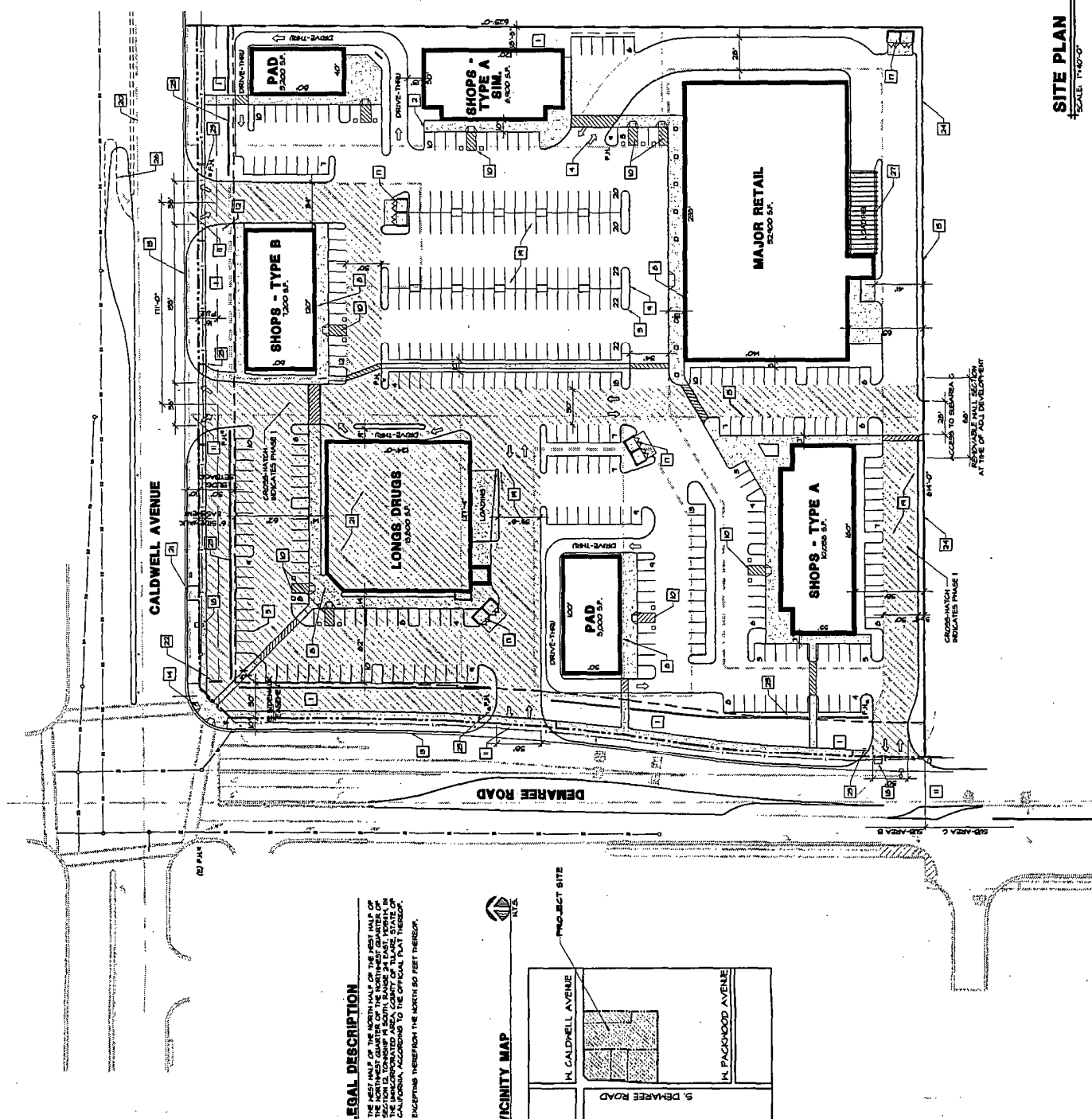
LAND AREA	972,497 SQ. FT. (22.3 ACRES)
BUILDING AREA	21,027 SQ. FT.
COVERAGE	2.16%
APN	028-000-000-000-000-000

PROPOSED PARKING:

STANDARD PARKING SPACES	• 247 SPACES
4" x 8" THROUGH CUT	• 11 SPACES
4" x 8" THROUGH CUT	• 11 SPACES
PARKING SPACES (6 PER 225 SQ. FT.)	• 247 SPACES
TOTAL PARKING PROVIDED	• 247 SPACES
PARKING RATIO	• 4.3 SPACES/1,000 SQ. FT.



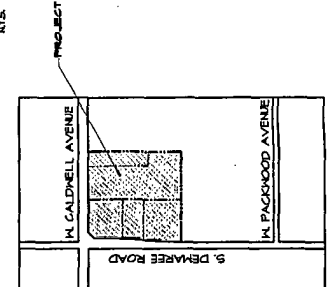
SITE PLAN
SCALE: 1"=40'-0"

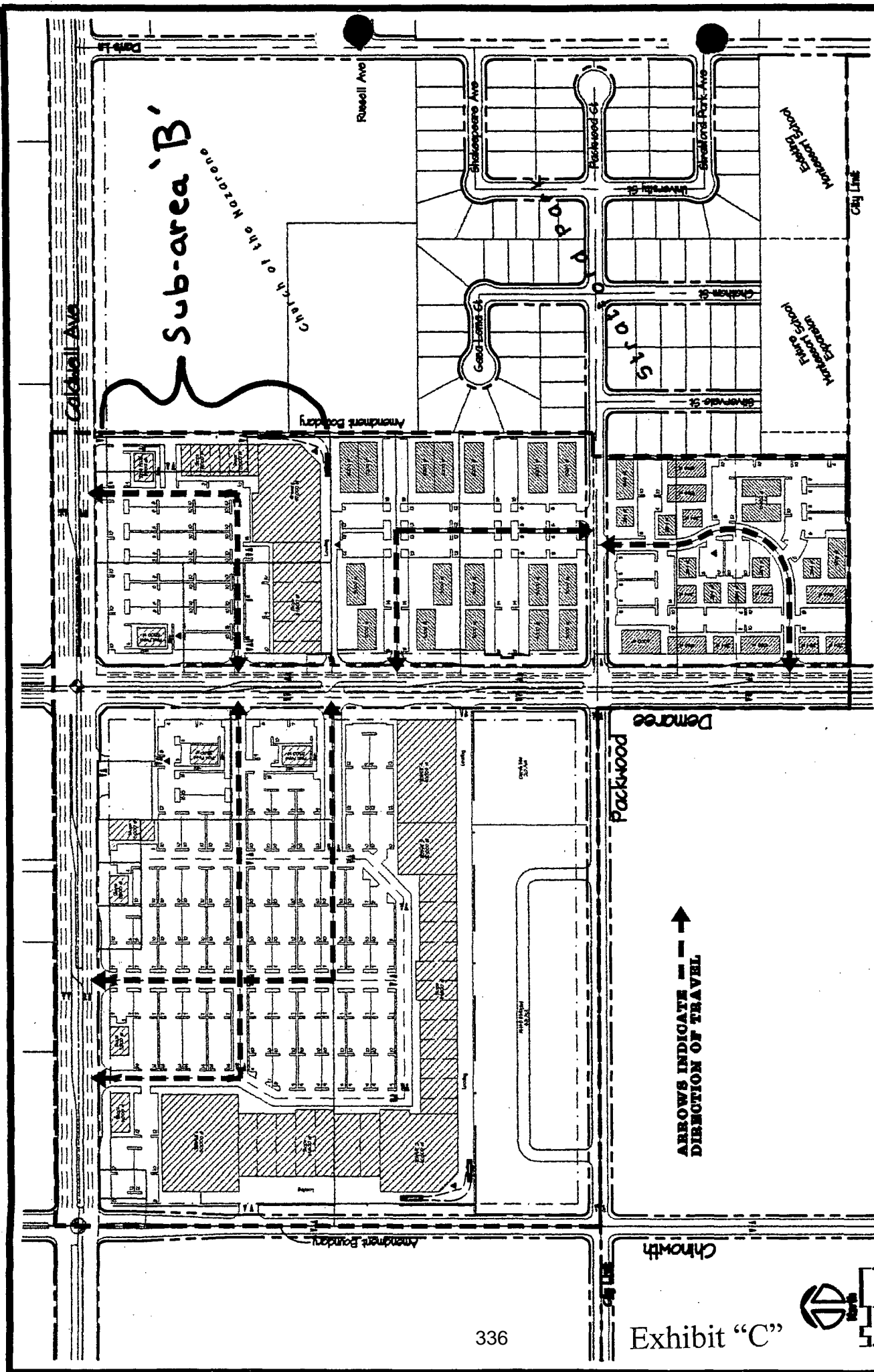


LEGAL DESCRIPTION

THE PROJECT SITE IS LOCATED IN THE NORTHWEST QUARTER OF THE SECTION 12, TOWNSHIP 4N, RANGE 24E, EAST 1/4, IN THE CITY OF VESALIA, CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, EXCEPTING THEREFROM THE NORTH 80 FEET THEREOF.

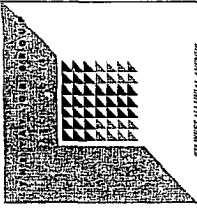
VICINITY MAP



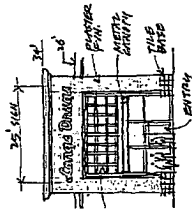


CALDWELL / DEMAREE SPECIFIC PLAN
ON-SITE CIRCULATION PLAN

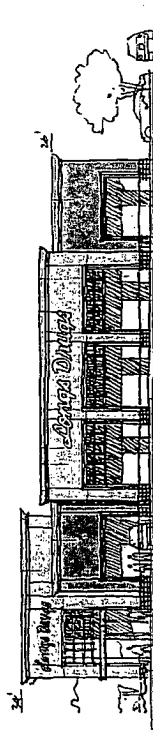
Figure 9



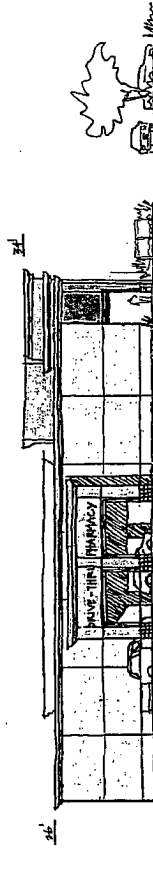
478 WEST ALLAMPA AVE
SUITE 107
FURNACE, CA 95111
TEL: 510-477-5900
FAX: 510-477-5905



TOWER HEAD-ON ELEVATION



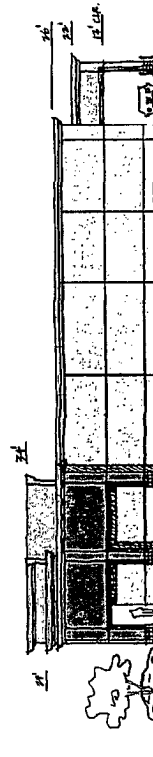
WEST ELEVATION



EAST ELEVATION



NORTH ELEVATION

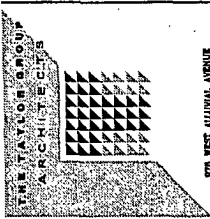


SOUTH ELEVATION

THESE DRAWINGS ARE THE PROPERTY OF THE ARCHITECT AND ARE NOT TO BE REPRODUCED OR COPIED IN ANY MANNER WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT.

PROJECT NO.	DATE
DESCRIPTION	BY
REVISIONS	DATE
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OF SHEETS



978 WEST ALLIANCE AVENUE
SUITE 107
FRESNO, CA 93711
TEL. 559.437.0000
FAX. 559.437.0005

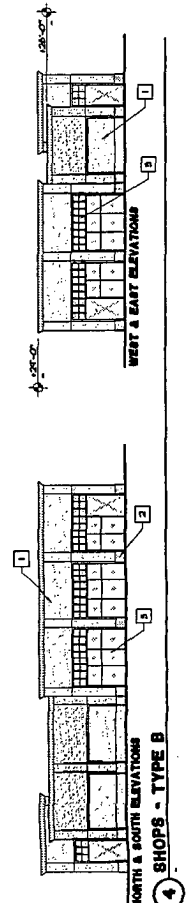
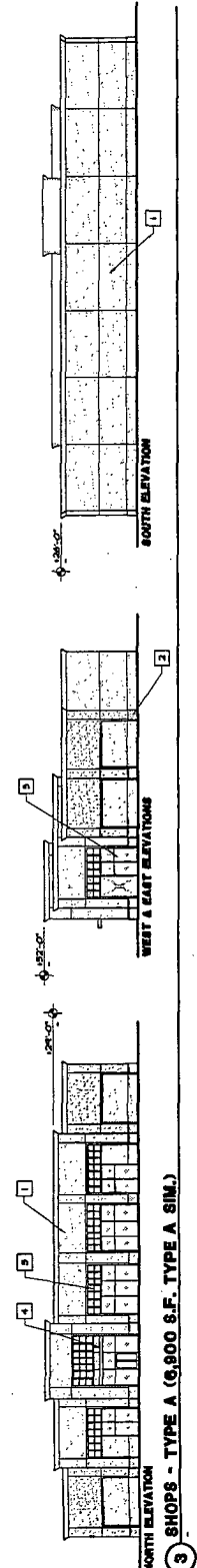
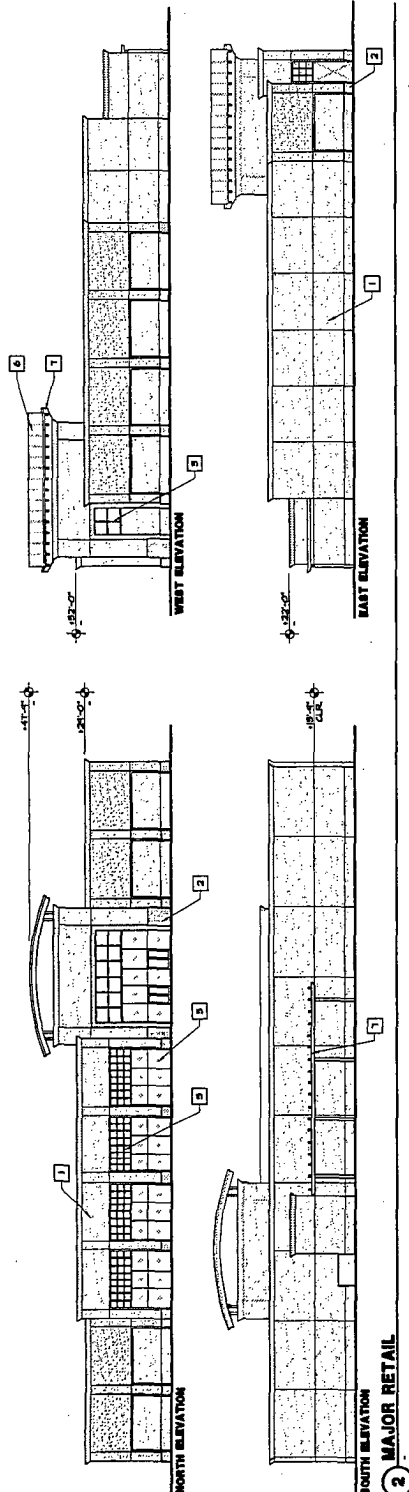
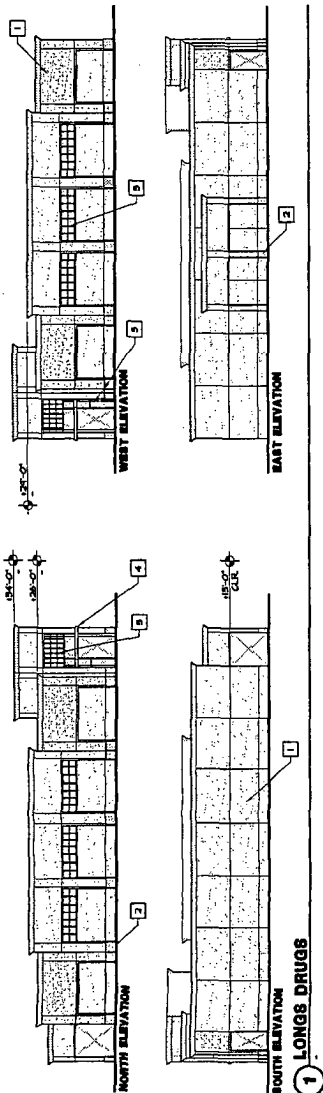
PROPOSED MASTER SITE PLAN FOR **LONGS DRUG STORES** CALDWELL AVE. & DEMAREE ROAD

