

# PLANNING COMMISSION AGENDA

CHAIRPERSON:

Brett Taylor



VICE CHAIRPERSON:

Liz Wynn

COMMISSIONERS: Brett Taylor, Liz Wynn, Chris Gomez, Marvin Hansen, Sarrah Peariso

**ON MONDAY, APRIL 9, 2018 AT 7:00 P.M., AT THE VISALIA CONVENTION CENTER  
LOCATED AT 303 E. ACEQUIA AVENUE, VISALIA:**

1. THE PLEDGE OF ALLEGIANCE –
2. CITIZEN'S COMMENTS – This is the time for citizens to comment on subject matters that are not on the agenda but are within the jurisdiction of the Visalia Planning Commission. The Commission requests that a 5-minute time limit be observed for comments. Please begin your comments by stating and spelling your name and providing your street name and city. Please note that issues raised under Citizen's Comments are informational only and the Commission will not take action at this time.
3. CHANGES OR COMMENTS TO THE AGENDA–
4. CONSENT CALENDAR - All items under the consent calendar are to be considered routine and will be enacted by one motion. For any discussion of an item on the consent calendar, it will be removed at the request of the Commission and made a part of the regular agenda.
  - No Items on Consent Calendar
5. PUBLIC HEARING – Paul Bernal  
Continued Public Hearing for Conditional Use Permit No. 2017-22: A request to develop a 1.91-acre area within the Cameron Creek Marketplace Shopping Center with an ARCO AM/PM gas station consisting of a 6,000 square foot convenience store building with a future fast-food restaurant space (no tenant has identified), a 4,933 square foot automated carwash building, and a 6,775 square foot fuel canopy with 10 fuel dispensers. The Cameron Creek ARCO AM/PM is part of the Cameron Creek Shopping Center and zoned C-MU (Commercial Mixed Use). The project area for the proposed ARCO AM/PM gas station is located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037 & 126-870-039).
6. DIRECTOR'S REPORT/ PLANNING COMMISSION DISCUSSION-

The Planning Commission meeting may end no later than 11:00 P.M. Any unfinished business may be continued to a future date and time to be determined by the Commission at this meeting. The Planning Commission routinely visits the project sites listed on the agenda.

For Hearing Impaired – Call (559) 713-4900 (TTY) 48-hours in advance of the scheduled meeting time to request signing services.

Any written materials relating to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the City Office, 315 E. Acequia Visalia, CA 93291, during normal business hours.

**APPEAL PROCEDURE**

**THE LAST DAY TO FILE AN APPEAL IS THURSDAY, APRIL 19, 2018 BEFORE 5 PM**

According to the City of Visalia Zoning Ordinance Section 17.02.145 and Subdivision Ordinance Section 16.04.040, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal form with applicable fees shall be filed with the City Clerk at 220 N. Santa Fe, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website [www.visalia.city](http://www.visalia.city) or from the City Clerk.

**THE NEXT REGULAR MEETING WILL BE HELD ON MONDAY, APRIL 23, 2018**

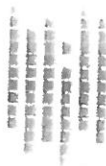


**ACOUSTICAL ANALYSIS**  
**CAMERON CREEK MARKETPLACE CAR WASH**  
**VISALIA, CALIFORNIA**

**WJVA Project No. 17-007**

**PREPARED FOR**  
**4CREEKS, INC.**  
**324 SOUTH SANTA FE AVENUE, SUITE A**  
**VISALIA, CA 93292**

**PREPARED BY**  
**WJV ACOUSTICS, INC.**  
**VISALIA, CALIFORNIA**



**wjv acoustics**

**FEBRUARY 26, 2018**

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113 N. Church Street, Suite 203 · Visalia, CA 93291 · (559) 627-4923

## **INTRODUCTION**

The project is a proposed ARCO-AM/PM gas station, car wash and convenience store facility to be located along the southeast corner of West Caldwell Avenue and South West Avenue, in Visalia, California. Existing single-family residences are located to the northwest, south and west of the project site.

This report is based upon the project site plan dated September 8, 2017, noise measurements obtained by WJV Acoustics, Inc. (WJVA) at the project site, reference noise measurements obtained at existing gas station and car wash facilities and information provided to WJVA by the project applicant concerning the proposed equipment and hours of operation of the project. Revisions to the site plan or other project-related information available to WJVA at the time the analysis was prepared may require a reevaluation of the findings and/or recommendations of the report. The Project Site Plan is provided as Figure 1.

Appendix A provides definitions of the acoustical terminology used in this report. Unless otherwise stated, all sound levels reported in this analysis are A-weighted sound pressure levels in decibels (dB). A-weighting de-emphasizes the very low and very high frequencies of sound in a manner similar to the human ear. Most community noise standards utilize A-weighted sound levels, as they correlate well with public reaction to noise. Appendix B provides typical A-weighted sound levels for common noise sources.

## **PROJECT DESCRIPTION**

The project applicant is requesting to develop a 1.91-acre area within the Cameron Creek Market Place Shopping Center with an ARCO-AM/PM gas station consisting of 6,000 square foot convenience store building, a 4,933 square foot automated carwash building, and a 6,775 square foot fuel canopy with 10 fuel dispensers. The Cameron Creek ARCO-AM/PM project site is zoned Commercial Mixed Use and is located on the southeast corner of West Caldwell Avenue and South West Street, in Visalia.

## **PURPOSE AND OBJECTIVES**

The City of Visalia has requested an acoustical analysis to determine if noise levels associated with the project will comply with the City's applicable noise level standards. This acoustical analysis was intended to determine project-related noise levels for all aspects of the proposed project. If the analysis determines that the project has the potential to exceed the City's applicable noise level standards at nearby noise sensitive receivers, appropriate mitigation measures will be provided to reduce project-related noise levels to below the City's applicable noise level standards.

## CRITERIA FOR ACCEPTABLE NOISE EXPOSURE

The City of Visalia Noise Element of the General Plan (noise element) establishes noise level criteria in terms of the Day-Night Average Level ( $L_{dn}$ ) metric. The  $L_{dn}$  is the time-weighted energy average noise level for a 24-hour day, with a 10 dB penalty added to noise levels occurring during the nighttime hours (10:00 p.m.-7:00 a.m.). The  $L_{dn}$  represents cumulative exposure to noise over an extended period of time and is therefore calculated based upon *annual average* conditions.

The exterior noise level standard of the noise element is 65 dB  $L_{dn}$  for outdoor activity areas of residential uses. Outdoor activity areas generally include backyards of single-family residences and individual patios or decks and common outdoor activity areas of multi-family developments. The intent of the exterior noise level requirement is to provide an acceptable noise environment for outdoor activities and recreation.

The noise element also requires that interior noise levels attributable to exterior noise sources not exceed 45 dB  $L_{dn}$ . The intent of the interior noise level standard is to provide an acceptable noise environment for indoor communication and sleep.

Additionally, the noise element establishes hourly acoustical performance standards for non-transportation noise sources. The standards are set in terms of the  $L_{eq}$  (hourly equivalent) and  $L_{max}$  (maximum) noise levels. The standards, provided in Table I, are made more restrictive during the nighttime hours of 10:00 p.m. to 7:00 a.m.

<p style="text-align: center;"><b>TABLE I</b></p> <p style="text-align: center;"><b>NON-TRANSPORTATION NOISE LEVEL STANDARDS, dBA</b></p> <p style="text-align: center;"><b>CITY OF VISALIA</b></p>			
Daytime (7 a.m.-10 p.m.)		Nighttime (10 p.m.-7 a.m.)	
$L_{eq}$	$L_{max}$	$L_{eq}$	$L_{max}$
50	70	45	65
Source: City of Visalia Noise Element of General Plan			

Section 8.36 of the City's Municipal Code (noise ordinance) applies to noise sources that are not pre-empted from local control by existing state or federal regulations. Commercial activities are not pre-empted noise sources and are therefore subject to the provisions of the noise ordinance.

The noise ordinance addresses the statistical distribution of noise over time and allows for progressively shorter periods of exposure to levels of increasing loudness. Table II summarizes the exterior noise level standards of the ordinance. Note that the ordinance is to be applied during any one-hour time period of the day, and that the standards are 5 dB more restrictive between the hours of 7:00 p.m. and 6:00 a.m.

<p style="text-align: center;"><b>TABLE II</b></p> <p style="text-align: center;"><b>EXTERIOR NOISE LEVEL STANDARDS, dBA</b> <b>CITY OF VISALIA NOISE ORDINANCE</b></p>			
Category	Cumulative # Min/Hr. (L <sub>n</sub> )	Daytime (6am-7pm)	Nighttime (7pm-6am)
1	30 (L <sub>50</sub> )	50	45
2	15 (L <sub>25</sub> )	55	50
3	5 (L <sub>8.3</sub> )	60	55
4	1 (L <sub>1.7</sub> )	65	60
5	0 (L <sub>max</sub> )	70	65
<p>Note: L<sub>n</sub> is an abbreviation for the percentage of time that a certain noise level is exceeded during a specified time period (in this case, one hour). For example, an L<sub>50</sub> value of 50 dBA may not be exceeded during the hours of 6 am-7pm.</p> <p>Source: City of Visalia Municipal Code</p>			

The City's noise ordinance also establishes interior residential noise level standards that would apply to the project. The interior noise level standards are established in allowable exceedance limits over differing amounts of time, within residential land uses. Similar to the applicable exterior standards, the interior standards become 5 dB more restrictive during nighttime hours. The applicable interior noise level standards are provided in Table III.

<p style="text-align: center;"><b>TABLE III</b></p> <p style="text-align: center;"><b>INTERIOR NOISE LEVEL STANDARDS, dBA</b> <b>CITY OF VISALIA NOISE ORDINANCE</b></p>			
Category	Cumulative # Min/Hr.	Daytime (6am-7pm)	Nighttime (7pm-6am)
1	5	45	35
2	1	50	40
3	0	55	45
<p>Source: City of Visalia Municipal Code</p>			

The City's noise ordinance also states *"In the event the measured ambient noise level without the alleged offensive source in operation exceeds an applicable noise level standard in any category above, the applicable standard or standards shall be adjusted so as to equal the ambient noise level"*.

The City of Visalia General Plan suggests *"The Noise Ordinance should be updated to be consistent with the General Plan"*.

In regards to construction noise, Section 8.36.050 of the City's noise ordinance limits acceptable hours of construction to between the hours of 6:00 a.m. to 7:00 p.m., Monday through Friday and between the hours of 9:00 a.m. to 7:00 p.m. on Saturdays and Sundays.

## EXISTING NOISE ENVIRONMENT

The project site is currently an undeveloped lot, located along the southeast corner of West Caldwell Avenue and South West Avenue, in Visalia, California. Existing single-family residences are located to the northwest, west and south of the project site. Existing noise levels within the project vicinity are dominated by vehicle traffic noise associated with roadways adjacent to the project site. A project location map is provided as Figure 2.