1.0. The following information is required for the project: a. The project is a proposed master site plan for the Longs Drug Stores. b. The project is located at the intersection of Caldwell Ave. & Demaree Road. c. The project is a proposed master site plan for the Longs Drug Stores. d. The project is a proposed master site plan for the Longs Drug Stores.	
DRAWN BY: 5/1/2006 PROJECT NO: 061006 SHEET NO: SPR3 OF: 3	DATE: 5/1/2006 PROJECT NO: 061006 SHEET NO: SPR3 OF: 3

- KEYNOTES:**
1. EXTERIOR LIGHT FIXTURES SEE COLORED ELEVATIONS & 1/2" x 11" PAGE OF SIGNIFICANT FOR THE COLOR SCHEME.
 2. TILE BASE
 3. DECORATIVE METAL GRILL
 4. METAL CANOPY
 5. STOREFRONT SYSTEM
 6. METAL STANDING SEAM ROOFING
 7. METAL TRELLIS / SUPPORT STRUCTURE



ELEVATIONS
SCALE: 1"=30'-0"

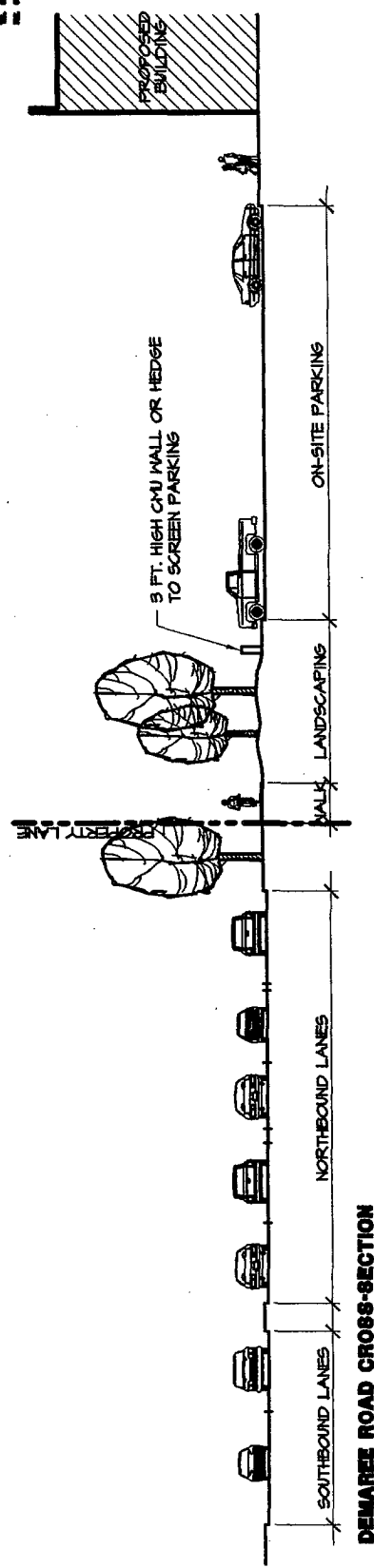




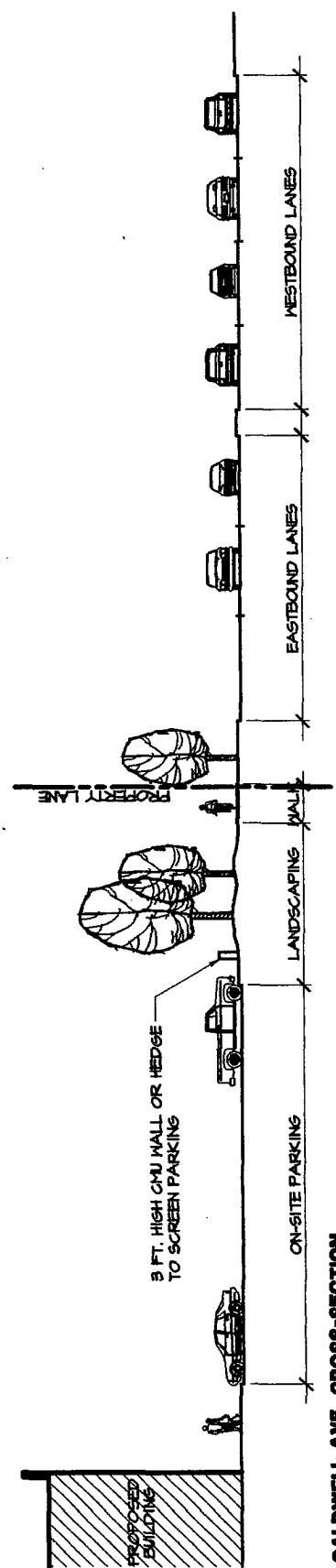
1" = 20' - 0"

1" = 40' - 0"

1" = 80' - 0"



DEMAREE ROAD CROSS-SECTION



CALDWELL AVE. CROSS-SECTION



PROPOSED STREET CROSS SECTION FOR
LONGS DRUG STORES
CALDWELL AVE. & DEMAREE ROAD



August 10, 2006

Mr. Joe Cavanagh
Longs Drug Stores Inc.
141 N. Civic Drive
Walnut Creek, CA 94596

Re: Caldwell/Demaree Shopping Center Access Analysis – 2nd Driveway along Caldwell Avenue

Dear Mr. Cavanagh:

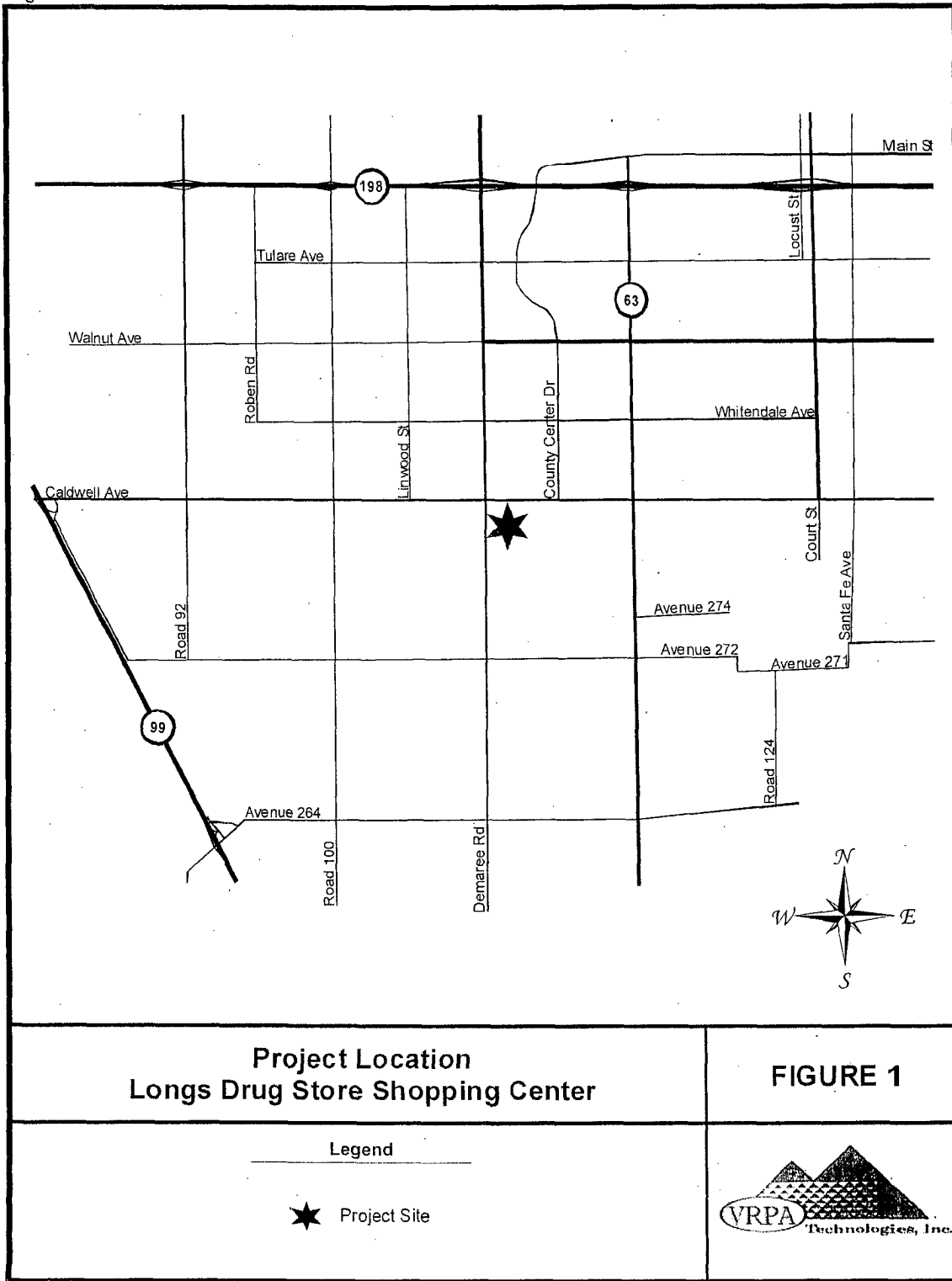
As requested, VRPA Technologies, Inc. has prepared an analysis regarding a second driveway along Caldwell Avenue in Visalia to the proposed shopping center located at the southeast corner of Caldwell and Demaree (reference Figure 1). The City of Visalia Specific Plan for this property includes a westbound left turn pocket from Caldwell to the easternmost driveway along Caldwell which is also taken into consideration in this study. VRPA has analyzed the benefits of the 2nd access point or driveway (reference Figure 2).

STEP 1

The first step applied to analyze the proposed access point or driveway spacing, was to generate peak hour trips for the shopping center. To accomplish this step, VRPA applied the total square footage of the proposed shopping center (divided by 1,000) to the shopping center trip generation rate in (Use 820) in the Institute of Transportation Engineer's (ITE) Trip Generation Manual. Table 1 shows the results of this first step.

STEP 2

The second step was to determine existing and future traffic volumes along Caldwell and Demaree and at the intersection of Caldwell and Demaree. Existing AM and PM Peak Hour volume counts were conducted by VRPA in May 2006 (reference Figure 3). Trips generated by the proposed shopping center were then added to the existing AM and PM Peak Hour traffic counts as depicted in Figures 4 and 5. Figures 4 and 5 provide an assessment of how the proposed shopping center trips were distributed to the four proposed access points or driveways along Demaree and Caldwell. Future year (2030) volumes were derived using the Tulare County Association of Governments' Regional Traffic Model. The Traffic Model indicates a 3.5% increase in traffic in the Study Area between 2006 and 2030. VRPA then increased the existing traffic volumes by 3.5% to derive the Future Year turning movement volumes without the proposed shopping center at the Demaree and Caldwell intersection and the segment volumes along Demaree and Caldwell (reference Figure 6). Future AM and PM Peak Hour traffic with the proposed shopping center trips were then developed as shown in Figures 7 and 8.