WJVA staff conducted background/ambient short-term (15-minute) noise level measurements on October 25, 2017 and January 31, 2018 near the project site. The intent of the noise measurements was to document existing ambient noise levels near the project site. Ambient noise level measurements were conducted at seven (7) locations near the project site, for a period of fifteen (15) minutes. The locations were representative of existing ambient noise levels effecting nearby residential land uses during midday hours. The locations of the ambient noise monitoring sites are provided as Figure 3.

Ambient noise monitoring equipment consisted of a Larson-Davis Laboratories Model LDL 820 sound level analyzer equipped with a Bruel & Kjaer (B&K) Type 4176 ½" microphone. The monitor was calibrated with a B&K Type 4230 acoustical calibrator to ensure the accuracy of the measurements. The equipment complies with applicable specifications of the American National Standards Institute (ANSI) for Type 1 (precision) sound level meters.

Table IV provides the measured noise levels at the ambient noise monitoring sites. Reference to Table I and Table II indicate that ambient noise levels measured during the monitoring intervals exceed the City's applicable standards in nearly all statistical category standards for each short-term monitoring site, with the exception of monitoring sites ST2 and ST4. In Table IV, noise levels presented in **bold** indicate the measured noise level exceeds the daytime standard, while noise levels presented in *italics* indicate that the measured noise level was below the applicable daytime noise level standard.

<p style="text-align: center;"><b>TABLE IV</b></p> <p style="text-align: center;"><b>SUMMARY OF AMBIENT NOISE LEVEL MEASUREMENTS</b>  <b>PROPOSED CAMERON CREEK MARKETPLACE ARCO-AM/PM</b>  <b>OCTOBER 25, 2017 &amp; JANUARY 31, 2018</b></p>								
Site Description	Noise Sources	Time	A-Weighted Decibels, dB					
			L <sub>eq</sub>	L <sub>50</sub>	L <sub>25</sub>	L <sub>8.9</sub>	L <sub>1.7</sub>	L <sub>max</sub>
ST1	Traffic	2:45 p.m.	59.5	52.4	58.5	65.1	68.6	71.8
ST2	Traffic	3:05 p.m.	55.9	52.1	56.1	59.8	63.8	68.8
ST3	Traffic	12:45 p.m.	65.9	56.6	65.0	71.7	75.3	79.1
ST4	Traffic	1:05 p.m.	52.0	48.2	51.5	55.5	59.4	67.0
ST5	Traffic	1:25 p.m.	64.2	57.7	63.7	69.0	71.4	82.8
ST6	Traffic	1:45 p.m.	70.4	66.9	71.3	75.5	78.0	80.3
ST7	Traffic	2:05 p.m.	70.6	66.8	71.6	75.7	77.8	79.7
Source: WJV Acoustics, Inc.								

It should be noted, noise levels described in Table IV for short-term noise monitoring sites ST1, ST3, ST4 and ST6 do not take into consideration acoustical shielding provided by the existing sound walls that are shielding the nearby backyards of these residences. It is assumed that existing ambient noise levels within the backyards of the residences located near these short-term monitoring sites would be approximately 5-6 dB lower than those described in Table IV. Taking into consideration the shielding provided by the existing sound wall, existing ambient noise levels within the backyards in the vicinity of sites ST1, ST3 and ST6 would not be high enough to warrant an adjustment to the City's exterior noise level standards.

Additionally, WJVA staff conducted nighttime noise measurements on the night/morning of January 22-23, 2018. Noise measurements were conducted at two locations (LT1 and LT2) within the project area. The intent of the measurements was to quantify existing (without project) ambient noise levels during the nighttime hours. The two measurement sites were intended to approximate the distances from both Caldwell Avenue and West Avenue to the closest existing residential backyards. The locations of the nighttime ambient noise measurement sites are provided on Figure 3 as LT1 and LT2.

Table V provides the hourly noise levels at the two nighttime measurement sites, in terms of the applicable statistical nighttime noise level standards (applicable nighttime standards provided for reference in parenthesis). The measured noise levels are provided for both nighttime measurement locations (LT1 and LT2). The ambient noise measurement field notes are provided as Appendix C.



**TABLE V**  
**SUMMARY OF NIGHTTIME AMBIENT NOISE LEVEL MEASUREMENTS**  
**PROPOSED CAMERON CREEK MARKETPLACE ARCO-AM/PM**  
**JANUARY 22-23, 2018**

Time	Statistical Category (Applicable Nighttime Standard)											
	A-Weighted Decibels, dB											
	L <sub>eq</sub> (50/45) <sup>1</sup>		L <sub>50</sub> (45)		L <sub>25</sub> (50)		L <sub>0.3</sub> (55)		L <sub>1.7</sub> (60)		L <sub>max</sub> (65)	
	LT1	LT2	LT1	LT2	LT1	LT2	LT1	LT2	LT1	LT2	LT1	LT2
7 pm	66.0	61.3	62.9	59.1	66.7	61.9	69.7	64.7	72.1	68.1	83.6	76.0
8 pm	66.5	61.7	62.6	58.7	66.5	61.6	69.9	64.5	72.3	67.1	90.3	82.9
9 pm	64.1	59.3	60.5	56.7	65.2	60.2	68.9	63.5	71.6	66.2	76.5	72.1
10 pm	62.9	58.5	57.9	54.8	62.7	58.3	67.3	62.2	70.5	65.4	85.3	80.0
11 pm	61.7	56.4	54.2	51.9	60.9	56.5	66.7	61.1	70.3	64.6	80.1	72.7
12 pm	59.7	54.8	50.6	48.8	56.0	53.2	62.6	58.6	67.6	62.6	85.0	77.9
1 am	55.5	51.3	45.1	44.1	50.8	49.3	60.0	55.9	66.3	61.4	73.5	69.5
2 am	54.0	50.0	42.1	41.7	47.9	46.8	57.5	54.4	64.9	60.3	73.6	68.0
3 am	55.4	51.6	42.8	42.6	49.6	47.6	61.2	57.1	65.6	61.8	72.6	67.9
4 am	60.4	56.4	51.2	49.4	57.9	55.2	64.2	60.6	68.8	64.6	83.0	76.7
5 am	62.1	58.5	57.1	55.0	62.6	59.0	67.4	63.2	70.4	66.2	75.2	71.9
Average	60.8	56.3	53.4	51.2	58.8	55.4	65.0	60.5	69.1	64.4	79.9	74.1

<sup>1</sup>Leq standard 50 dB (7 am to 10 pm) 45 dB (10 pm to 7am)

Source: WJV Acoustics, Inc.

Reference to Table V indicates that existing (without project) nighttime ambient noise levels in the project vicinity exceed the applicable nighttime noise level standards in nearly every category during every hour of the nighttime noise measurements, at both locations. In Table V, noise levels presented in **bold** indicate the measured noise level exceeds the nighttime standard, while noise levels presented in *italics* indicate that the measured noise level was below the applicable nighttime noise level standard. The results of the nighttime ambient noise level measurements are also presented graphically as Figure 4.

As the homes along West Street have an existing 6.5-foot sound wall, noise levels would be expected to be approximately 5-6 dB less than those measured at nighttime ambient noise monitoring sites LT1 and LT2.



## PROJECT-RELATED NOISE LEVELS

### Car Wash Operations:

Noise levels from automated/drive-thru car wash systems are dominated by noise levels associated with the dryer blowers. The blowers are located at the exit end of the car wash tunnel. Therefore, noise levels produced at the exit and of the tunnel are higher than those at the entrance end of the tunnel.

**Car Wash Tunnel:** The applicant has indicated that the proposed car wash would utilize a Belanger Drying Systems three fixed-nozzle, 30-HP blower system with silencers. According to manufacturer-supplied noise level data, noise levels associated with this dryer system would be approximately 72 dB at a distance of 100 feet from the blowers (located near the tunnel exit).

Additionally, WJVA previously measured noise levels at an existing, similar car wash facility located at a Lexus car dealership in San Jose. The tested car wash facility also utilized a Belanger Wash Systems washer with a Belanger Drying Systems three fixed-nozzles 30-HP blower system with silencers, the same as that proposed by the project applicant.

Reference noise measurements were obtained at a distance of approximately 25 feet from the car wash tunnel exit. At a distance of 25 feet from the exit tunnel, noise levels were observed to be in the range of 84-86 dB, while the blowers were in full operation. Taking into account noise attenuation that occurs with increased distance from the source (6dB/doubling of distance), the noise levels measured by WJVA at the San Jose site were consistent with the manufacturer-supplied noise level data.

During peak hours of operation, the blowers do not cycle off, and remain in use for ongoing periods of time. Noise levels generated by the blowers would be constant during peak hours of operation. Therefore, project-related (car wash) noise levels would correlate to the applicable  $L_{50}$  noise level standards established in the City's noise ordinance (and the City's  $L_{eq}$  noise level standard established in the General Plan). The applicable standards would therefore be 50 dB  $L_{50}$  during daytime hours (6:00 a.m. to 7:00 p.m.) and 45 dB  $L_{50}$  during nighttime hours (7:00 p.m. to 6:00 a.m.).

There are existing single-family residential land uses located to the northwest, west and south of the project site. The residential land uses to the northwest would be partially exposed to blower noise from the tunnel entrance, as the car wash tunnel would provide some acoustical shielding. The residential land uses to the south will be partially exposed to blower noise from the tunnel exit, as the tunnel itself will provide acoustical shielding. However, those residences located directly to the west (along South West Street) would not be shielded. WJVA analyzed potential car wash noise levels at thirteen (13) individual receiver locations. These locations are provided as Figure 5, and are labeled A through M.

The above described blower noise levels were used to calculate project-related noise levels at the closest residential land uses. For the calculations, it was assumed that sound is attenuated with increasing distance at the normal rate for a "point" noise source (6 dB/doubling of distance). The car wash tunnel would provide acoustical shielding to the existing residential land uses to the northwest and the south but would not provide shielding to the residential land uses west the tunnel entrance.

As described above, the residences located west of the project site (along West Street) and south of the project site (along Kimball Avenue, east of Watson Street) have an existing 6.5-foot sound wall along the property line. The acoustic shielding provided by the existing sound walls was considered when calculating project-related noise levels described below. The residences located along Kimball Avenue between West Street and Watson Street do not have an existing sound wall.

Noise levels associated with car wash blowers are louder at the exit end of the tunnel than the entrance end of the tunnel. WJVA has conducted numerous car wash noise level measurements and has determined that noise levels are typically 6-8 dB lower at the entrance of a car wash tunnel than noise levels at the tunnel exit. Applying a conservative reduction of 6 dB at the entrance side, and partial shielding provided by the car wash tunnel, car wash noise levels at receivers A, B, F and G would be 47.0 dB, 46.6, 49.8 dB and 48.0 dB, respectively. While receivers C, D and E, which would receive relatively no measurable acoustic shielding by the tunnel, and would be exposed to car wash noise levels of 53.4 dB, 54.7 dB and 54.5 dB, respectively. Such levels exceed the City's 50 dB standard and mitigation must be required.

Noise levels at the residences along Kimball Avenue (receivers H through M) would be exposed to a varying combination of entrance and exit noise levels, but would be acoustically shielded by the car wash tunnel as well as the existing sound wall along Kimball Avenue, east of Watson Street (receivers J through M have an existing sound wall). Car wash noise levels at receiver H through M would be in the range of 46.0-48.3 dB.

The noise levels at the residential land uses located west of the project site would exceed applicable City of Visalia daytime (6:00 a.m. to 7:00 p.m.) and nighttime (7:00 p.m. to 6:00 a.m.) noise level standards. The noise levels at the residential land uses located south of the project site would exceed applicable City of Visalia nighttime (7:00 p.m. to 6:00 a.m.) noise level standard but would not exceed applicable daytime standard. Therefore, noise mitigation must be considered for the project for the residential land uses located along South West Street.

**Vacuum System:** The project will also include a self-vacuum kiosk utilizing a central vacuum system. According to the project applicant, the vacuum system would be a Eurovac dual 30-HP vacuum with individual vacuum hose ends at the vehicle stalls. According to noise data supplied by the applicant, the vacuum equipment itself would produce noise levels of 95 dB at a distance of 3 feet from the equipment, and the individual hose ends would produce noise levels of 65 dB at a distance of 3 feet from the hose ends. At the time this analysis was prepared the exact location of the vacuum equipment was unknown. It is common to locate the vacuum equipment

within an enclosure. Taking into account standard attenuation with increased distance from a point noise source, if the vacuum equipment was not enclosed or shielded it would need to be located at least 275 feet from a noise-sensitive receiver. The closest proposed vacuum hose (vacuum stall) would be located approximately 200 feet from the closest noise sensitive receivers (receivers D & E). Vacuum noise levels at this distance was calculated to be 28.5 dB. Such levels are below the City's noise standards and below existing ambient noise levels in the project area.

#### **Gas Station/Convenience Store:**

Noise sources associated with gas station and convenience store operations would typically consist of vehicle movements, alarm horns, doors closing, vehicles starting (ignition), vehicle stereos, tire inflation equipment, and mechanical equipment (air conditioners, trash compactors, etc.).

**Vehicle Movements:** Noise due to traffic in parking lots is typically limited by low speeds and is not usually considered to be significant. Human activity in parking lots that can produce noise includes voices, stereo systems, vehicle alarms and the opening and closing of car doors and trunk lids. Such activities can occur at any time. The noise levels associated with these activities cannot be precisely defined due to variables such as the number of parking movements, type of vehicles, and other factors. It is typical for a passing car in a parking lot to produce a maximum noise level of 60 to 65 dB at a distance of 50 feet, which is comparable to the level of a raised voice. For this project, the closest proposed on-site vehicle movements to off-site noise-sensitive receivers would occur at distances of 150 feet or greater. Noise levels associated with vehicle movements and parking lot activities would be expected to be below 50 dB at the closest off-site noise-sensitive receivers.

**Truck Movements:** File data for slowly moving heavy trucks indicate that the maximum noise level ( $L_{max}$ ) is approximately 73 dB at 50 feet. Truck movements that do not occur on a public roadway are considered to be a stationary noise source. Noise levels associated with on-site truck movements would be expected to be below 57 dB at the closest off-site noise-sensitive receivers.

Since noise levels associated with truck and vehicle movements would be periodic, and would occur at varying distances from the closest residential land uses, associated noise levels would not be expected to exceed any of the City's applicable noise level standards.

According to the project applicant, with the exception of fuel supply trucks, all deliveries would occur during daytime hours. According to the project applicant fuel delivery trucks do not idle while unloading fuel, nor does the fueling operations require any pump or noise-producing equipment. Noise levels associated with nighttime fuel deliveries would be comparable to those described above, and would be below existing ambient noise levels in the project vicinity.

**Trash Compactors:** Detailed information about air conditioners and trash compactors is not available at this time. It is not known if the project would include a trash compactor. However, based upon noise studies conducted by WJVA for other projects, the maximum noise level produced by a typical un-enclosed trash compactor (Hydra-Fab Model 1200) is approximately 74

dB at a distance of 10 feet from the equipment, or approximately 50 dB at a distance of 150 feet. As trash compactor noise is typically limited to short time periods, the noise levels associated with a trash compactor (if included in project design) would not be expected to exceed any City of Visalia noise level standards.

**Tire Inflation Equipment:** In order to determine noise levels associated with coin-operated tire inflation equipment, WJVA staff conducted reference noise measurements of the tire inflation equipment at the existing ARCO-AM/PM located on Lover's Lane in Visalia. The equipment measured was an AirServ Legacy II model. Maximum noise levels were measured to be approximately 75 dB at a distance of five (5) feet from the equipment. The proposed location of the tire infiltration equipment is along the east side of the project site, at a distance of 350 feet or greater from the closest noise-sensitive receivers. At this distance, noise levels associated with the tire inflation equipment would be approximately 37 dB. Such levels are below the City's applicable daytime and nighttime noise level standards and below existing ambient noise levels.

**HVAC Units:** It can be assumed that the project would include roof-mounted HVAC units. Based upon data from larger stores than proposed for the project, it is estimated that noise levels from roof-mounted HVAC units at the closest homes to the project site would be in the range of 40-45 dB. These noise levels take into consideration the screening provided by the roof parapet. These levels would not be audible above existing ambient noise levels at the nearby noise-sensitive receivers and they do not exceed the City noise level standards.

**Additional Measurements at Lover's Lane ARCO-AM/PM:** To further quantify noise levels that could be expected from typical gas station/convenience store operations, WJVA staff conducted reference noise measurements at an existing ARCO-AM/PM facility located on Lover's Lane in Visalia. The Lover's Lane locations is a similar facility to that being proposed for the Cameron Creek Marketplace project. In order to obtain project-related reference noise levels without interference from roadway traffic noise levels, WJVA staff conducted the noise measurements between 3:45 a.m. and 5:00 a.m.

Noise measurements were conducted at distance of approximately 90 feet from the closest gas pump and 40 feet from the closest parking space. The location of the reference noise measurement site is provided as Figure 6 (labeled Reference Noise Measurement Location). WJVA staff noted the number of vehicles entering the project site, and tallied them in 15-minute intervals. The number of observed vehicles (patrons) were as follows:

- 3:45-4:00 a.m. : 13
- 4:00-4:15 a.m. : 12
- 4:15-4:30 a.m. : 10
- 4:30-4:45 a.m. : 17
- 4:45-5:00 a.m. : 14

As described above, observed (and measured) sources of noise during the period of measurements were vehicle movements, vehicle starts (ignition), door/trunk closures, vehicle

alarm sets and vehicle stereos. As vehicles would enter the site and navigate to the pump or parking space of their choice, the distance between each activity and the noise monitor varied. In general, the vehicles tended to utilize the closest pumps to the gas station entrance, which corresponded to the closest spaces to the meter location.

Table VI provides the measured noise levels for each noise source type. Also provided in Table VI is the total number of events for each noise source, the average noise levels for all the events for each noise source as well as the range of noise levels. During each “event” WJVA staff observed the noise meter and noted the maximum ( $L_{max}$ ) noise level for the event. It should be noted, for many of the vehicles/patrons, the noise sources described in Table VI were not audible or measurable over other existing off-site noise sources, typically traffic noise associated with vehicles on Lover’s Lane.

<p style="text-align: center;"><b>TABLE VI</b> <b>SUMMARY OF PROJECT-RELATED NOISE LEVEL MEASUREMENTS</b> <b>ARCO-AM/PM, LOVER’S LANE</b> <b>JANUARY 18, 2018</b></p>			
Noise Source	Number of Measured Events	Average (Range), $L_{max}$ A-Weighted Decibels, dB	Noise Levels at Closest Noise-Sensitive Receiver
Vehicle Movements	20	54 (45-72)	46 (37-64)
Vehicle Starts (Ignition)	14	54 (47-71)	46 (39-63)
Door/Trunk Closure	16	49 (43-57)	41 (35-49)
Vehicle Alarm Set	4	53 (43-57)	45 (35-49)
Vehicle Stereo	2	59 (58-60)	51 (50-52)
Source: WJV Acoustics, Inc.			

Taking into account the standard rate of attenuation with increasing distance from a point source (-6 dB/doubling of distance), table VI also provides the measured reference noise levels at the distance of the closest existing backyard to the proposed project (approximately 150 feet). Reference to Table V and Table VI indicate that noise levels associated with gas station/convenience store activities would be expected to be below applicable nighttime noise level standards and below existing nighttime ambient (without project) noise levels in the project area.

Additionally, WJVA staff conducted continuous nighttime noise level measurements (between the hours of 8:00 p.m. and 6:00 a.m.) at the Lover’s Lane ARCO-AM/PM facility on the night of January 17, 2018. The noise meter was placed on a light post at a distance of approximately fifty (50) feet from the closest gas pump and adjacent to the closest parking space. The meter location was selected due to its proximity to the operations and provided a secure location for the monitoring equipment. The location of the monitoring site is provided as Figure 6 (labeled Nighttime Noise Measurement Location).



Noise levels measured at the Lover's Lane ARCO-AM/PM are provided as Table VII. The noise levels provided as "50'" represent those measured at the Nighttime Noise Monitoring Site, which was located approximately 50 feet from the closest gas pump and adjacent to the closest parking space.

The noise levels measured at the Lover's Lane ARCO-AM/PM were heavily influenced by existing traffic noise associated with vehicles on Lover's Lane. While it is unreasonable to assume that the noise levels described in Table VII (50') were entirely attributable to on-site sources, WJVA calculated what these noise levels would be at a distance of 150 feet (the approximate distance of the closest existing backyard to the proposed closest gas pump and parking space is 200 feet).

Table VII provides these noise levels at a distance of 150 feet (a conservative assessment of proposed setback distances from gas pumps/parking spaces to closest homes). Again, this assumes ALL noise measured at the Lover's Lane ARCO-AM/PM was attributable to on-site sources (which WJVA observed not be the case). Therefore, the noise levels described in Table VII at a distance of 150 feet should be considered a worst-case assessment of project-related nighttime noise levels at the closest homes to the proposed Cameron Creek Marketplace ARCO-AM/PM.