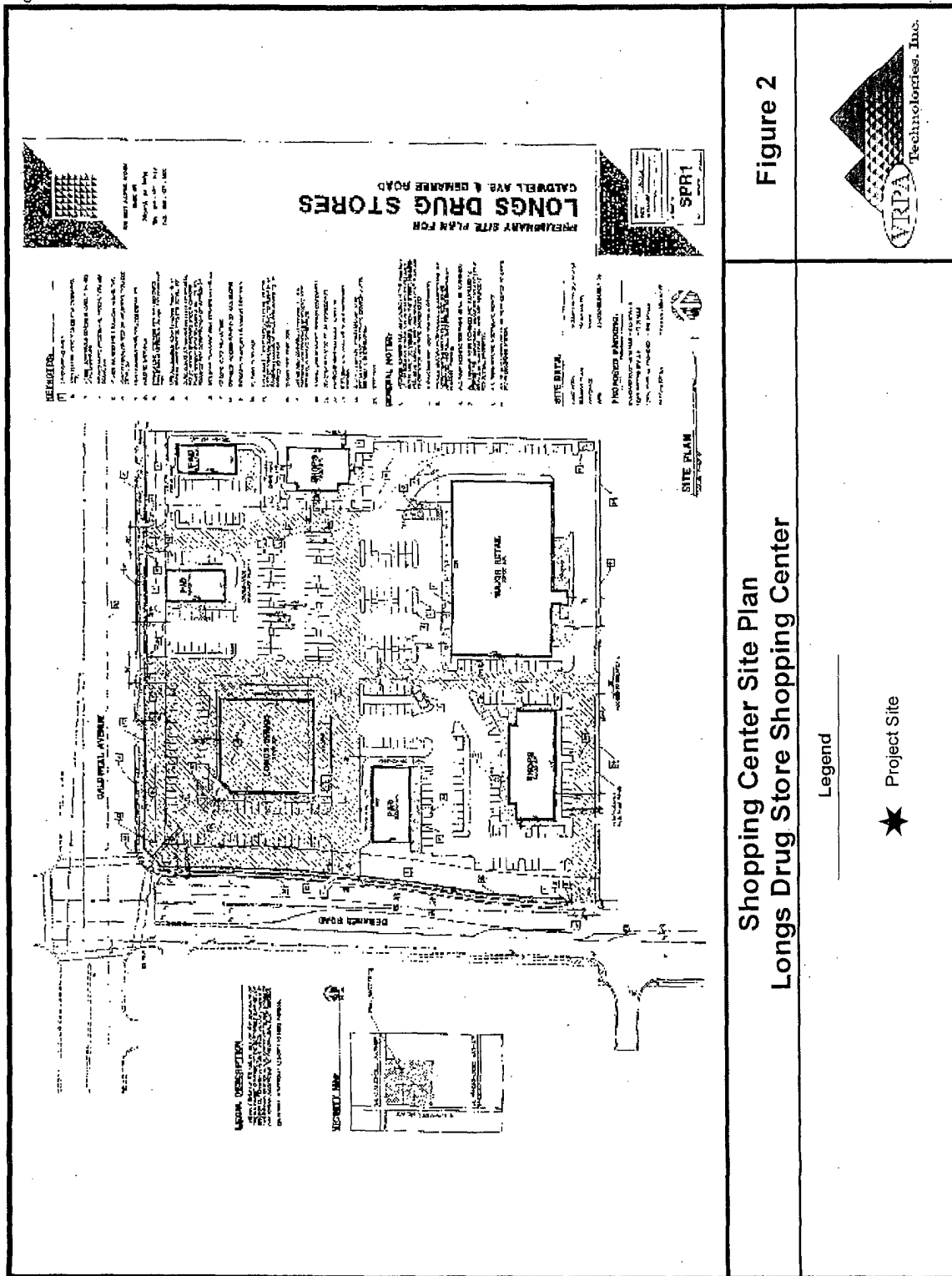
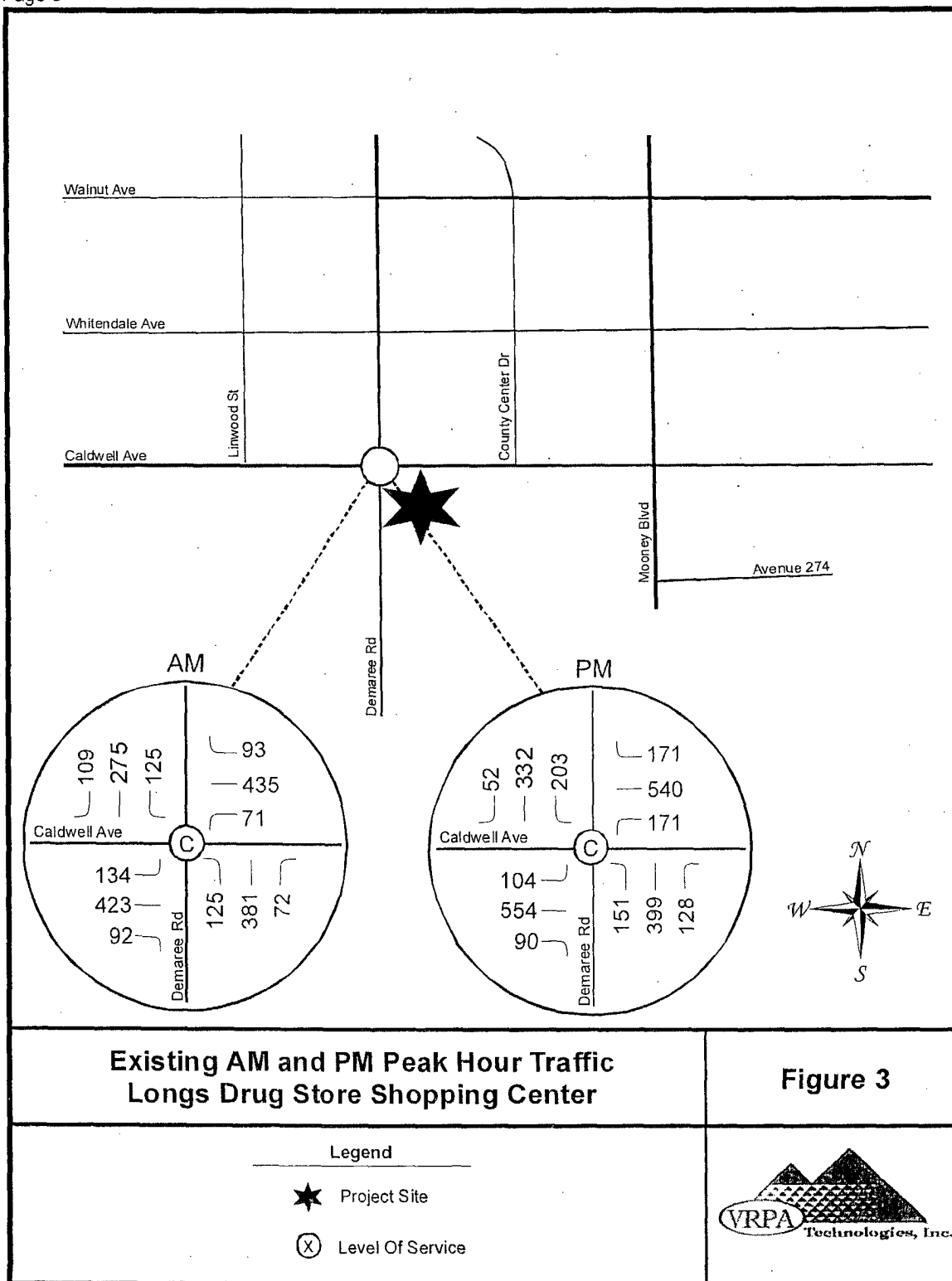
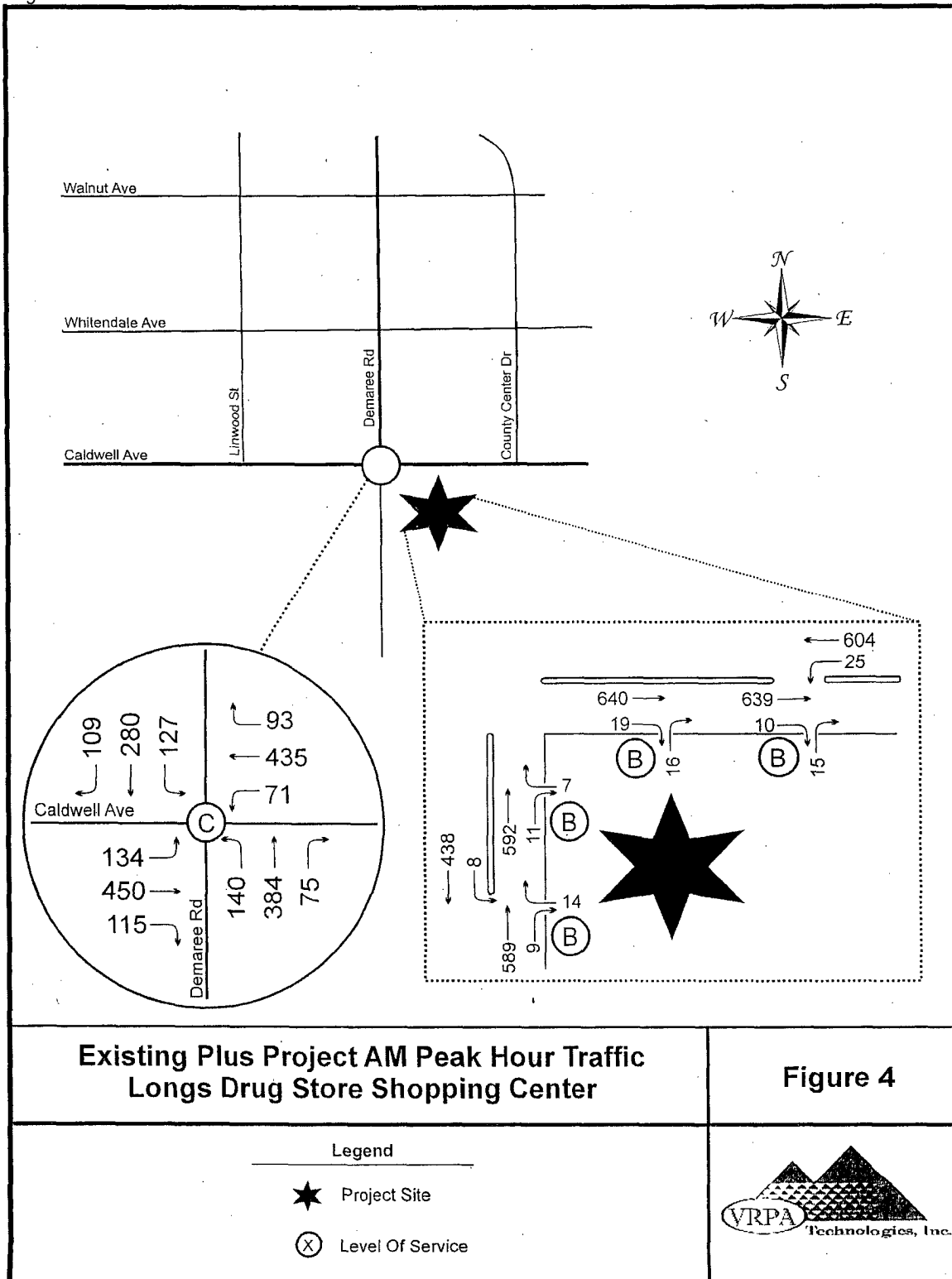


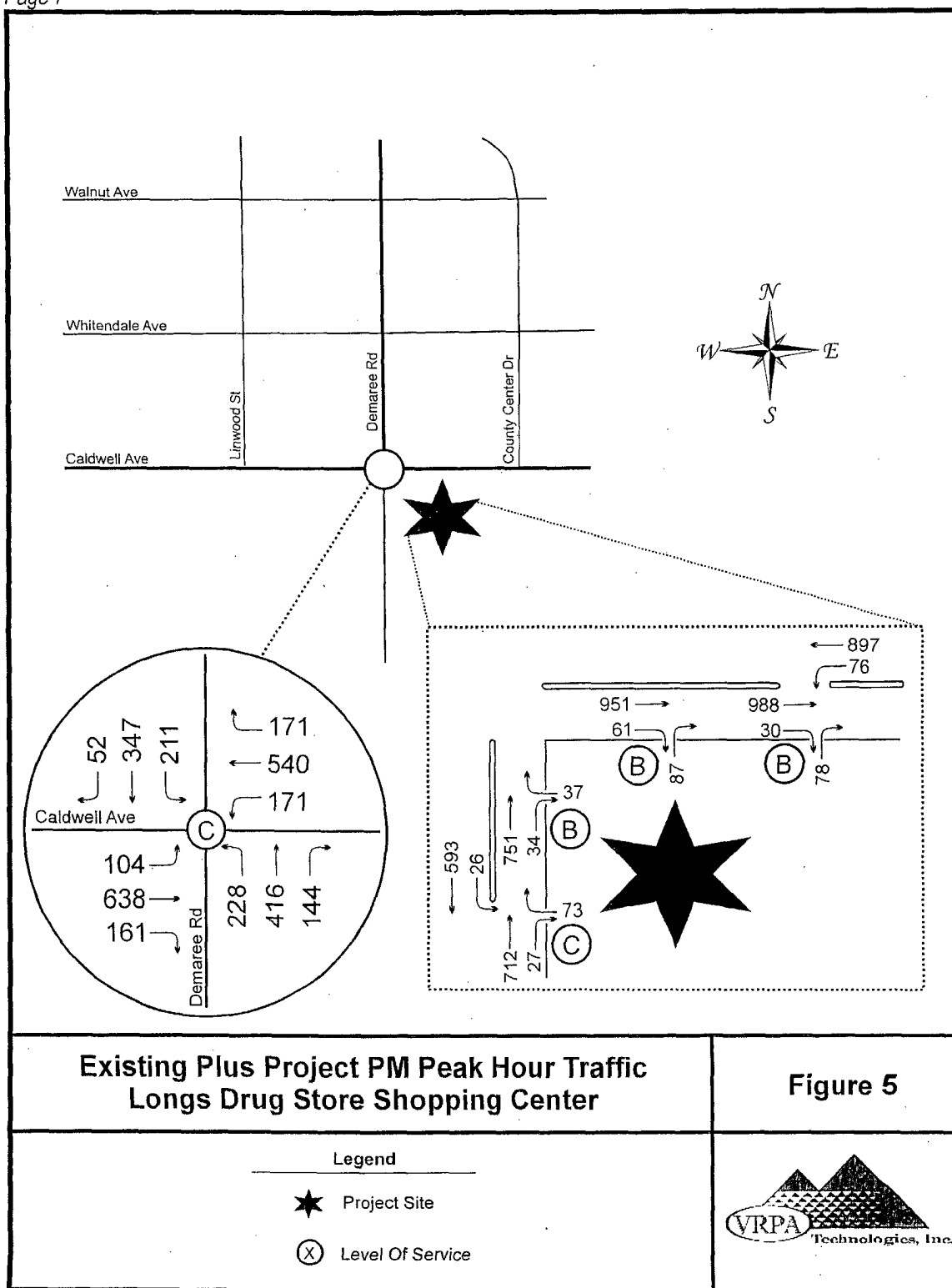
Table 1
PROJECT TRAFFIC GENERATION
Longs Drug Store Shopping Center

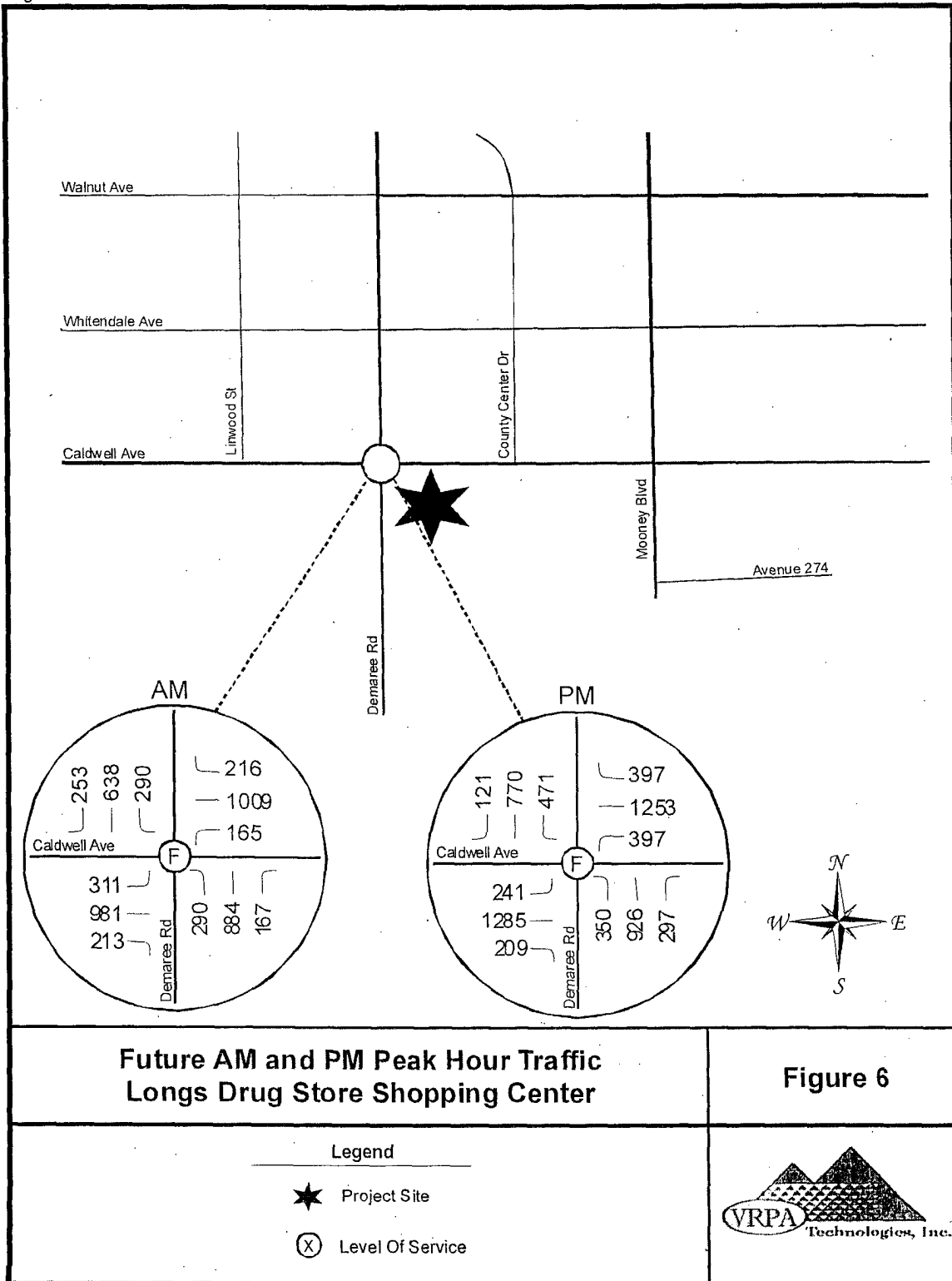
USE	SIZE	DAILY TRIP ENDS		AM PEAK HOUR				PM PEAK HOUR			
		RATE	VOLUME	RATE	IN:OUT SPLIT	VOLUME		RATE	IN:OUT SPLIT	VOLUME	
						IN	OUT			IN	OUT
Shopping Center (820)	77,400	74.28	5,749	1.73	61:39	82	52	6.83	48:52	254	275
TOTAL PROJECT TRIPS			5,749			82	52			254	275