**TABLE VII**  
**SUMMARY OF NIGHTTIME NOISE LEVEL MEASUREMENTS**  
**LOVER'S LANE ARCO-AM/PM**  
**JANUARY 17, 2018**

Time	Statistical Category (Applicable Nighttime Standard) A-Weighted Decibels, dB											
	L <sub>eq</sub>		L <sub>50</sub>		L <sub>25</sub>		L <sub>5.5</sub>		L <sub>1.7</sub>		L <sub>max</sub>	
	50'	150'	50'	150'	50'	150'	50'	150'	50'	150'	50'	150'
8 pm	63.4	53.9	60.0	50.5	62.4	52.9	65.6	56.1	73.1	63.6	86.1	76.6
9 pm	62.3	52.8	58.9	49.4	61.5	52.0	64.3	54.8	69.5	60.0	87.5	78.0
10 pm	60.3	50.8	58.0	48.5	60.5	51.0	63.4	53.9	66.8	57.3	75.9	66.4
11 pm	58.3	48.8	52.8	43.3	57.1	47.6	62.1	52.6	68.2	58.7	78.9	69.4
12 pm	62.7	53.2	53.5	44.0	59.5	50.0	66.2	56.7	71.8	62.3	89.8	80.3
1 am	55.1	45.6	48.7	39.2	54.6	45.1	59.3	49.8	65.1	55.6	73.9	64.4
2 am	58.5	49.0	55.6	46.1	57.6	48.1	61.9	52.4	66.7	57.2	81.4	71.9
3 am	54.7	45.2	48.2	38.7	54.3	44.8	59.8	50.3	63.6	54.1	72.1	62.6
4 am	58.9	49.4	54.9	45.4	58.8	49.3	62.8	53.3	67.8	58.3	82.3	72.8
5 am	61.0	51.5	57.4	47.9	61.3	51.8	65.6	56.1	69.2	59.7	75.0	65.5
Average	59.5	50.0	54.8	45.3	58.8	49.3	63.1	53.6	68.2	58.7	80.3	70.8

Source: WJV Acoustics, Inc.

It should also be noted, as the homes along West Street have an existing 6.5-foot sound wall, noise levels would be expected to be approximately 5 dB less than those described in Table VII at the 150-foot setback distance.

Reference to Table V and Table VII indicate that the measured nighttime noise levels at the Lover's Lane ARCO-AM/PM (when adjusted for a setback distance of 150) are below existing ambient noise levels at the project site in all statistical nighttime noise level standard categories established by the City's noise ordinance as well as the City's General Plan. As stated above, the noise levels provided in Table VII take into account ALL noise sources in the area of the Lover's Lane ARCO-AM/PM (including traffic noise associated with vehicles on Lovers Lane), and should be considered a worst-case assessment of operational noise levels.

### CONSTRUCTION NOISE

Construction noise is not typically considered to be a significant impact if construction is limited to the daytime hours and construction equipment is adequately maintained and muffled. Extraordinary noise-producing activities (e.g., pile driving) are not anticipated. The City of Visalia Noise Ordinance limits hours of construction to be between 6:00 a.m. and 7:00 p.m., Monday through Friday and 9:00 a.m. to 7:00 p.m. on Saturdays and Sundays. Table VIII provides typical construction-related noise levels at reference distances of 25 feet, 50 feet, and 100 feet.

<p><b>TABLE VIII</b></p> <p><b>TYPICAL CONSTRUCTION EQUIPMENT</b></p> <p><b>MAXIMUM NOISE LEVELS, dBA</b></p>			
Type of Equipment	25 Ft.	50 Ft.	100 Ft.
Backhoe	84	78	72
Concrete Saw	96	90	84
Crane	87	81	75
Excavator	87	81	75
Front End Loader	85	79	73
Jackhammer	95	89	83
Paver	83	77	71
Pneumatic Tools	91	85	79
Dozer	88	82	76
Rollers	86	80	74
Trucks	92	86	80
Pumps	86	80	74
Scrapers	93	87	81
Portable Generators	86	80	74
Front Loader	92	86	80
Backhoe	92	86	80
Excavator	92	86	80
Grader	92	86	80
<p>Source: FHWA</p> <p><i>Noise Control for Buildings and Manufacturing Plants, Bolt, Beranek &amp; Newman, 1987</i></p>			



## NOISE MITIGATION

### Exterior Noise Mitigation:

Noise levels associated with the proposed gas station and convenience store operations are not expected to exceed the City's applicable daytime or nighttime noise level standards established in the Noise Ordinance as well as those established in the Noise Element of the General Plan. Additional mitigation is not required for this portion of the proposed project.

In regards to the proposed car wash operations, the City of Visalia Noise Element has established an hourly  $L_{eq}$  standard of 50 dB during the hours of 7:00 a.m. to 10:00 p.m. and the City's noise ordinance has established an hourly  $L_{50}$  standard of 50 dB during the hours of 6:00 a.m. to 7:00 p.m. During hours of peak operation, the blower noise would remain relatively constant, and as such, both standards are considered to be comparable.

As described above, project-related noise levels at the receiver locations C, D and E (west of the project site) would be expected to be exposed to hourly  $L_{eq}/L_{50}$  levels of 53.4, 54.7 and 54.5 dB, respectively (assuming the tunnel is oriented such that the entrance is facing toward West Street). These calculated noise level take into consideration the existing 6.5-foot sound wall located at the residences. In order to mitigate project-related noise levels to City of Visalia standards at nearby residential land uses, additional mitigation measures are required. Potential mitigation measures are described below.

### **Sound Wall (preferred mitigation measure):**

A sound wall could be constructed along the tunnel entrance, between entrance opening and the existing residential land uses west of the project site. The minimum required height of the sound wall along the car wash tunnel entrance was calculated using a sound wall insertion loss model. The model calculates the insertion loss of a wall of a given height based on the effective height of the noise source, height of the receiver, distance from the receiver to the wall, and distance from the noise source to the wall.

Based upon the above-described project-related noise levels and insertion loss model, it was determined that a sound wall with a minimum height of seven (7) feet above ground elevation would mitigate project related noise levels at receiver locations C, D and E to 48.6 dB, 49.8 dB and 49.7  $L_{eq}/L_{50}$ , respectively. The sound wall should be continuous from the south side of the tunnel entrance, and continue as the tunnel entrance lane turns to the north, as shown in red on Figure 1. The sound wall would reduce project-related noise levels to below applicable City of Visalia daytime standards, but would not reduce project-related noise levels to below the City's applicable nighttime standards.

## **Reconfigure Car Wash Tunnel:**

**180 Degree Rotation:** Noise levels associated with car wash blowers are louder at the exit end of the tunnel than the entrance end of the tunnel. If the tunnel was rotated 180 degrees from its currently proposed location, so that the exit is west-facing, noise levels associated with the car wash would be approximately 59 dB L<sub>50</sub> at the closest residential land uses west of the project site. Such levels would exceed the City's applicable noise level standards. Using the above-described sound wall insertion loss model, it was determined that a sound constructed to a minimum height of twelve (12) feet would be required at the tunnel exit, if the tunnel exit were to be located facing West Street. Figure 3 shows the location of the 12-foot sound wall (on a previous project site plan), if the tunnel exit were to be oriented with the exit facing west.

**90/270 Degree Rotation:** The tunnel could be rotated 90 or 270 degrees from its currently proposed alignment. Doing so would result in acoustic shielding (provided by the tunnel) at the existing residential land uses west of the project site, and would reduce project-related noise levels to below the City's standards at the homes along West Street. However, the residential land uses south of the site, along Kimball Avenue, would be exposed to unshielded project-related noise levels. Additionally, the homes along Kimball Avenue, between West Street and Watson Street, do not have an existing sound wall that would provide additional noise attenuation. If the tunnel were to be rotated 90 degrees from its currently proposed alignment (exit facing Caldwell Avenue) or 270 degrees (exit facing Kimball Avenue), project-related noise levels at the closest homes south of the project site would be expected to be approximately 56 dB (exit facing Caldwell Avenue) and 62 dB (exit facing Kimball Avenue), respectively. Such levels would also exceed the City's applicable noise level standards, and these configurations would also require the inclusion of a sound wall.

Using the above-described sound wall insertion loss model, it was determined that a sound wall constructed to a minimum height of ten (10) feet would be required along the tunnel entrance, if the tunnel was configured with the entrance facing south (toward Kimball Avenue) and the exit facing north (toward Caldwell Avenue). Additionally, the required height of a sound wall (20+ feet) necessary to mitigate project noise levels if the tunnel exit were to be facing south (toward Kimball Avenue), is not considered to be a feasible option.

## **Tunnel/Bay Door:**

The inclusion of a tunnel (bay) door at west side of the tunnel could be included into project design. WJVA staff analyzed noise level test data provided by Baywatch Car Wash Tunnel Door Solutions. Baywatch provides custom car wash doors constructed to provide acoustic attenuation in noise-sensitive environments (other similar products could be assessed). The door could be installed at the west side of the tunnel (facing the nearby residential land uses), and would close once the vehicle has entered the car wash tunnel. The door would remain closed until the dry cycle has completed and the vehicle has exited the tunnel.

According to test data provided by Baywatch, the inclusion of a polycarbonate door would reduce noise levels by approximately 11-14 dB at the nearby residential land uses. The noise level reduction provided by the door would reduce project-related noise levels at the nearby residential land uses to below the applicable City of Visalia daytime and nighttime noise level standards.

It should be noted, the inclusion of such a door into project design would reduce noise levels to below applicable noise level standards for all potential tunnel orientation scenarios.

**Elimination of Car Wash:**

If the project applicant chose to eliminate the car wash entirely from project design, noise levels associated with the remaining gas station and convenience store operations would not be expected to exceed the City's applicable noise level standards.

**Interior Noise Mitigation:**

The most restrictive City of Visalia interior noise level standard is an hourly noise level of 45 dB  $L_{eq}$ . With the proposed sound wall (or bay door) in place, residential land uses adjacent to the project site will need to be capable of providing a minimum outdoor-to-indoor noise level reduction (NLR) of approximately 5 dB ( $50-45=5$ ).

A specific analysis of interior noise levels was not performed. However, it may be assumed that residential construction methods complying with current building code requirements will reduce exterior noise levels by approximately 25 dB if windows and doors are closed and 10-15 dB if windows are open. This will be sufficient for compliance with the City's 45 dB  $L_{eq}$  interior standard.

## CONCLUSIONS AND RECOMMENDATIONS

The proposed Cameron Creek Marketplace Car Wash will comply with applicable City of Visalia exterior and interior noise level requirements provided that one of the following noise mitigation measures is included in the proposed project design and the car wash operations are limited to daytime hours.

1. If the proposed tunnel is constructed with the exit facing east (entrance facing West Street), a sound wall with a minimum height of seven (7) feet above ground level must be constructed along the tunnel entrance (as shown on Figure 1). Suitable construction materials include concrete blocks, masonry or stucco on both sides of a wood or steel stud wall.

OR

2. If the proposed tunnel is constructed with the exit facing west (toward West Street), a sound wall with a minimum height of twelve (12) feet above ground level must be constructed along the exit tunnel (as shown on Figure 3). Suitable construction materials include concrete blocks, masonry or stucco on both sides of a wood or steel stud wall.

OR

3. If the proposed tunnel is constructed with the exit facing north (toward Caldwell Avenue) a sound wall with a minimum height of ten (10) feet above ground level must be constructed along the tunnel entrance, (in the same relative location as shown on Figure 1). Suitable construction materials include concrete blocks, masonry or stucco on both sides of a wood or steel stud wall.

OR

4. The inclusion of a tunnel (bay) door at the tunnel side facing existing residential land uses could be included into project design. This would be sufficient with any tunnel orientation scenario.