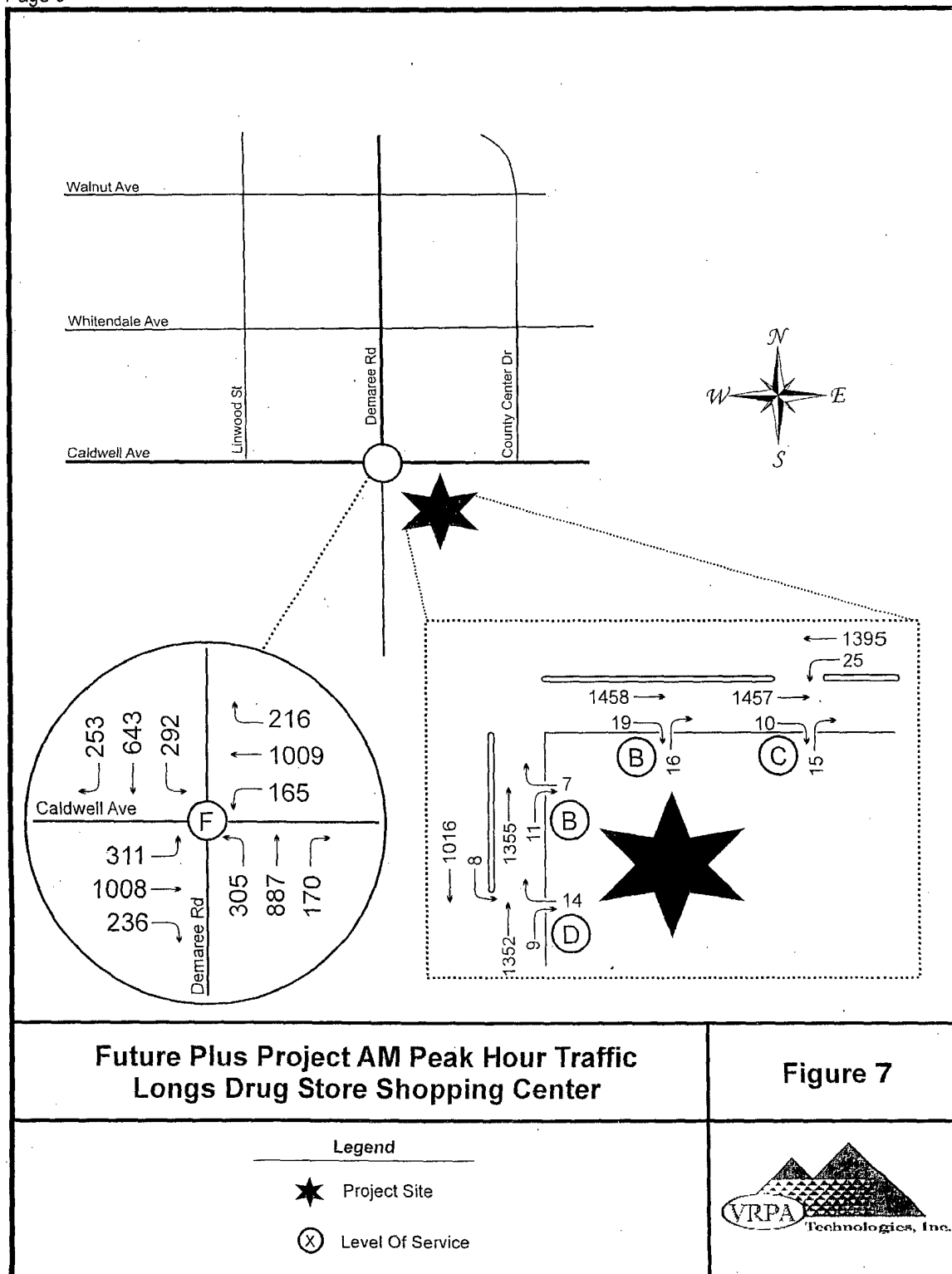
Source: Generation factors from ITE Trip Generation Manual, 7th Edition.
Trip ends are one-way traffic movements, entering or leaving.
The numbers in parenthesis are ITE land use codes.

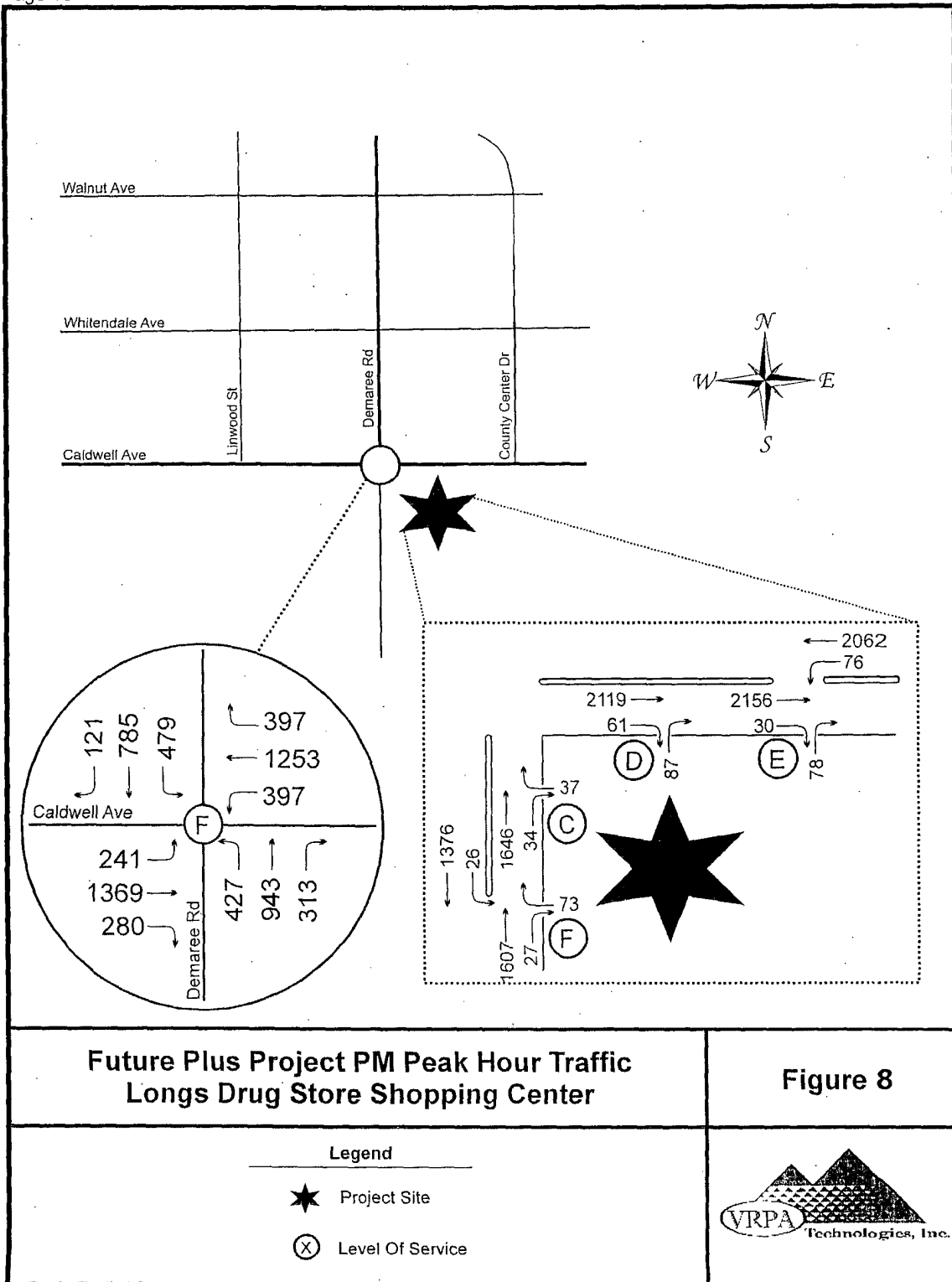












STEP 3

The third step in this process was to analyze the level of service associated with the each of the proposed access points and the intersection of Demaree and Caldwell. The Highway Capacity Manual (HCM) software (HCS) was applied to determine LOS for each of the scenarios discussed in Step 2 (Existing, Existing plus the proposed shopping center, and Future Year 2030 without and with the proposed shopping center. The results of this step are shown in Table 2 and in each of the figures referenced in Step 2 (Figures 3 through 8). Referencing Table 2, the intersection at Demaree and Caldwell is projected to function at LOS F during the AM and PM without further improvement. The minimum LOS standard for the City of Visalia is LOS D. According to the TCAG Traffic Model, by the Year 2030, only 2 lanes in each direction are expected to be in place along Demaree and Caldwell.

Table 2
INTERSECTION OPERATIONS WITH 2 ACCESS POINTS ON CALDWELL
Longs Drug Store Shopping Center

INTERSECTION	PEAK HOUR	EXISTING		EXISTING PLUS PROJECT		FUTURE NO PROJECT		FUTURE PLUS PROJECT	
		DELAY	LOS	DELAY	LOS	DELAY	LOS	DELAY	LOS
Caldwell Ave / Demaree Rd ⁽¹⁾	AM	29.5	C	29.8	C	>80.0	F	>80.0	F
	PM	30.8	C	32.8	C	>80.0	F	>80.0	F
Southern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	D
	PM			N/A	C			N/A	F
Northern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	C
Eastern Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	C
	PM			N/A	B			N/A	E
Western Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	D

DELAY is measured in seconds.

LOS = Level of Service

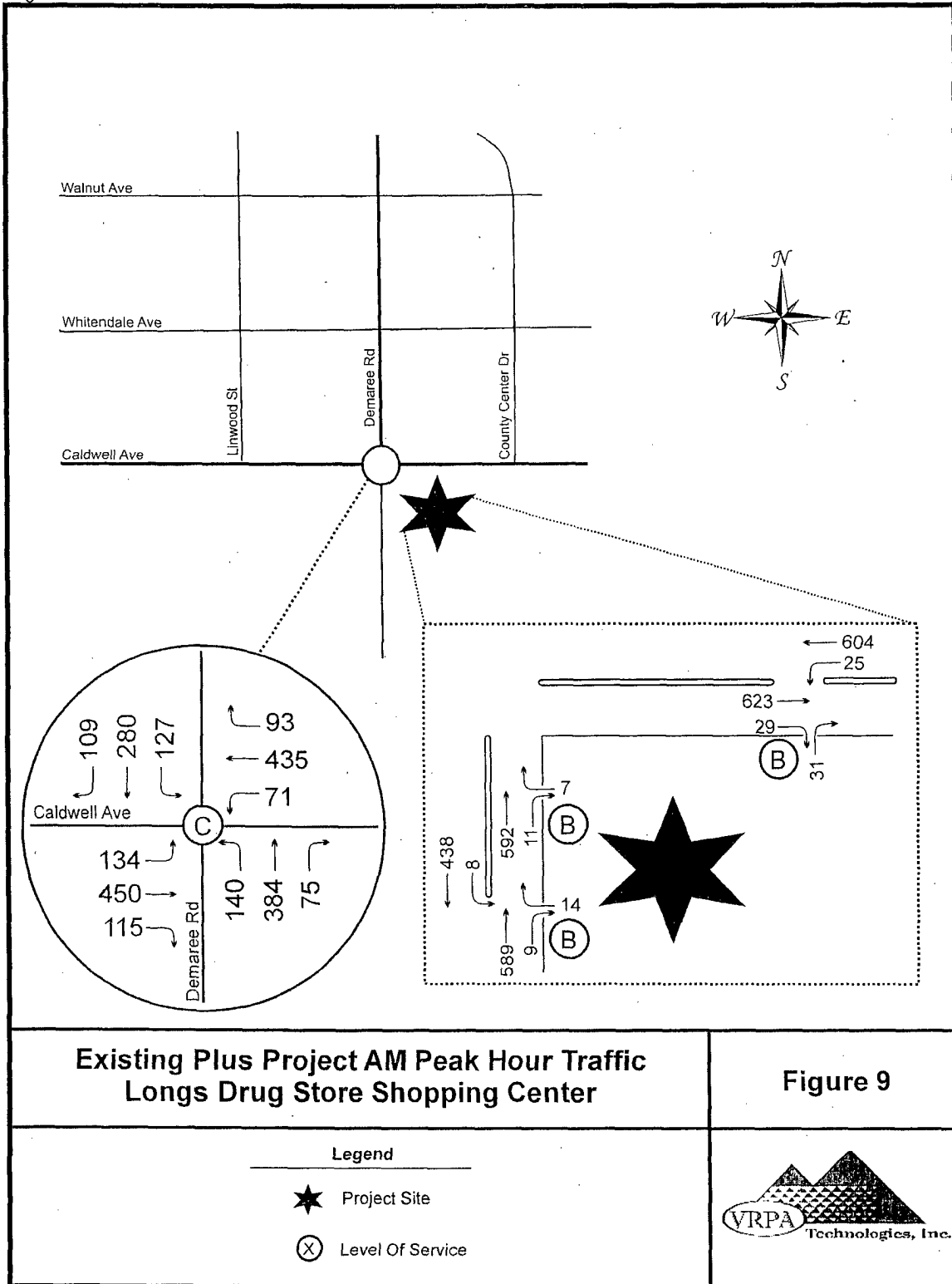
(1) Signalized intersection

(2) One-way stop controlled intersection

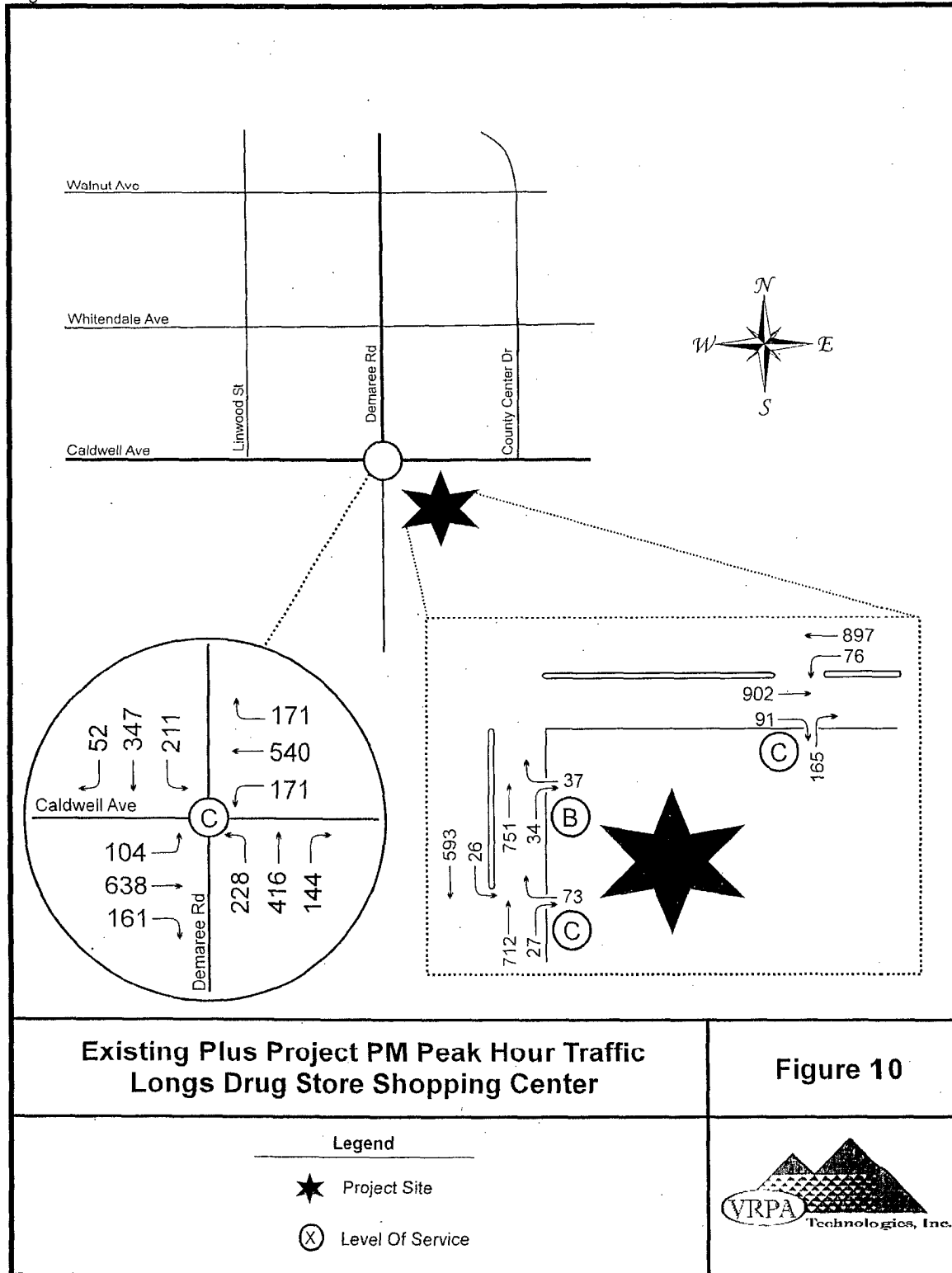
Referencing Table 2, the LOS at each of the proposed access points under Existing plus the proposed shopping center and Future Year 2030 without and with the proposed shopping center will operate at acceptable levels with the exception of the southernmost driveway along Demaree and easternmost driveway along Caldwell. These intersections will operate at LOS F and E respectively in the PM Peak Hour. As a result of the westbound left turn pocket along Caldwell, the "U" turns and left turns at the Caldwell and Demaree intersection, as well as the access points along Demaree to the project will be reduced. The volumes at these intersections or access points could very well increase in only one (1) access point along Caldwell is provided since entering vehicles would be concentrated at one (1) driveway thereby increasing delay and congestion.