OR

5. The project applicant could choose to eliminate the car wash entirely from project design, noise levels associated with the remaining gas station and store operations would not be expected to exceed the City's applicable noise level standards.

AND

6. If the project applicant chooses to incorporate one of the mitigation measures described above (options 1-3), car wash activities should not occur between the hours of 7:00 p.m. and 6:00 a.m., as the mitigation options would not reduce project-related noise levels to below the City's applicable nighttime noise level standards.
7. If the vacuum system equipment is not enclosed or shielded, it must be located at least 275 feet from the closest noise-sensitive receiver.

Additionally, in regards to the City's exterior noise level standard of 65 dB  $L_{dn}$ , established in the Noise Element of the General Plan, with any of the proposed mitigation options and assuming daytime operations only (6:00 a.m. to 7:00 p.m.), the project related noise level exposure would be approximately 50 dB  $L_{dn}$  at the closest residential land uses. These levels are below the City's 65 dB  $L_{dn}$  noise level standard established in the General Plan.

The conclusions and recommendations of this acoustical analysis are based upon the best information known to WJV Acoustics Inc. (WJVA) at the time the analysis was prepared concerning the proposed site plan, project equipment and proposed hours of operation. Any significant changes in these factors will require a reevaluation of the findings of this report. Additionally, any significant future changes in car wash equipment, noise regulations or other factors beyond WJVA's control may result in long-term noise results different from those described by this analysis.

Respectfully submitted,



Walter J. Van Groningen  
President

WJV:wjv

**FIGURE 1: PROJECT SITE PLAN AND SOUND WALL LOCATION**



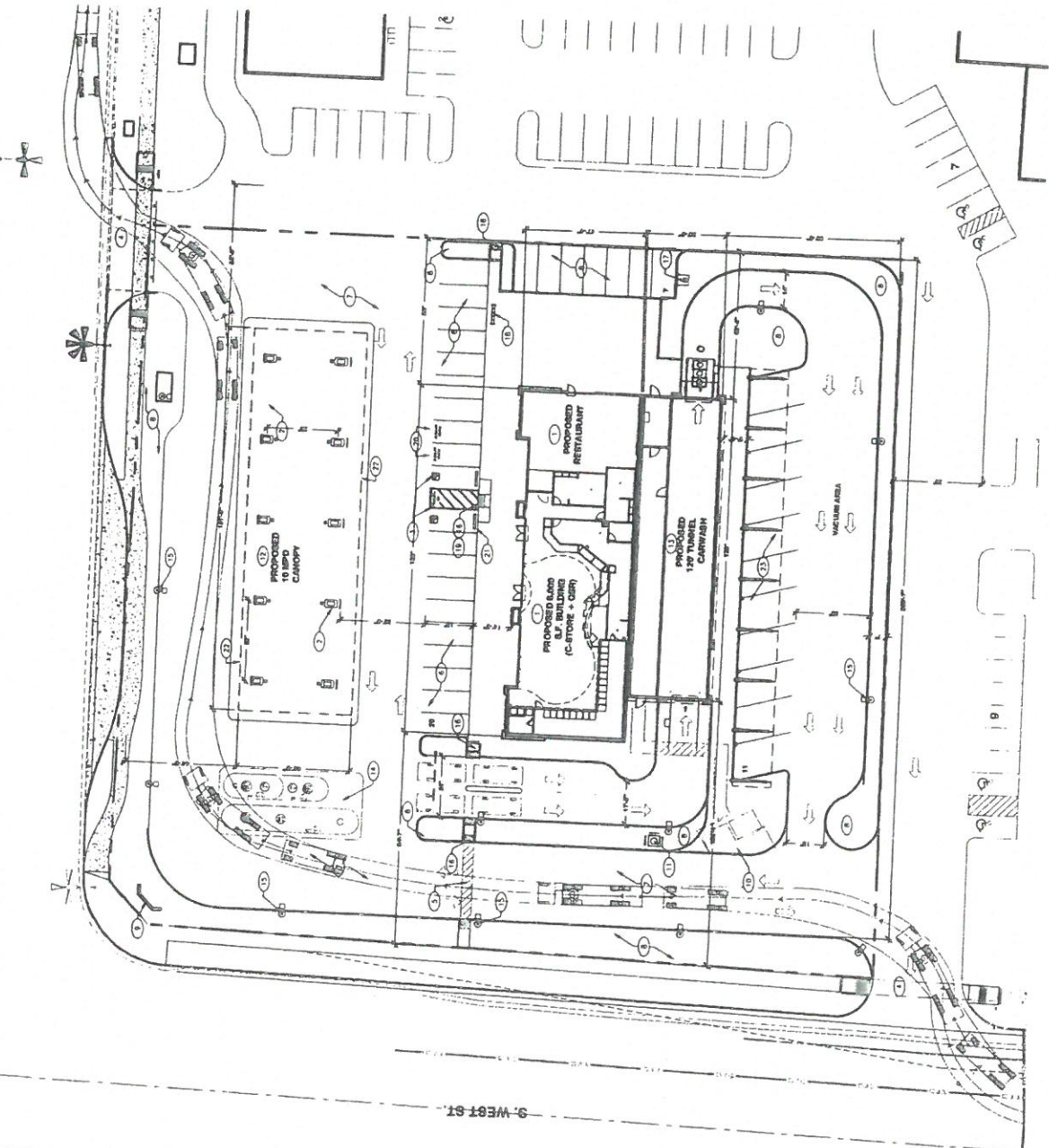
**APPLICANT INFORMATION**  
 AGC DESIGN CONCEPT, INC.  
 10000 CALIFORNIA BLVD.  
 SUITE 200, CA 91305  
 TEL: 909/441-1111  
 FAX: 909/441-1112

**SITE INFORMATION**

AREA OF PROPERTY	DESCRIPTION	AREA
EXISTING IMPROVEMENTS	EXISTING IMPROVEMENTS	13,217 SQ. FT.
PROPOSED IMPROVEMENTS	PROPOSED IMPROVEMENTS	13,217 SQ. FT.
TOTAL IMPROVEMENTS	TOTAL IMPROVEMENTS	26,434 SQ. FT.
EXISTING IMPROVEMENTS	EXISTING IMPROVEMENTS	13,217 SQ. FT.
PROPOSED IMPROVEMENTS	PROPOSED IMPROVEMENTS	13,217 SQ. FT.
TOTAL IMPROVEMENTS	TOTAL IMPROVEMENTS	26,434 SQ. FT.
EXISTING IMPROVEMENTS	EXISTING IMPROVEMENTS	13,217 SQ. FT.
PROPOSED IMPROVEMENTS	PROPOSED IMPROVEMENTS	13,217 SQ. FT.
TOTAL IMPROVEMENTS	TOTAL IMPROVEMENTS	26,434 SQ. FT.

**KEY PLAN**

- 1. NEW STONE AND CONCRETE (100% OF 1)
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**1. PRELIMINARY SITE PLAN**

AS.1

**NEW GAS STATION @ CAMERON CREEK**  
 S.E.C. CALDWELL AVENUE AND S. WEST ST. WISLA, CA  
 SITE PLAN

AGC DESIGN CONCEPT, INC.  
 10000 CALIFORNIA BLVD.  
 SUITE 200, CA 91305  
 TEL: 909/441-1111  
 FAX: 909/441-1112



**FIGURE 2: PROJECT LOCATION MAP**







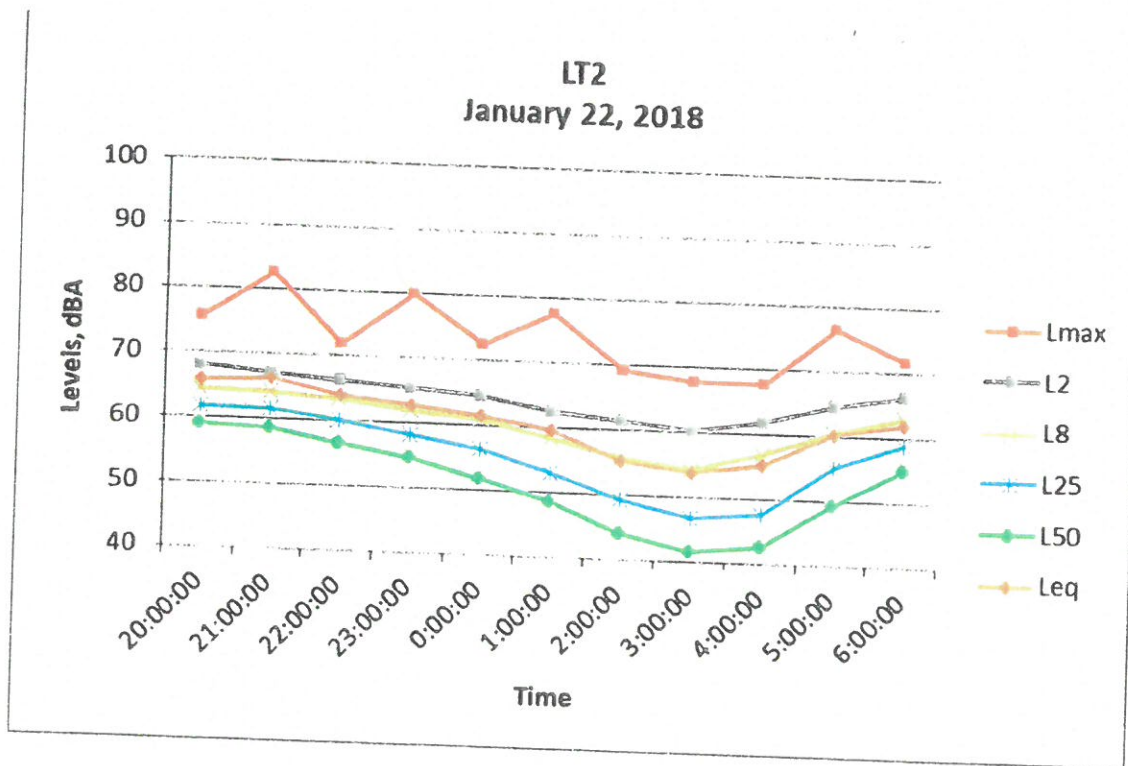
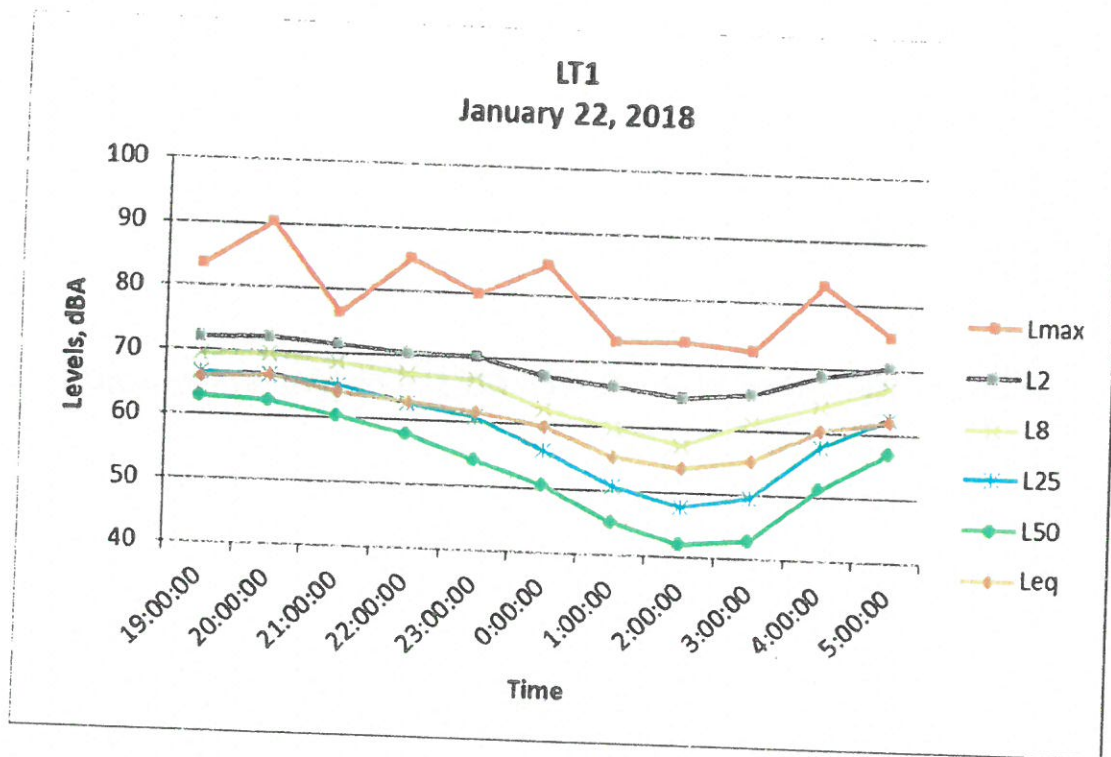
**FIGURE 3: AMBIENT NOISE MONITORING SITE LOCATIONS**







**FIGURE 4: NIGHTTIME AMBIENT NOISE LEVELS AT PROJECT SITE**



**FIGURE 5: LOCATIONS OF ANALYZED RECEIVERS (CAR WASH NOISE)**

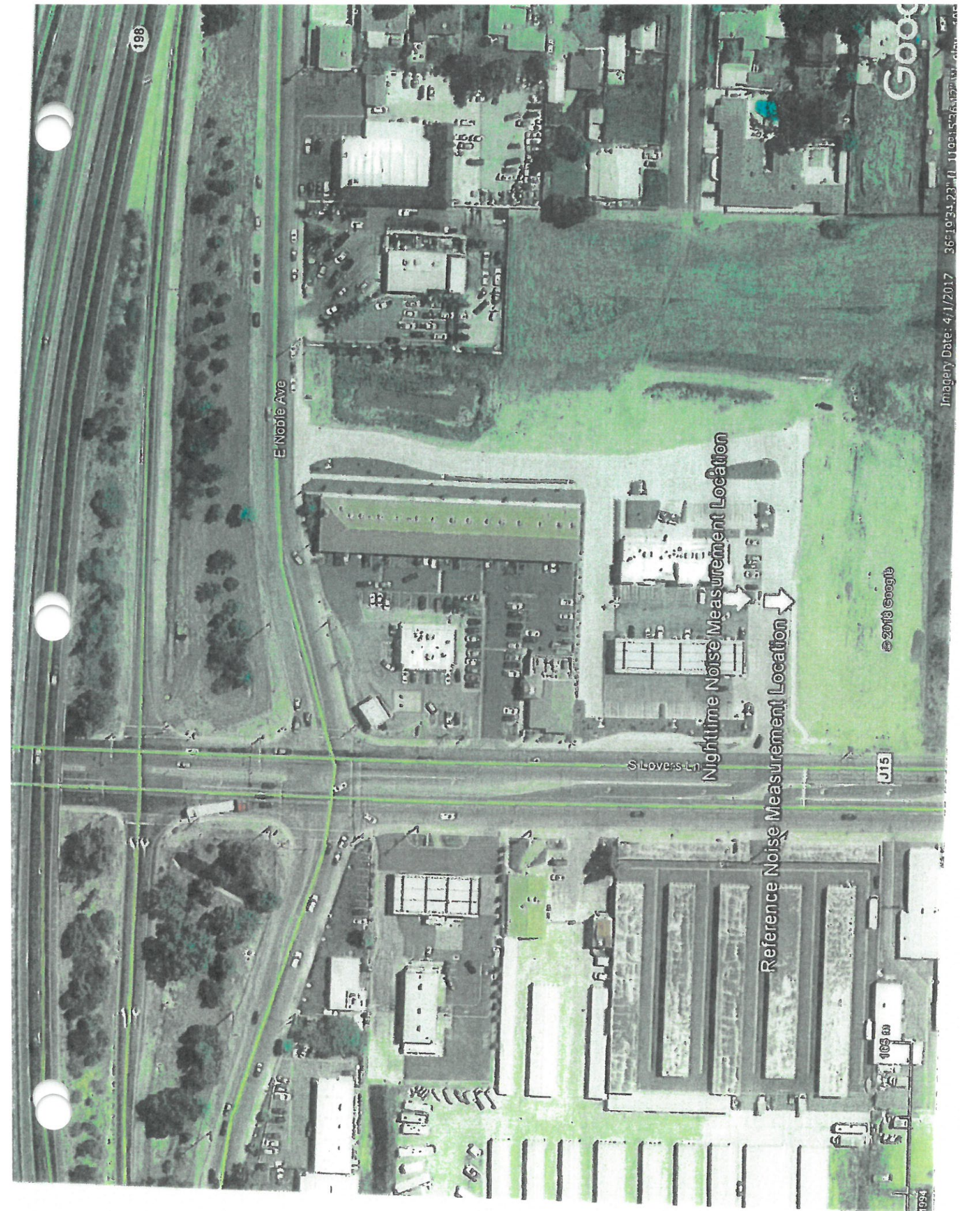






**FIGURE 6: REFERENCE NOISE MONITORING LOCATIONS  
LOVER'S LANE ARCO-AM/PM**





198

E Noble Ave

S Lovers Ln

Nighttime Noise Measurement Location

Reference Noise Measurement Location

© 2013 Google

105

1994

Imagery Date: 4/1/2017

36°10'34.23"N 110°15'26.19"W

Google



**FIGURE 7: CAR WASH NOISE LEVELS AT ANALYZED RECEIVER LOCATIONS  
WITH ENTRANCE SOUND WALL MITIGATION**







## APPENDIX A

### ACOUSTICAL TERMINOLOGY

<b>AMBIENT NOISE LEVEL:</b>	The composite of noise from all sources near and far. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location.
<b>CNEL:</b>	Community Noise Equivalent Level. The average equivalent sound level during a 24-hour day, obtained after addition of approximately five decibels to sound levels in the evening from 7:00 p.m. to 10:00 p.m. and ten decibels to sound levels in the night before 7:00 a.m. and after 10:00 p.m.
<b>DECIBEL, dB:</b>	A unit for describing the amplitude of sound, equal to 20 times the logarithm to the base 10 of the ratio of the pressure of the sound measured to the reference pressure, which is 20 micropascals (20 micronewtons per square meter).
<b>LDN/L<sub>dn</sub>:</b>	Day/Night Average Sound Level. The average equivalent sound level during a 24-hour day, obtained after addition of ten decibels to sound levels in the night after 10:00 p.m. and before 7:00 a.m.
<b>L<sub>eq</sub>:</b>	Equivalent Sound Level. The sound level containing the same total energy as a time varying signal over a given sample period. L <sub>eq</sub> is typically computed over 1, 8 and 24-hour sample periods.
<b>NOTE:</b>	The CNEL and LDN represent daily levels of noise exposure averaged on an annual basis, while L <sub>eq</sub> represents the average noise exposure for a shorter time period, typically one hour.
<b>L<sub>max</sub>:</b>	The maximum noise level recorded during a noise event.
<b>L<sub>n</sub>:</b>	The sound level exceeded "n" percent of the time during a sample interval (L <sub>90</sub> , L <sub>50</sub> , L <sub>10</sub> , etc.). For example, L <sub>10</sub> equals the level exceeded 10 percent of the time.



## A-2

### ACOUSTICAL TERMINOLOGY

#### **NOISE EXPOSURE CONTOURS:**

Lines drawn about a noise source indicating constant levels of noise exposure. CNEL and LDN contours are frequently utilized to describe community exposure to noise.

#### **NOISE LEVEL REDUCTION (NLR):**

The noise reduction between indoor and outdoor environments or between two rooms that is the numerical difference, in decibels, of the average sound pressure levels in those areas or rooms. A measurement of "noise level reduction" combines the effect of the transmission loss performance of the structure plus the effect of acoustic absorption present in the receiving room.

#### **SEL or SENEL:**

Sound Exposure Level or Single Event Noise Exposure Level. The level of noise accumulated during a single noise event, such as an aircraft overflight, with reference to a duration of one second. More specifically, it is the time-integrated A-weighted squared sound pressure for a stated time interval or event, based on a reference pressure of 20 micropascals and a reference duration of one second.