An analysis of just one access point along Caldwell with a westbound left turn pocket from Caldwell was analyzed to compare the benefits of two (2) access points versus one (1) access point. Steps 1 – 3 above were also completed for the analysis of one (1) access point along Caldwell and results of this analysis are shown in Figures 9-12 and in Table 3.

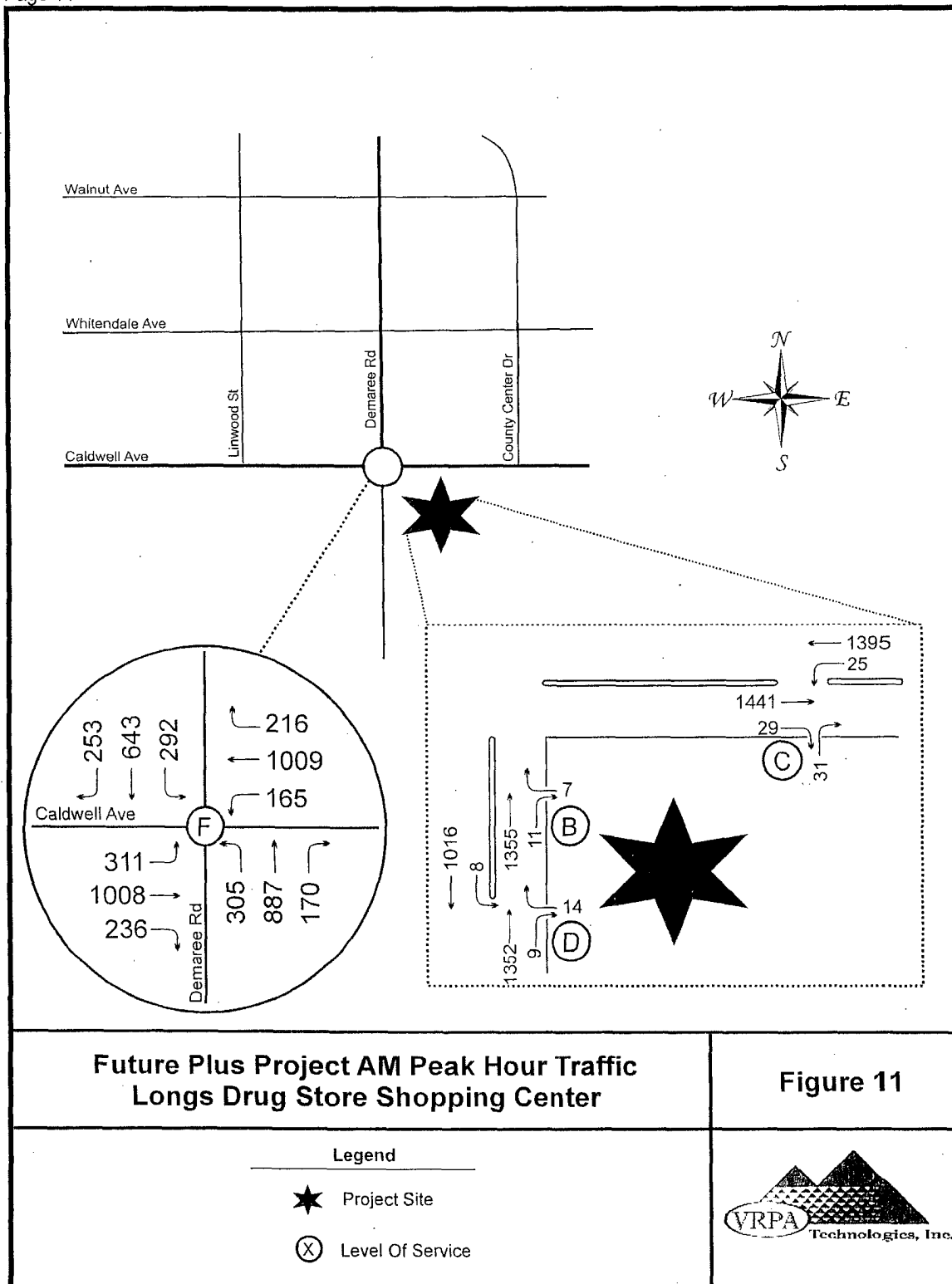
Mr. Chris Finn



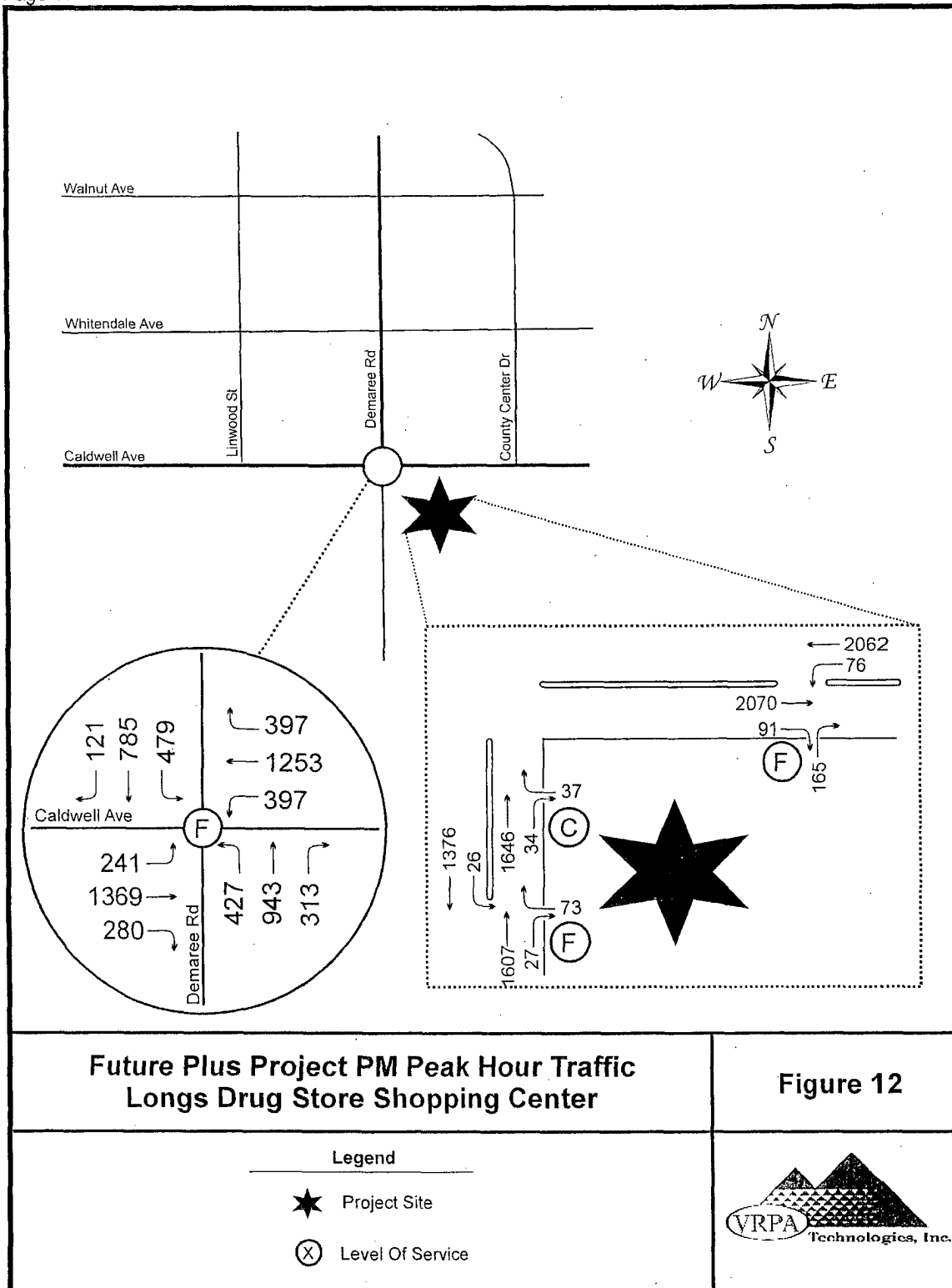
Mr. Chris Finn



Mr. Chris Finn



Mr. Chris Finn



Mr. Chris Finn

Table 3
INTERSECTION OPERATIONS WITH 1 ACCESS POINT ON CALDWELL
Longs Drug Store Shopping Center

INTERSECTION	PEAK HOUR	EXISTING		EXISTING PLUS PROJECT		FUTURE NO PROJECT		FUTURE PLUS PROJECT	
		DELAY	LOS	DELAY	LOS	DELAY	LOS	DELAY	LOS
Caldwell Ave / Demaree Rd ⁽¹⁾	AM	29.5	C	29.8	C	>80.0	F	>80.0	F
	PM	30.8	C	32.8	C	>80.0	F	>80.0	F
Southern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	D
	PM			N/A	C			N/A	F
Northern Driveway / Demaree Rd ⁽²⁾	AM			N/A	B			N/A	B
	PM			N/A	B			N/A	C
Driveway / Caldwell Ave ⁽²⁾	AM			N/A	B			N/A	C
	PM			N/A	C			N/A	F

DELAY is measured in seconds.

LOS = Level of Service

(1) Signalized intersection

(2) One-way stop controlled intersection

Referencing Table 3, the LOS at each of the proposed access points under Existing plus the proposed shopping center and Future Year 2030 without and with the proposed shopping center will operate at acceptable levels with the exception of the southernmost driveway along Demaree and the driveway along Caldwell. These intersections will operate at LOS F in the PM Peak Hour.

BENEFITS OF RELOCATED ACCESS/DRIVEWAY

The provision of two (2) closer spaced driveways will result in the following benefits:

- ◆ Greater safety
- ◆ Greater convenience
- ◆ Increase in speed along Caldwell Avenue

Specifics regarding these benefits are as follows:

- ◆ Provision of two (2) driveways or access points as depicted in Figure 13 along Caldwell Avenue will improve safety to bicyclists and pedestrians by providing more opportunity for motoring patrons to exit and enter the proposed development. With only one (1) driveway or access point for that portion of the development, patrons desiring to turn into the development will only have one opportunity to make a right-turn.
- ◆ With only one (1) driveway or access point to the shopping area, the queues on-site will grow long during the PM Peak Hour; drivers will get frustrated, and as a result, will begin to take chances when merging into eastbound Caldwell Avenue traffic. Two (2) closer spaced access points will reduce the resulting delay and frustration.

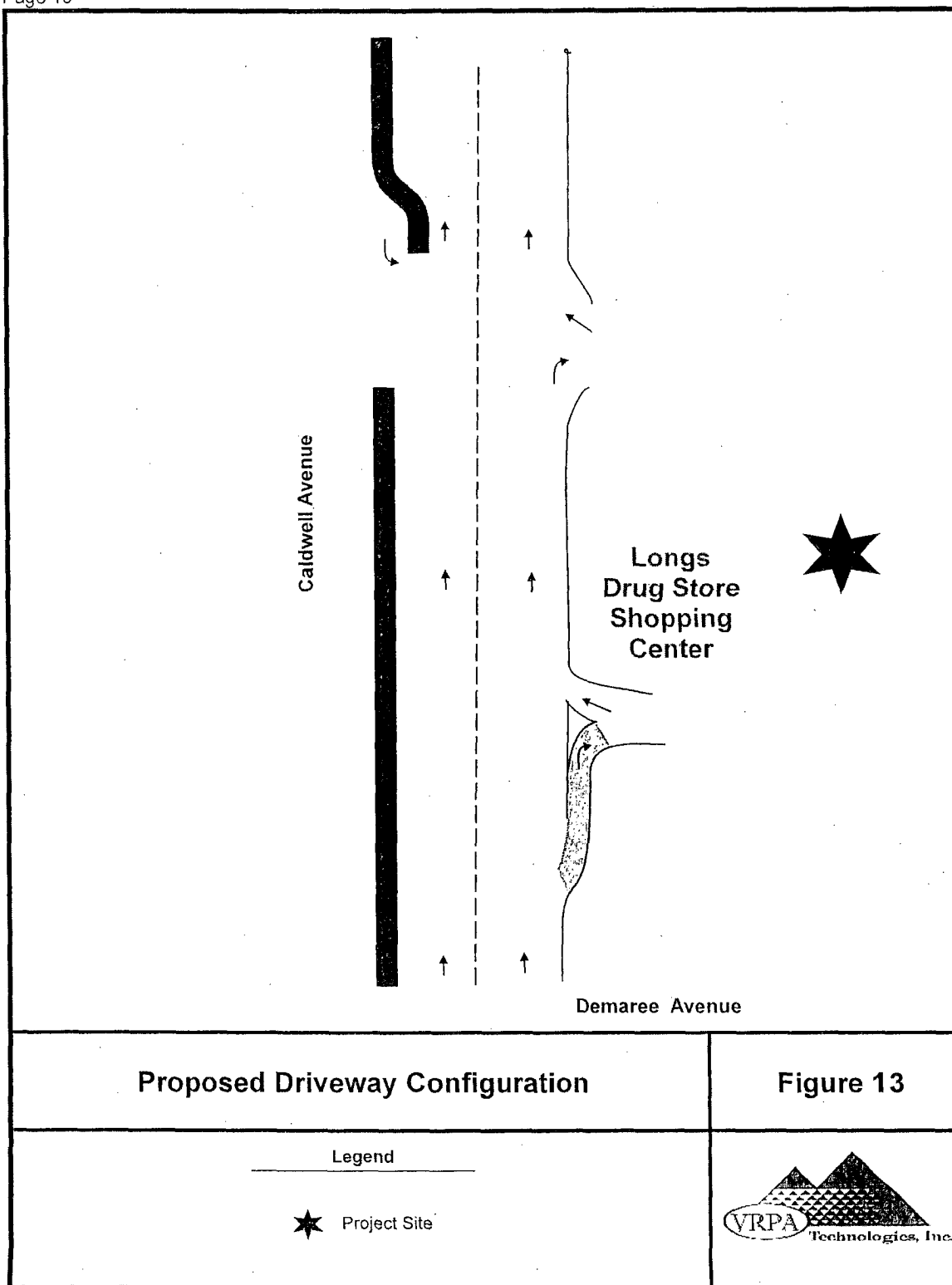
Mr. Chris Finn
August 10, 2006
Page 17

- ◆ With two (2) driveways or access points providing access to the shopping areas, patrons turning right into the development will have two opportunities to access the development, lessening the potential for "quick" or perceived "last opportunity" decisions, which may cause rear-end collisions and detrimentally affect passing pedestrians and bicyclists. The two (2) driveways along Caldwell will reduce the accidents related to vehicles turning right into the site because the vehicles will have two (2) opportunities to enter vs. one (1) opportunity.
- ◆ Patrons exiting the project along Caldwell Avenue will have two (2) opportunities to exit resulting in shorter on-site queuing during the PM Peak Hour and less frustration when trying to turn right and merge into traffic along eastbound Caldwell Avenue.
- ◆ The provision of one (1) vs. two (2) closely spaced access points does not negatively affect the resulting levels of service or seconds of delay experienced by motorists along Caldwell Avenue. In fact, provision of two (2) closer spaced access points or driveways along Caldwell Avenue would improve travel speed in the project area given the availability of a 2nd point of convenient access to the proposed development.