#### **SOUND LEVEL:**

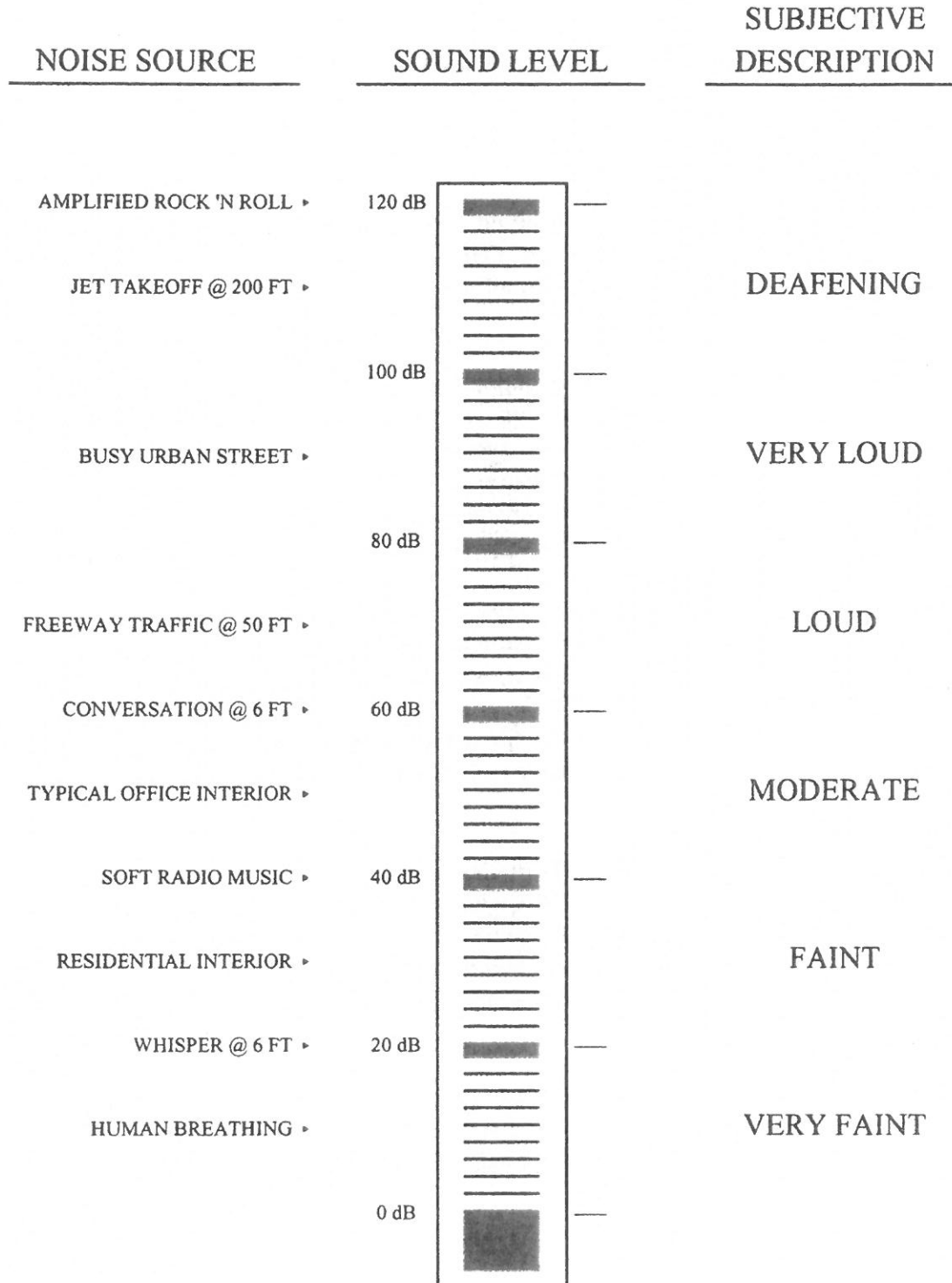
The sound pressure level in decibels as measured on a sound level meter using the A-weighting filter network. The A-weighting filter de-emphasizes the very low and very high frequency components of the sound in a manner similar to the response of the human ear and gives good correlation with subjective reactions to noise.

#### **SOUND TRANSMISSION CLASS (STC):**

The single-number rating of sound transmission loss for a construction element (window, door, etc.) over a frequency range where speech intelligibility largely occurs.

# APPENDIX B

## EXAMPLES OF SOUND LEVELS



**APPENDIX C**  
**AMBIENT NOISE MEASUREMENT FIELD NOTES**

17-007 (Cameron Creek Marketplace Car Wash, Visalia) 2-26-18

## NOISE LEVEL FIELD DATA SHEET

Project #: 17-007

Date: 10/25/17

Site Description: CCMP

Monitoring Equipment: LD820 SLM #1419 Calibrator: BK 4230 #1026382 Data by: WJV

Temperature:  $\approx 90^\circ$  Wind: 0.5 mph Sky: clear R.H.: 30% Notes: \_\_\_\_\_

[illegible]

Project #: 17-007

Date: 1/31/18

Site Description: CCMP

Monitoring Equipment: LD820SLM # 1419 Calibrator: BK 4230 # 1026382 Data by: WJV

Temperature: 63° Wind: 0.5 mph Sky: Overcast R.H.: 65% Notes: \_\_\_\_\_

[illegible]

## **Attachment 6**

Planning Commission Staff Report from the August 28, 2017  
public hearing including all correspondence received





# REPORT TO CITY OF VISALIA PLANNING COMMISSION

**HEARING DATE:** August 28, 2017

**PROJECT PLANNER:** Paul Bernal, Principal Planner  
Phone No.: (559) 713-4025

**SUBJECT:** **Conditional Use Permit No. 2017-22:** A request by ARCO AM/PM to construct a new ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers. The Cameron Creek ARCO AM/PM is part of the Cameron Creek Shopping Center and zoned C-MU (Commercial Mixed Use). The parcel for the proposed ARCO AM/PM gas station is located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

## STAFF RECOMMENDATION

Staff recommends approval of Conditional Use Permit No. 2017-22, as conditioned, based upon the findings and conditions in Resolution No. 2017-37. Staff's recommendation is based on the conclusion that the request is consistent with the Visalia General Plan and Zoning Ordinance.

## RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2017-22, as conditioned, based on the findings and conditions in Resolution No. 2017-37.

## PROJECT DESCRIPTION

The applicant has filed Conditional Use Permit No. 2017-22 requesting to develop a 1.29-acre parcel (i.e. Parcel 1 of Parcel Map 5141) within the Cameron Creek Marketplace Shopping Center. The development consist of an ARCO AM/PM gas station with a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers (see Exhibit "A"). The Cameron Creek ARCO AM/PM project site is zoned C-MU (Commercial Mixed Use) and located on the southeast corner of West Caldwell Avenue and South West Street.

The ARCO AM/PM gas station site will be developed with 26 parking spaces, excluding the 20 fueling station spaces located under the fueling island canopy. In addition, 11 vacuum parking stations will be provided south of the carwash building. The ARCO AM/PM gas station site will be accessible by two new drive approaches installed along both the Caldwell Avenue and West Street frontages. In addition, curb, gutter, sidewalk and park strip landscaping will be installed along the West Street frontage providing pedestrian access to the residential neighborhood to the south of the ARCO AM/PM gas station site. Additional on-site improvements include construction of the primary drive aisle which will extend to the east which will allow on-site vehicular access from the ARCO gas station site to the Jack and the Box site as depicted on the overall Cameron Creek Market Place Shopping Center (see Exhibit "B"). Requirement to extend the on-site private driveway to the east is included as Condition No. 9.

The Operational Statement (see Exhibit "C") indicates that the fuel islands and convenience store will operate 24 hours a day. The applicant has indicated their request to have the carwash operate between the hours of 6:00 a.m. to 7:00 p.m. There are no prohibitions to operating 24 hours a day as long as Community Noise Standards are maintained, as specified in Chapter 8.36 of the Visalia Municipal Code. Because of noise levels associated with the carwash and vacuum cleaners, Mitigation Measures have been identified that recommend the hours of operation for the carwash and vacuum bays be limited between 7:00 a.m. to 9:00 p.m.

The Cameron Creek Marketplace Shopping Center project is a phased master-planned development (see Exhibit "B") in the C-MU zone. The Cameron Creek Marketplace Shopping Center project also had an associated Tentative Parcel Map (TPM No. 2005-1) which subdivided the entire 15.55-acre site into eight parcels with shared access, parking, drainage and maintenance agreements. The ARCO AM/PM gas station and convenience store is located on Parcel 1 of the approved parcel map.

### **BACKGROUND INFORMATION**

General Plan Land Use Designation:	Commercial Mixed Use
Zoning:	C-MU (Commercial Mixed Use)
Surrounding Land Use and Zoning:	North: Caldwell Avenue / Divided four lane Arterial street South: C-MU – Vacant Parcel 3 of the Cameron Creek Marketplace Shopping Center East: C-MU – Vacant Parcel 2 of the Cameron Creek Marketplace Shopping Center West: West Street / Two lane Collector street
Environmental Review:	Mitigated Negative Declaration (MND) No. 2017-39
Special Districts:	N/A
Site Plan Review No:	2016-140

### **RELATED PLANS & POLICIES**

Please see attached summary of related plans and policies. The proposed project is consistent with applicable plans and policies.

### **RELATED PROJECTS**

**Conditional Use Permit (CUP) No. 2005-022, Variance No. 2005-08 and Parcel Map No. 2005-14**, were submitted for the development of the 15.55-acre Cameron Creek Shopping Center. The development consisted of a total building area of 106,132 square feet. The shopping center will consist of two large anchor pads and various retail buildings along the southern portion of the site. The remaining balance of the site includes the development of out-pads buildings with drive-thru lanes along the Caldwell Avenue street frontage. The variance established a sign program for the entire shopping center while the parcel map depicted the subdivision of the entire shopping center into eight parcels. The Planning Commission approved the CUP, Variance and Parcel Map on July 25, 2005.



**Conditional Use Permit No. 2008-06 and Variance No. 2008-02** was a request by the applicant to develop the Caldwell and Court Commercial Center (i.e., Cameron Creek Shopping Center) with a total building area consisting of 118,558 square feet. The commercial center consisted of commercial and office space, a pharmacy with a drive-thru and a neighborhood grocery. The Variance allows a sign program for a retail center that varies from the standard signage provisions of Design District A, specifically with regard to quantities of freestanding signs. The Planning Commission approved the CUP and Variance on March 10, 2008. A parcel map was not submitted as part of this project and the applicants never submitted building plans to initiate their land use entitlement. CUP No. 2008-06 and Variance No. 2008-02 expired two years after the Planning Commission approval this project.

**Conditional Use Permit No. 2015-18:** was a request by Cameron Creek Marketplace, LLC to amend Conditional Use Permit No. 2005-22, to allow the development of a 3,037 square foot Jack in the Box fast-food restaurant with a drive-thru lane at the southwest corner of West Caldwell Avenue and South Court Street. The Planning Commission approved the CUP on July 13, 2015.

### **Similar Project**

**Conditional Use Permit No. 2014-11:** was a request by the Chandi Group to develop a shopping center consisting of six commercial parcels to be developed in four phases with a total of approximately 106,400 square feet of building space, including an ARCO AM/PM gas station with a fast food with drive-thru window. The CUP was approved by the Planning Commission on May 27, 2014.

**Conditional Use Permit No. 2015-06:** was a request by the Chandi Group to construct a new ARCO fueling station with a 6,000 square foot convenience store with a drive-thru lane. The ARCO fueling station is proposed to be constructed within the Square at Plaza Drive master-planned development located on the southwest corner of North Plaza Drive and West Crowley Avenue. The CUP was approved by the Planning Commission on April 13, 2015.

## **PROJECT EVALUATION**

Staff recommends approval of the conditional use permit, as conditioned, based on the project's consistency with the Land Use Element Policies of the General Plan and the Zoning Ordinance.

### **Entitlement History – Cameron Creek Marketplace Shopping Center**

The 15.55-acre commercial zoned property was initially entitled with the approval of CUP No. 2005-22, Variance No. 2005-08 and Parcel Map No. 2005-14 for the Cameron Creek Marketplace Shopping Center. The Planning Commission approved these entitlements on July 25, 2005. The approval of these entitlements was subject to the expiration timelines for the Parcel Map. However, the California State Legislature passed numerous Assembly and Senate Bills automatically extending the expiration dates for parcel and subdivision maps. The automatic time extensions approved by the State Legislature have kept the 2005 land use entitlements for this shopping center active. A Final Parcel Map was recorded creating parcels subject to the 2005 approved tentative map and the underlying 2005 master site plan.