CONCLUSIONS

Based upon the results of Steps 1 through 3, the following conclusions can be drawn:

- ◆ The proposed driveway configuration along Caldwell Avenue depicted in Figure 13 will operate at LOS D or better on opening day or under Future Year conditions regardless of where the driveways are located along Caldwell Avenue with the exception of the easternmost driveway in the Future Year condition with project for the PM peak hour.
- ◆ To ensure that safety is addressed along Caldwell adjacent to the proposed shopping center, a minimum spacing of 100' should be provided between the proposed access points or driveways along Caldwell.
- ◆ To ensure additional safety and to reduce the potential for conflicts between vehicles traveling east and turning right into and out of the proposed access points or driveways along Caldwell, it is recommended that the westernmost driveway be configured as a "right turn deceleration lane with a raised island" prohibiting a right turning vehicle from changing its mind and deciding to turn right into the easternmost access point or driveway along Caldwell (reference Figure 13).

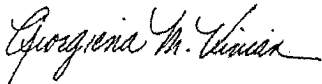


Mr. Joe Cavanagh
August 10, 2006
Page 19

Should you have any questions regarding our analysis, please feel free to contact me at (559) 259-9257 or by email at gvivian@vrpatechnologies.com.

Sincerely,

VRPA TECHNOLOGIES, INC.

A handwritten signature in cursive script, reading "Georgiena M. Vivian".

Ms. Georgiena M. Vivian,
Vice President

GV/ldb
Attachments

cc: Mr. Erik Ruehr, P.E., Dir. Of Traffic Engineering,
VRPA Technologies, Inc.



January 21, 2025

Site Plan Review No. 2025:002

Pursuant to Zoning Ordinance Chapter 17.28 the Site Plan Review process has found that your application complies with the general plan, municipal code, policies, and improvement standards of the city. A copy of each Departments/Divisions comments that were discussed with you at the Site Plan Review meeting are attached to this document.

Based upon Zoning Ordinance Section 17.28.070, this is your Site Plan Review determination. However, your project requires a CUP and Building Permit which is stated on the attached Site Plan Review comments. You may now proceed with filing your permit to the Planning Division.

This is your Site Plan Review Permit; your Site Plan Review became effective **January 8, 2025**. A site plan review permit shall lapse and become null and void one year following the date of approval unless, prior to the expiration of one year, a building permit is issued by the building official, and construction is commenced and diligently pursued toward completion.

If you have any questions regarding this action, please call the Community Development Department at (559) 713-4359.

Respectfully,

A handwritten signature in blue ink, appearing to read "Paul Bernal".

Paul Bernal
Community Development Director
315 E. Acequia Ave.
Visalia, CA 93291

Attachment(s):

- Site Plan Review Comments

City of Visalia

315 E. Acequia Ave., Visalia, CA 93291



Planning Division

Tel: (559) 713-4359; Fax: (559) 713-4814

MEETING DATE January 8, 2025
SITE PLAN NO. 2025-002
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with

☐ Planning

☐ Engineering prior to resubmittal plans for Site Plan Review.

☐ Solid Waste

☐ Parks and Recreation

☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☐ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 7:30 a.m. and 5:00 p.m., Monday through Thursday, offices closed on Fridays.

☒ Your plans must be reviewed by:

☐ CITY COUNCIL

☐ REDEVELOPMENT

☒ PLANNING COMMISSION

☐ PARK/RECREATION

☒ Conditional Use Permit

☐ HISTORIC PRESERVATION

☐ OTHER:

☐ ADDITIONAL COMMENTS:

If you have any questions or comments, please call the Site Plan Review Hotline at (559) 713-4440
Site Plan Review Committee

SITE PLAN REVIEW COMMENTS

Colleen A. Moreno, Planning Division, (559) 713-4031; colleen.moreno@visalia.city

Date: January 8, 2025

SITE PLAN NO: 2025-002
PROJECT TITLE: Ryan Dudley
DESCRIPTION: Resubmittal 2024-252 – Site Plan for a “Jack in the Box” restaurant at the SE corner of Caldwell Ave. and Demaree St.
APPLICANT: Ryan Dudley
OWNER: MFT Demaree LLC
LOCATION: Southeast corner of Caldwell & Demaree – south of CVS
APN: 121-580-008
ZONING: C-MU (MIXED USE COMMERCIAL)
GENERAL PLAN: COMMERICAL MIXED USE

Planning Division Recommendation:

- ☒ Revise and Proceed
☐ Resubmit

Reference SPR No.:
2024-252, 2024-090,
2024-090-1, &
2024-090-1-1.

Project Requirements

- Compliance with the Demaree-Caldwell Specific Plan
- Conditional Use Permit (CUP)
- Queuing Analysis
- Noise Study
- Photometric Plan
- Building Permit

PROJECT SPECIFIC INFORMATION: January 8, 2025

1. All previous comments apply, see SPR 2024-252, 2024-090, 2024-090-1, 2024-090-1-1.
2. A Conditional Use Permit shall be required. The previous amendment to the Demaree-Caldwell Specific Plan (CUP 2006-32) depicted a drive-thru in the site plan, however, it was never analyzed in relation to the other drive-thrus onsite.
3. A detailed operational statement shall be provided.
4. A Lot Line Adjustment has been submitted, a lot exhibit detailing the proposed LLA request shall also be provided with the CUP application.
5. A detailed floor plan shall be provided with CUP submittal.
6. A detailed site plan shall be provided with CUP submittal. Plans shall note the location of all compact stalls, ensuring that no more than 30% of the stalls are for compact use, and that no more than four compact stalls are clustered in any one area.
7. A minimum five-foot landscape setback shall be provided along the southern property boundary.
8. A landscaping plan shall be provided with CUP submittal. Plans shall verify that a minimum 10% of the parking lot is landscaped.
9. The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three (3) feet. Screening devices shall be a combination of berming, hedge, and landscape materials, and solid walls as approved by the City Planner. If utilizing solid walls, submit detailed elevations depicting the screening.
10. A photometric plan shall be provided with CUP submittal. Plans shall demonstrate that no more than 0.5 foot candles shall be produced at property lines.
11. A Queuing analysis shall be required with CUP submittal.
12. Given the proximity of sensitive land uses near the project site, a Noise Study shall be required with the CUP submittal.
13. Comply with the Demaree-Caldwell Specific Plan.

14. Frontage Improvements along S. Demaree St are required.
15. Per the Demaree-Caldwell Specific plan, the applicant shall maintain a 30 ft. wide setback along South Demaree Street, as measured from the property line shall be depicted on the site plan.
16. A shared parking and access agreement shall be required. An exhibit showing all properties involved (including existing/proposed uses) in the shared use agreement shall also be provided.
17. Comply with the requirements of the Solid Waste Division.
18. Obtain a Building Permit.
19. All signage shall require a separate building permit submittal.
20. Comply with the requirements of the Solid Waste, Traffic Engineering, and Engineering Divisions.

NOTES

1. The applicant shall contact the San Joaquin Valley Air Pollution Control District to verify whether additional permits are required through the District.
2. Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.

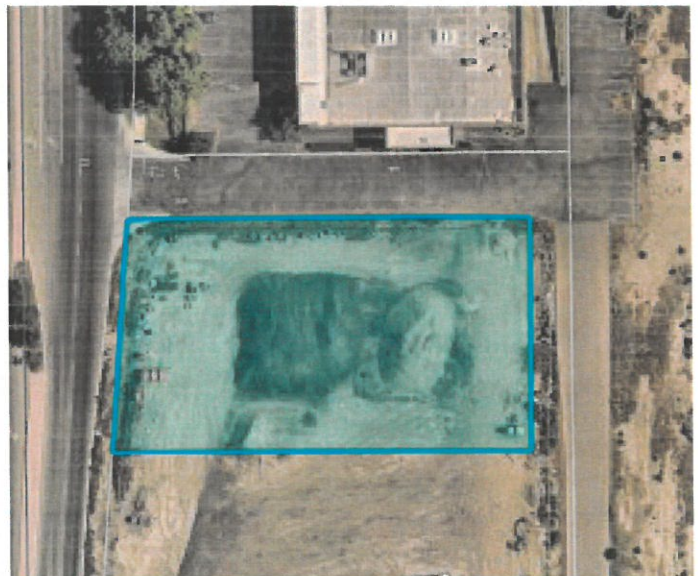
Applicable Sections of the Visalia Municipal Code to review:

- 17.19. Mixed Use Zones
- 17.30 Development Standards
- 17.32.32.162 Drive Thru Performance Standards
- 17.34 Off-street parking and loading facilities

Accessible at <https://codelibrary.amlegal.com/codes/visalia/latest/overview>

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

Signature:



**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

<input type="checkbox"/>	Edelma Gonzalez	713-4364
<input checked="" type="checkbox"/>	Luqman Ragabi	713-4362
<input type="checkbox"/>	Sarah MacLennan	713-4271
<input type="checkbox"/>	Jesus Carreno	713-4268

SITE PLAN REVIEW COMMENTS

☒ REQUIREMENTS (indicated by checked boxes)

☐ Install curb return with ramp, with _____ radius;

☒ Install curb; ☒ gutter

☐ Drive approach size: ☐ Use radius return;

☒ Sidewalk: 7' width; ☒ 10' parkway width at **DEMAREE, SEE ADDITIONAL COMMENTS**

☒ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.

☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.

☒ Right-of-way dedication required. A title report is required for verification of ownership. **DEMAREE**

☒ Deed required prior to issuing building permit; **A TOTAL OF 10' FROM BACK OF CURB TO BE DEDICATED AS RIGHT OF WAY**

☒ City Encroachment Permit Required. **FOR ANY WORK NEEDED WITHIN PUBLIC RIGHT-OF-WAY**

Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.

☐ CalTrans Encroachment Permit required. ☐ CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;

☐ Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.

☐ Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.

☒ Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. ☒ Prepared by registered civil engineer or project architect. ☒ All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) ☒ directed to the City's existing storm drainage system; b) ☐ directed to a permanent on-site basin; or c) ☐ directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____ : _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance. **UTILIZE EXISTING INFRASTRUCTURE ONSITE**

☐ Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.

☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .20%, V-gutter = 0.25%)

☒ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.

ITEM NO: 5 DATE: JANUARY 8, 2025

SITE PLAN NO.: 25-002

PROJECT TITLE: JACK IN THE BOX

DESCRIPTION: RESUBMITTAL 2022-252, SITE PLAN FOR "JACK IN THE BOX" RESTAURANT AT THE SE CORNER OF CALDWELL AVE AND DEMAREE ST.

APPLICANT: RYAN DUDLEY

PROP OWNER: MFT DEMAREE LLC

LOCATION: SOUTHEAST CORNER OF CALDWELL & DEMAREE

APN: 121-580-008

- ☐ All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- ☐ Traffic indexes per city standards:
- ☒ Install street striping as required by the City Engineer.
- ☒ Install landscape curbing (typical at parking lot planters). **ONSITE, PER CIVIL PLANS**
- ☐ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☒ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☐ Provide "R" value tests: each at
- ☐ Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide wide riparian dedication from top of bank.
- ☐ Show Valley Oak trees with drip lines and adjacent grade elevations. ☐ Protect Valley Oak trees during construction in accordance with City requirements.
- ☐ A permit is required to remove Valley Oak trees. Contact Public Works Admin at 713-4428 for a Valley Oak tree evaluation or permit to remove. ☐ A pre-construction conference is required.
- ☒ Relocate existing utility poles and/or facilities. **AS NEEDED WITH PROPOSED DEVELOPMENT**
- ☐ Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer:
- ☒ Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- ☒ If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- ☒ If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- ☒ Comply with prior comments. ☐ Resubmit with additional information. ☐ Redesign required.

Additional Comments:

1. ***A building permit is required, standard plan check and inspection fees will apply.***
2. ***Project is subject to the underlying Specific Plan established with overall development, comply with master plan conditions.***
3. ***Install remaining frontage improvements along Demaree. Match parkway width of 10-feet per previous development to the north and in accordance with Specific Plan. A 7-foot sidewalk will need to be installed per current City Standards.***
4. ***Utilize/Connect to onsite sewer/storm infrastructure previously installed with Specific Plan improvements.***
5. ***In addition to the applicable impact fees to be assessed with proposed project, the new development is subject to an existing Area of Benefit for the installation of a traffic signal at the intersection of Packwood and Demaree. Per the Area of Benefit, the proportionate share for this parcel has been calculated to be \$2,010.45, due at time of building permit issuance.***
6. ***Provide any common access and utilities easements/agreements established and any revisions thereof necessary for this project to be made part of the master development.***
7. ***Provide pedestrian accessible connection internally to the future developments.***

- 8. Any backflow or fire devices shall be installed on private property and not in the public right of way or any existing easements.**
- 9. Adhere to City Standard LLA processes. Coordinate with the Planning Department for LLA criteria.**
- 10. If a parcel map is desired, applicant would need to submit a Tentative Parcel Map separately through the Site Plan Review process.**

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: **25-002**

Date: **01/08/2025**

Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date: **8/17/2024**)

(Project type for fee rates: **RETAIL/FAST FOOD**)

☒ Existing uses may qualify for credits on Development Impact Fees.

FEE ITEM	FEE RATE
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	\$1,663/AC
<input checked="" type="checkbox"/> Transportation Impact Fee	\$18,735/1KSF
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	\$5,828/EACH X 1
<input checked="" type="checkbox"/> Treatment Plant Fee	\$25,558/EACH X 1
<input checked="" type="checkbox"/> Sewer Front Foot Fee	\$55/LF (DEMAREE)
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	\$8,355/AC
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	\$6,138/AC
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	\$11,142/AC
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	\$2,437/AC
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$713/1KSF
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Luqman Ragabi

City of Visalia
Building: Site Plan
Review Comments

SPR 25002
JACK IN THE BOX
121520002

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
Please refer to the applicable California Code & local ordinance for additional requirements.

- ☐ A building permit will be required. *For information call (559) 713-4444*
- ☐ Submit 1 digital set of professionally prepared plans and 1 set of calculations. *(Small Tenant Improvements)*
- ☐ Submit 1 digital set of plans prepared by an architect or engineer. Must comply with 2016 California Building Cod Sec. 2308 for conventional light-frame construction or submit 1 digital set of engineered calculations.
- ☐ Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- ☐ Meet State and Federal requirements for accessibility for persons with disabilities.
- ☐ A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- ☐ All accessible units required to be adaptable for persons with disabilities.
- ☐ Maintain sound transmission control between units minimum of 50 STC.
- ☐ Maintain fire-resistive requirements at property lines.
- ☐ A demolition permit & deposit is required. *For information call (559) 713-4444*
- ☐ Obtain required permits from San Joaquin Valley Air Pollution Board. *For information call (661) 392-5500*
- ☐ Plans must be approved by the Tulare County Health Department. *For information call (559) 624-8011*
- ☐ Project is located in flood zone _____ * ☐ Hazardous materials report.
- ☐ Arrange for an on-site inspection. (Fee for inspection \$157.00) *For information call (559) 713-4444*
- ☐ School Development fees.
- ☐ Park Development fee \$ _____, per unit collected with building permits.
- ☐ Additional address may be required for each structure located on the site. *For information call (559) 713-4320*
- ☐ Acceptable as submitted
- ☒ No comments at this time

Additional comments: _____

VAL CARRERA 1/2/25
Signature

**Site Plan Comments**

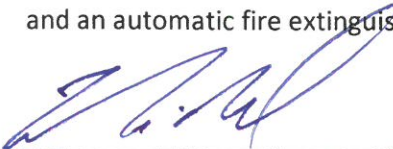
Visalia Fire Department
Corbin Reed, Fire Marshal
420 N. Burke
Visalia CA 93292
559-713-4272 office
prevention.division@visalia.city

Date	January 8, 2025
Item #	5
Site Plan #	25002
APN:	12158008

- The Site Plan Review comments are issued as **general overview** of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2022 California Fire Code (CFC), 2022 California Building Codes (CBC) and City of Visalia Municipal Codes.
- Construction and demolition sites prior to and during construction shall comply with the following:
 - **Water supply** for fire protection, either temporary or permanent, shall be made available as soon as combustible materials arrive on the site. 2022 CFC §3312
 - Provide an all-weather, 20 feet width **construction access road** capable of holding a 75,000 pound fire apparatus. Fire apparatus access shall be provided within 100 feet of temporary or permanent fire department connections. 2022 CFC §3310
- **Address numbers** must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses are served by a common driveway, the range of numbers shall be posted at the roadway/driveway. 2022 CFC 505.1
- All hardware on **exit doors, illuminated exit signs and emergency lighting** shall comply with the 2022 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- **Commercial dumpsters** with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. 2022 CFC 304.3.3
- A **Knox Box key lock system** is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. Go to knoxbox.com to order and please allow adequate time for shipping and installation. 2022 CFC 506.1
- Where a portion of any building is more than 400 feet from a hydrant on a fire apparatus access road, **on-site fire hydrant(s)** shall be provided. 2022 CFC 507.5.1, App B and C
- Due to insufficient building information, the number and distance between **fire hydrants** cannot be determined by the Site Plan Review process. The number of fire hydrants and distance between required fire hydrants shall be determined by utilizing type of construction and square footage in accordance with 2022 CFC §507, App B and C.

To determine **fire hydrant** location(s) and distribution the following information should be provided to the Site Plan Review committee: Type of construction_____ Square footage _____

- Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produce grease laden vapors shall be provided with a **Type 1 Hood**, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. 2022 CFC 904.12 & 609.2



Corbin Reed
Fire Marshal



City of Visalia
Police Department
303 S. Johnson St.
Visalia, CA 93292
(559) 713-4370

Date: 01/08/25
Item: 5
Site Plan: SPR25002
Name: Austin Huerta

Site Plan Review Comments

- ☒ No Comment at this time.
- ☐ Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- ☐ Public Safety Impact Fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001.
- ☐ Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.
- ☐ Not enough information provided. Please provide additional information pertaining to:

☐ Territorial Reinforcement: Define property lines (private/public space).

☐ Access Controlled/ Restricted etc.

☐ lighting Concerns:

☐ Traffic Concerns:

☐ Surveillance Issues:

☐ Line of Sight Issues:

☐ Other Concerns:

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

January 8, 2025

ITEM NO: 5 Added to Agenda MEETING TIME: 10:00
SITE PLAN NO: [SPR25002](#) ASSIGNED TO: Colleen Moreno Colleen.Moreno@visalia.city
PROJECT TITLE: Ryan Dudley
DESCRIPTION: Resubmittal of SPR 2024-252 -- Site Plan for a "Jack in the Box" restaurant at the SE corner of Caldwell Ave. and Demaree St.
APPLICANT: Ryan Dudley - Applicant
OWNER: MFT DEMAREE LLC

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- ☐ No Comments
- ☒ See Previous Site Plan Comments
- ☐ Install Street Light(s) per City Standards at time of development.
- ☐ Install Street Name Blades at Locations at time of development.
- ☐ Install Stop Signs at **local road intersection with collector/arterial** Locations.
- ☒ Construct parking per City Standards PK-1 through PK-4 at time of development.
- ☐ Construct drive approach per City Standards at time of development.
- ☐ Traffic Impact Analysis required (CUP)
 - ☒ Provide more traffic information such as see below. Depending on development size, characteristics, etc., a TIA may be required.

- ☐ Additional traffic information required (Non Discretionary)
- ☐ Trip Generation - Provide documentation as to concurrence with General Plan.
- ☐ Site Specific - Evaluate access points and provide documentation of conformance with COV standards. If noncomplying, provide explanation.
- ☐ Traffic Impact Fee (TIF) Program - Identify improvements needed in concurrence with TIF.

Additional Comments:

- Demaree is an arterial status roadway. Distance between driveways is restricted to a minimum of 500' apart. Noted that second proposed driveway on Demaree was removed from site plan and new layout is acceptable.
- Applicant to ensure drive thru queue does not impede main drive aisles nor spillover onto public ROW.
- Provide conformance with TIA previously performed for master plan of site. Provide change (Increase/decrease) in projected trips generated by project in comparison with previous proposed land use in master plan. If an increase and depending on size of increase, additional traffic information may be required.
- Show carwash, and connection with internal drive aisles for master plan of site.

Leslie Blair

Leslie Blair

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4532
COMMERCIAL BIN SERVICE

25002

January 8, 2025

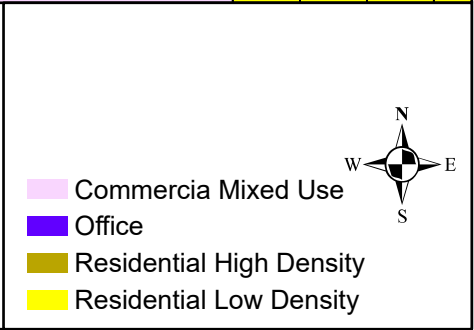
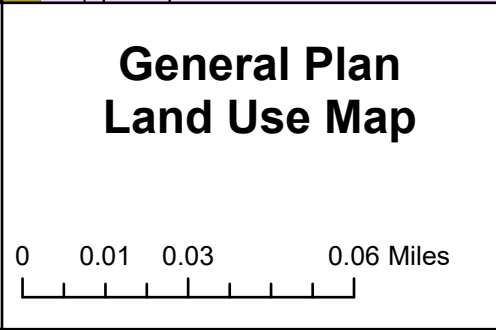
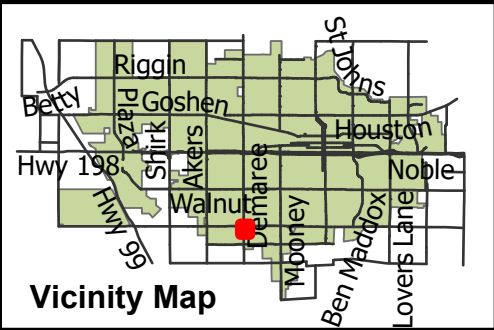
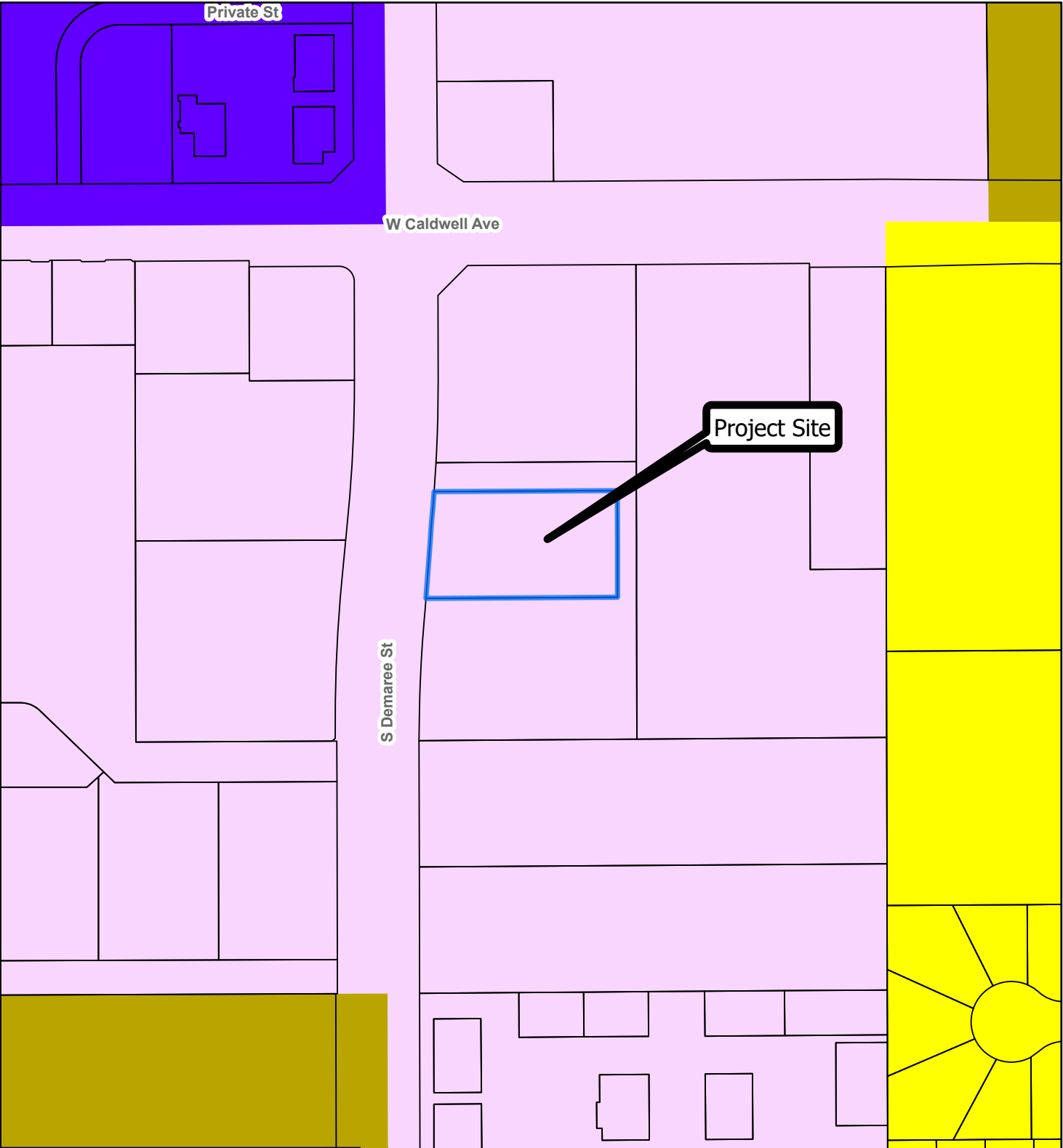
- ☐ No comments.
- ☒ See comments below
- ☐ Revisions required prior to submitting final plans. See comments below.
- ☐ Resubmittal required. See comments below.
- ☒ Customer responsible for all cardboard and other bulky recyclables to be broken down before disposing of in recycle containers
- ☒ ALL refuse enclosures must be city standard R-1 OR R-2 & R-3 OR R-4
- ☒ Customer must provide combination or keys for access to locked gates/bins
- ☐ Type of refuse service not indicated.
- ☐ Location of bin enclosure not acceptable. See comments below.
- ☐ Bin enclosure insufficient to comply with state recycling mandates. See comments for suggestions.
- ☐ Inadequate number of bins to provide sufficient service. See comments below.
- ☐ Drive approach too narrow for refuse trucks access. See comments below.
- ☐ Area not adequate for allowing refuse truck turning radius of : Commercial 50 ft. outside 36 ft. inside; Residential 35 ft. outside, 20 ft. inside.
- ☒ Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- ☒ Bin enclosure gates are required
- ☐ Hammerhead turnaround must be built per city standards.
- ☐ Cul - de - sac must be built per city standards.
- ☒ Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- ☒ Area in front of refuse enclosure must be marked off indicating no parking
- ☒ Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS) with no less than 38' clear space in front of the bin, included the front concrete pad.
- ☐ Customer will be required to roll container out to curb for service.
- ☒ Must be a concrete slab in front of enclosure as per city standards, the width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.
- ☐ Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.
- ☒ City ordinance 8.28.120-130 (effective 07/19/18) requires contractor to contract with City for removal of construction debris unless transported in equipment owned by contractor or unless contracting with a franchise permittee for removal of debris utilizing roll-off boxes.

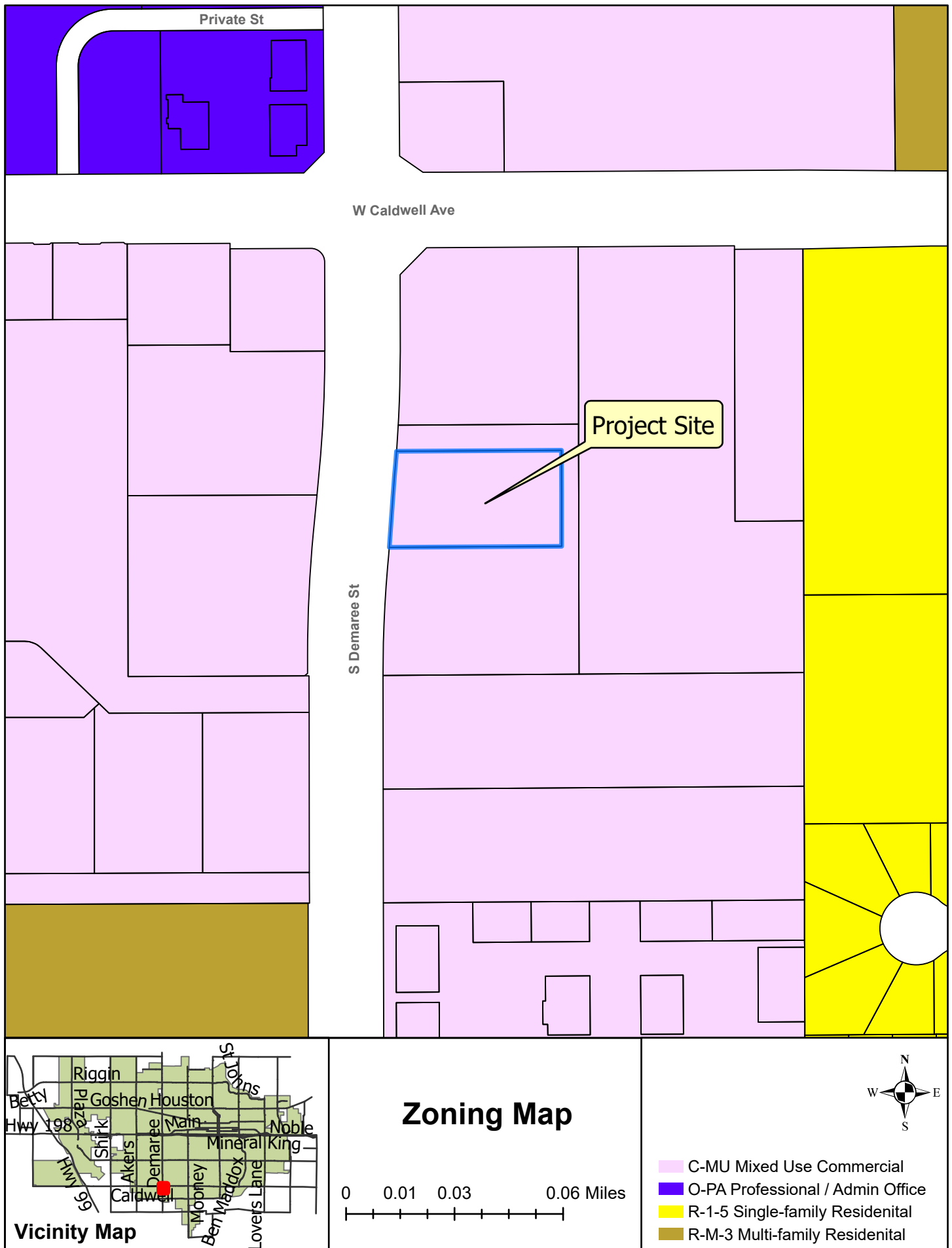
Comment

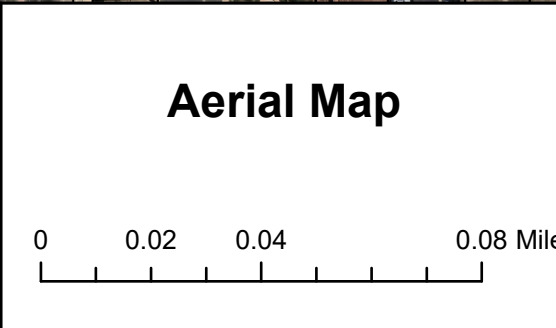
Solid waste services to include trash, recycling, and organics recycling, per the State of California's mandatory recycling laws (AB-341 & AB-1826). The proposed triple enclosure looks good for STAB load collections. Enclosure gates are required and must swing 180 degrees, clearing all curbing. Gates are to swing 180 degrees, clear all curbing, and must include Cane bolts to secure them when opened. Property owner to complete and submit a City of Visalia "Concrete/Driveway Release of Liability Waiver Agreement".

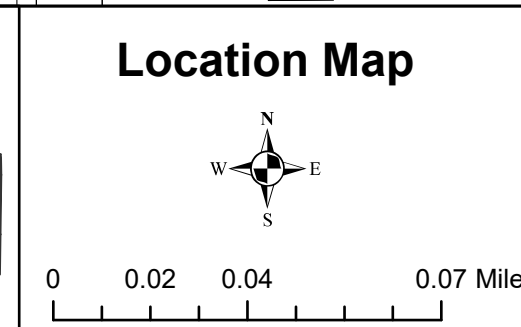
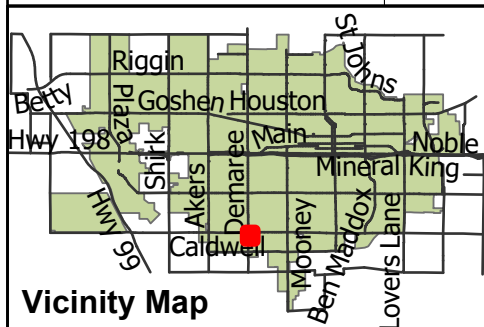
Jason Serpa, Solid Waste Manager, 559-713-4532
Edward Zuniga, Solid Waste Supervisor, 559-713-4338

Nathan Garza, Solid Waste, 559-713-4532









City of Visalia



To: Planning Commission

From: Brandon Smith, Principal Planner
Paul Bernal, Director/City Planner

Date: May 12, 2025

Re: 2024 Housing Element Annual Progress Report (APR) and Program Reporting

Summary

Pursuant to State Housing Law (Government Code Section §65400(a)(2)), cities and counties are required to report their progress on implementing their current Housing Elements. The report includes a summary of the jurisdiction's housing activity, progress in reaching their Regional Housing Needs Assessment (RHNA) by income level, and progress in implementing the City's Housing Element's goals and programs.

The report is a useful tool for City officials, staff, citizens, and stakeholder groups interested in the City's housing trends, particularly as they relate to affordable housing. The report must be reviewed by the City Council, submitted to HCD by April 1st, and accepted by HCD. The failure to prepare and deliver the report on a timely basis incurs the risk of the City losing future housing grant funding opportunities.

The report demonstrates that the City follows all applicable State laws and policies pertaining to housing, and that the City is implementing its housing policies and programs as articulated in its 6th Cycle Housing Element adopted in December 2023, for the reporting period from December 31, 2023, through December 31, 2031.

Background Discussion

The 2024 APR forms and tables are unchanged from the previous year's format. The APR continues to require listing and affordability determination of all "considered" housing projects (i.e., potential projects submitted to the Site Plan Review Committee), completed entitlements, and individual building permits for every individual housing unit. Jurisdictions report these units to gain credit against their RHNA housing unit requirements. The expanded reporting is in accordance with the State's enhanced focus on the housing development process.

All residential building permits must be tracked through to their final grant of occupancy. Only units that receive issuance of a building permit during the reporting period are counted toward the jurisdiction's RHNA allocation. Notwithstanding the interest in the jurisdiction's performance on providing affordable housing to lower income households, the APR requires detailed permit information on all residential units.

The contents of the APR are described below along with their applicability to the City.

	Table Name	Description
Table A	Housing development applications submitted	Lists all applications deemed complete in 2024. Examples: Subdivision Maps, Conditional Use Permits, Site Plan Review applications receiving a “Revise & Proceed” and needing no further entitlements.
Table A2	Annual building activity report summary	Lists all housing units that have received an entitlement, issued and/or finalized building permit in 2024. Examples: building permits for single-family, multi-family, and accessory dwelling units, applications listed in Table A.
Table B	Regional Housing Needs Allocation (RHNA) Progress	Summary of permits issued by income level in each year of RHNA allocation.
Table C	Sites identified / rezoned to accommodate shortfall housing need	Lists all sites identified in the City’s re-zone program (i.e. certain sites identified in prior cycles and annexation sites) adopted through ZTA 2024-05 on 12/16/2024.
Table D	Housing Element Program implementation status	Reports on all 6 th Cycle Housing Element programs.
Table E	Commercial development bonuses approved	N/A for Visalia. No such applications received in the reporting period.
Table F	Units rehabilitated, preserved and acquired for alternative adequate sites	N/A for Visalia. Optional table.
Table F2	Above moderate income units converted to moderate income units	N/A for Visalia. Optional table.
Table G	Locally owned lands included in Housing Element sites inventory that have been sold, leased, or disposed	N/A for Visalia. No locally owned inventory land has been sold.
Table H	Locally Owned or Controlled Lands Declared Surplus	Lists sites designated as surplus. Currently none of these sites contain housing.
Table J	Student housing development with a density bonus approved	N/A for Visalia. No student housing applied for.
Table K	Tenant Preference Policy	N/A for Visalia. No tenant preference policy adopted by City.
Summary	Summary of data from preceding tables	Summary of data from preceding tables
LEAP Reporting	Status of proposed uses listed in LEAP application and the corresponding impact on housing	Reports on the City’s progress towards its LEAP grant, which was approved in 2021.

Report Highlights

Table A: Application Submittal Summary

- Entitlement Applications. Entitlement applications reflecting a total of 926 units were submitted and deemed complete in 2024, down from 1,878 units in 2023. This includes:
 - six subdivisions totaling 517 single-family residential units, including two that were deemed complete in 2024 and scheduled for Planning Commission public hearings in 2025 (see Table A),
 - one conditional use permit (CUP) application for 276 market-rate apartment dwelling units as part of a mixed use development associated with the Packwood Creek commercial development, north of the Costco Warehouse on Cameron Avenue,
 - one application yielding two duplexes on two R-1-5 zone properties, upon approval of a lot line adjustment, allowed through Senate Bill 9 legislation, and
 - four parcel maps yielding 9 dwelling units.
- Site Plan Review by-right uses. Units tallied through Site Plan Review applications for by right uses receiving a “Revise & Proceed” are approximately 120 units over eight projects. This includes:
 - Rancho Colegio, an 80-unit deed-restricted Very Low Income development (Dinuba & Glendale, Self-Help Enterprises). Building permits were also issued in 2024.
 - A 6-unit deed-restricted Very Low Income Development (Santa Fe & Tulare, Habitat For Humanity). This was previously submitted to Site Plan Review in 2022.

Table A2: Building Activity

Comparative data sets between 2024 and the previous four years’ APR submittals are shown in the tables below. As shown in the table below, this represents a 71% increase in the total number of building permits issued in calendar year 2023. The increase in building permit activity is largely attributed to two multi-family residential projects. ADU activity has also seen significant increase for the 4th straight year.

<u>Residential Permit Category</u>	<u>2020</u>	<u>2021</u>	<u>2022</u>	<u>2023</u>	<u>2024</u>
Issued building permits:					
ADUs	2	5	16	22	37
Single-Family Units	599	632	540	317	236
Multi-Family Units	6	204	695	12	327
Granted occupancy:					
ADUs	3	4	8	20	24
Single-Family Units	571	590	651	448	297
Multi-Family Units	38	2	43	296	170

<u>Affordability by Income</u> (issued permits)	<u>2020</u>	<u>2021</u>	<u>2022</u>	<u>2023</u>	<u>2024</u>
Above Moderate	92	76	124	165	184
Moderate	504	548	411	156	61
Low	10	170	657	16	197
Very Low	1	40	22	11	156
Extremely Low	0	7	87	3	2
TOTAL	607	841	1301	351	600

- Multi-Family Residential. The 327 multi-family units that were issued permits are all classified as affordable to the Very Low or Low Income categories based on the density of the units' underlying zone district. The largest contributing projects are:

- Demaree & Houston Apartments (222 units),
- Rancho Colegio (Court & Glendale, 80 units),
- Santa Fe Duplexes (1400 block S. Santa Fe Street, 12 units),

- Deed Restricted Affordability. Permits issued for 302 multi-family units will be deed restricted, affordable to Very Low and Low Income households. These are affiliated with the Demaree & Houston Apartments, and Rancho Colegio to be managed by Self-Help Enterprises.

- Single-Family Residential - Very Low Income Affordability. A total of nine accessory dwelling units (ADUs) were issued permits classified as affordable to the Very Low Income category based on total valuation of the permit and number of bedrooms.

- Single-Family Residential - Low Income Affordability. A total of 17 ADUs were issued permits classified as affordable to the Low Income category based on total valuation of

the permit and number of bedrooms. No market-rate single-family residences qualified for low-income affordability.

- Single-Family Residential - Moderate & Above Moderate Affordability. A total of 7 ADUs and 54 single-family residences are classified as affordable to the Moderate Income category, and 184 single-family residences are classified as affordable to the Above Moderate Income category, based on total valuation of the permit and number of bedrooms.

- Affordability Determination. Like in previous reporting years, affordability in the 2024 reporting year was determined based on an affordability calculator made available by State HCD through their website.

Table B: Regional Housing Needs Assessment (RHNA) Compliance:

The table reports the first full year (2024) of progress toward the City of Visalia's Regional Housing Needs Allocation.

Table C: Sites Identified or Rezoned to Accommodate Shortfall Housing Need:

Under the City's obligation in conformance with state law, the Planning Division in 2024 identified certain sites that are inventoried to meet lower income capacity assumptions that were pending annexation. These sites were added to the sites inventory and were subject to by-right approval without discretionary action. The City Council adopted Zone Text Amendment No. 2024-05 in December 2024 to create a corresponding Overlay District for these sites.

There were no discretionary actions that reduced the City's RHNA obligations requiring the City to identify or rezone additional sites, in conformance with the No Net-Loss Law.

Table D: Housing Element Implementation:

One program accomplishment involved changes to the City's Zoning Ordinance to address Implementation Programs 1.1 and 1.7 (briefly mentioned above), 5.8 to clarify that ADUs are allowed by right in all zones allowing residential uses, and 5.11 to adopt a policy ensuring compliance with the Permit Streamlining Act and timing requirements of the California Environmental Quality Act.

There were several additional outreach and financial accomplishments that took place in 2024.

The City held 4 public hearings and 6 community meetings (both an increase from 2023) covering CDBG, HOME, and NSP funded programs, which included affordable housing programs (Program 1.4 - Housing Education; Program 3.7 - Inform Public).

Maps illustrating the City's infill site inventory were updated in January 2024 (Program 2.1 - Infill Site Inventory).

The City approved the sale of City-owned land at low-cost to Habitat for Humanity for the development of six homes to be sold to first-time homebuyer veterans (Program 3.13 - Non-Profit Housing Development Corporations).

The City provided \$118,000 in PLHA funds for a Seasonal Emergency Low-Barrier Shelter (Warming Center), and \$125,000 in CDBG funds for a transitional bridge housing project called Eden House for people experiencing homelessness. The City also provided an additional \$1,950,182 in PLHA funds, \$200,000 in Successor Housing funds, and \$3,628,371 in ERF funds for the development of a 100- bed Low-Barrier Navigation Center (Program 5.2 - Homeless Shelter Program).

Regional Housing Needs Assessment (RHNA) Compliance:

There were no discretionary actions that affected the City's RHNA obligations. Consequently, the RHNA remains in compliance with the Housing Element and State law.

Housing Element Annual Report

The Housing Element contains three implementation programs that require an annual report to the Planning Commission on certain topics that help to better facilitate the construction of affordable housing or housing for lower income groups. The programs and the corresponding reports are below.

Program 1.3: Conditional Use Permit Process for Multi-Family Projects. This program requires that the City annually assess the Conditional Use Permit (CUP) process for multi-family residential projects greater than 80 housing units to ensure that the entitlement process does not adversely impact the timing, cost, or supply of multi-family development. An assessment that summarizes all multi-family residential projects considered by the Commission over the past year shall be provided to the Commission.

In the 2024 calendar year, the Planning Commission considered one CUP in association with a multi-family project in the C-R zone district exceeding 80 units, and did not consider any other CUPs associated with multi-family projects. The one CUP exceeding 80 units is described as follows:

- Conditional Use Permit No. 2024-02: A request by Mulberry Springs LLC, to master plan and develop a 15.55-acre site into a mixed-use development. Phase 1 consists of a 276-unit multi-family development with three-story buildings, a community club house, and outdoor recreation amenities while Phase 2 consists of two multi-tenant commercial buildings and one retail building with a drive-thru lane. The project site is located at the Northwest corner of West Cameron Avenue and South Stonebrook Street. (Address: N/A) (APNs: 122-332-039, 122-332-040, and 122-332-041). The Planning Commission considered and approved this CUP on March 25, 2024.

With regards to timing, the following dates describe when the project was received, deemed complete, and received a public hearing.

	<u>Project Received</u>	<u>Project Deemed Complete</u>	<u>Public Hearing</u>
CUP 2024-02	1/3/2024	2/19/2024 (1.5 months)	3/25/2024 (3 months)

CUP 2024-02 Background: The C-R zone provides residential development as a conditionally-allowed use, whether paired with non-residential uses or stand-alone. Therefore, a CUP was necessary for any residential use occupying the site. The General Plan does not specify a density range for development in the zone.

As described above, this project received approval of a CUP in three months from the time of the initial submittal and five weeks from the time of the project being deemed complete, which meets the estimated four to eight-week total processing time for this project as indicated in the 6th Cycle Housing Element Table 48. The CUP process and the associated cost was not considered a deterrent to the number of units associated with the project since the applicant was able to achieve the desired unit count and product type associated with their application. Furthermore, the project did not receive any public opposition that was triggered in association with the CUP's public notification requirements.

Program 2.5: Incentives for Infill Affordable Housing. This program requires that the City annually report to the Planning Commission projects that meet the threshold criteria and take advantage of the Affordable Housing Infill Incentive Program. This program reduces Transportation Impact Fees up to 60% of the base fee for residential projects in qualifying infill sites. This program became effective November 16, 2017, in fulfillment of a previously established Housing Element Program.

In the calendar year 2024, there were no projects that were issued building permits and took advantage of this Program.

Program 9.4: CUP Requirement Impacts on Affordable Housing. This program requires that the City monitor every proposed development on RHNA land inventory sites to ensure that the effects of processing a CUP, when required, does not unreasonably delay entitlement processing, increase development costs in a manner that eliminates potential affordability, or affect potential development density. The City must annually report to the Planning Commission the effect of CUP requirements compared with comparable "permitted by right" projects. This is to ensure that the CUP processing and conditions do not adversely affect the project's affordability.

In the 2024 calendar year the City processed one CUP on a RHNA land inventory site, corresponding with the location of the one CUP described above. As stated in the analysis for Program 1.4, this project's entitlement timeframe, cost, and density was not adversely affected by the CUP process. No comparable permitted by-right project could be found for this mixed-use project. The City did see building permits issued for a permitted by-right complex in 2024 (Rancho Colegio), located in the Commercial Mixed Use zone, however this complex only contained 80 units. The one project subject to the CUP process was conditioned to conform to the applicant's submitted site plan, elevations, and operational statement, all of which the applicants consented to as determined on their submittals. Otherwise, the CUPs did not impose any conditions beyond the development standards contained in the Zoning Ordinance.

Attachments

- 2024 Annual Progress Report (Electronic)