### **Land Use Compatibility**

The overall approval of the Cameron Creek Marketplace Shopping Center is consistent with the Land Use Policies of the Visalia General Plan. The 2005 master site plan depicted the development of several commercial pads with associated drive-thru lanes located along the Caldwell Avenue frontage.

Staff concludes the CUP for the ARCO AM/PM gas station and carwash is consistent in nature and character with the original 2005 approval. Caldwell Avenue is a major arterial roadway and the future development of this 15.55-acre site will provide a wide range of commercial services and shopping opportunities to the surrounding residential neighborhoods that are not currently being served with these types of services.

### **Noise Analysis for Carwash Drive-Thru**

The drive-thru carwash for the proposed ARCO AM/PM gas station site is attached to the south building wall of the proposed convenience store. The construction and operational noise for the ARCO AM/PM site has been addressed in the project's Initial Study.

A Noise Impact Analysis prepared for the proposed project (Cameron Creek Noise Impact Analysis, Kunzman Associates, Inc., April 27, 2017 attached to Mitigated Negative Declaration No. 2017-39) has concluded that an exterior noise level in excess of the daytime 65 dB DNL standard for noise-sensitive land uses, specified in the City's Noise Element, will occur during the construction of the ARCO AM/PM gas station site unless mitigated. Furthermore, in the event the measured ambient noise levels without the alleged offensive source in operations exceeds an applicable noise level standard in any category, the applicable standard or standards shall be adjusted so as to equal the ambient noise level. The noise level data presented in the Noise Impact Analysis identified that ambient noise levels generally exceed the performance standards identified in the Noise Ordinance. Conversely, interior noise levels, when adjusted for ambient noise levels, are below the applicable noise standards for interior noise.

To ensure that community noise standards are met for noise-sensitive land uses adjacent to the site, the Noise Analysis identifies Construction Practice Mitigation Measures that shall be followed. Therefore, the project site shall be developed and shall operate in substantial compliance with the Mitigation Measures 1.1 through 1.7. These Mitigation Measures are included as part of the Conditions of Project Approval for the CUP (see Conditions No. 6 and 7).

Staff finds that mitigation measures are consistent with the policies which encourage implementing measures to reduce the potential of noise-related impacts between residential and non-residential uses.

### **Signage for ARCO AM/PM**

The applicant has provided Exhibit "D" depicting the proposed signage for the ARCO AM/PM site. Exhibit "D" identifies the location and wall placement of each sign as well as the sign areas for wall/canopy signage and the monument sign. The thematic signage is consistent with signage proposed on other ARCO AM/PM gas stations developed in Visalia over the past three years.

Wall signage for both the convenience store and the canopy signage are within the allowable sign area for a commercial development. The monument sign height is within the allowable sign height for freestanding monument signs; however, the monument sign area exceeds the maximum 35-square feet per sign face. The monument sign, as depicted on Exhibit "D" identifies a sign face area of 46-square feet. Staff has included Condition No. 4 requiring the monument sign area be approved at 35-square feet and located 5-feet from property lines, which is consistent with Section 17.48.110.C.4 and 5 of the Sign Ordinance for freestanding monument signs.

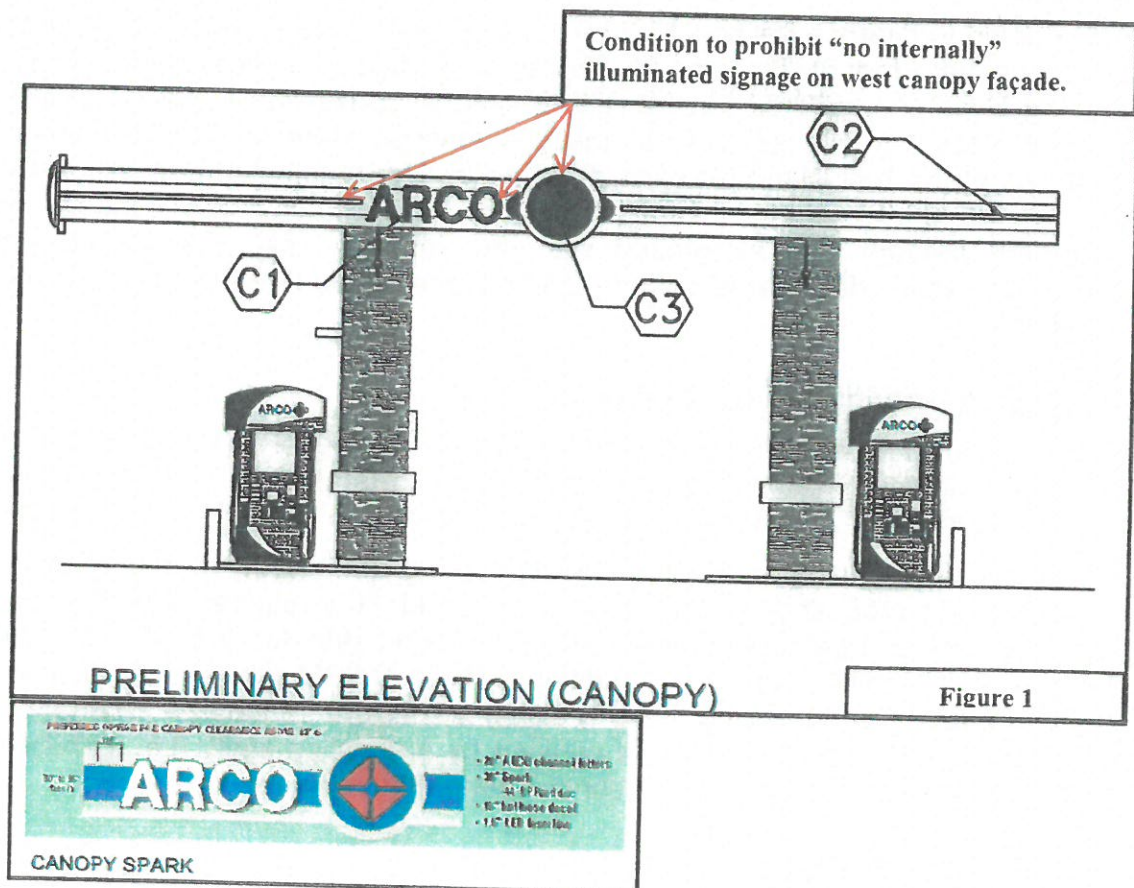


### Illuminated Signage on Gas Station Canopy – Impacts to Residential

The project site is adjacent to existing residential development to the north and west. The applicant has provided a lighting photometric diagram for the gas station site. Staff has determined that the photometric diagram demonstrates that there are a few instances where lighting exceeds the 0.5 lumens beyond the property lines of the commercial site.

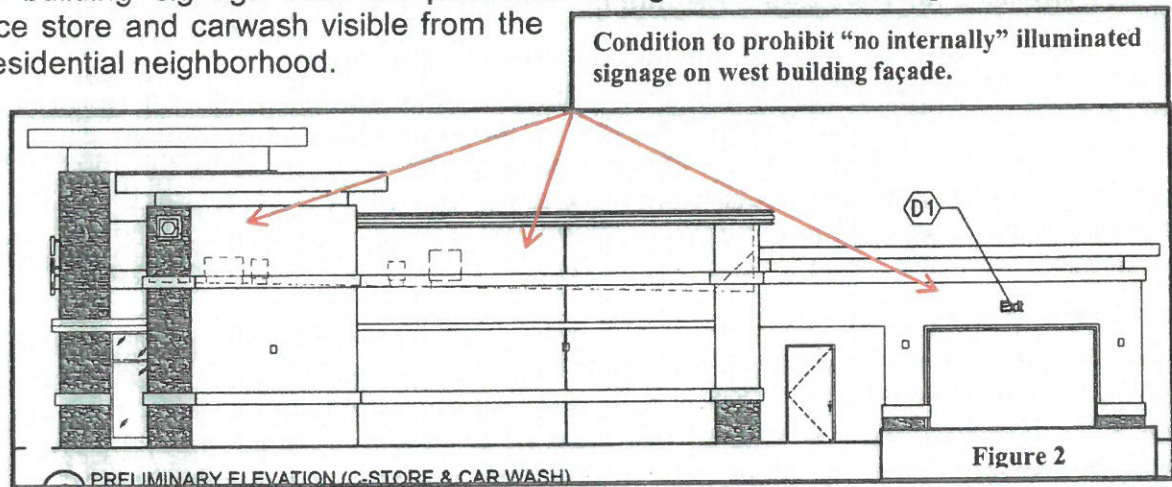
Staff recommends several conditions related to sight lighting and signage to reduce light impacts to residential properties to the north and west. Condition No. 11 requires that the fuel island canopy lighting be recessed into the canopy and shielded to prevent any significant light or glare from exceeding the 0.5 lumens beyond the property lines of the commercial site. Condition No. 12 also requires all yard lighting fixtures along West Street be 0.5 lumens or less at property lines and directed and/or shielded to not fall upon adjacent properties west of the project site.

In addition, Condition No. 13 requires that there shall be no internally illuminated signage on the west exterior fascia of the canopy including any type of light strips or LED/neon banding (see Figure 1). The elimination of canopy lighting along the west exterior of the canopy is due to the fact that the canopy signage is approximately 16 to 18 feet in height, and could be clearly visible from residences over the block wall west of West Street. The sign exhibit submitted and attached as Exhibit "D" currently shows use of a lighted decal/spark and band on the canopy elevations.





Condition No. 14 is also included to address illuminated building signage near the top of the west building façade for the ARCO AM/PM convenience store and carwash building that may be clearly visible from adjacent residential properties to the west. Condition No. 14 states that illuminated building signage shall be prohibited along the west building facades for the convenience store and carwash visible from the adjacent residential neighborhood.



### **Parking**

The Zoning Ordinance Section 17.34.020.F.14 establishes a parking requirement of one space per 300 square feet of floor area for convenience stores. Based on this parking requirement, 22 parking stalls are required for the ARCO AM/PM convenience store. Exhibit "A" identifies 26 stalls provided on-site adjacent to the convenience store. In addition, there are 20 parking positions at the fuel island gasoline dispensers. The automated carwash does not require any parking, though the proposed vacuum machine bays located south of the car wash each have a dedicated parking stall associated with the carwash use. The 11 vacuum stalls provide customers with a designated parking stall if those customers elect to clear the interior of their vehicles.

### **Building Elevations and Landscaping**

The applicant has provided elevations proposed for the ARCO AM/PM and gas canopy. The elevations depict stucco siding and stone veneer. Staff has included Condition No. 5 requiring that the building elevations be developed consistent with those provided in Exhibit "E".

Street tree landscaping within the park strips along Caldwell Avenue and West Street within the boundaries of proposed development (i.e., Parcel 1) is required with this project (see Exhibit "B"). The street tree requirement within the park strip for the ARCO AM/PM frontages is included as Condition No. 15. In addition, ARCO AM/PM will develop their onsite-landscaping planters subject to complying the City's Water Efficient Landscape Ordinance. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State and City's water conservation requirements.

### **On-Site Improvements**

Caldwell Avenue along the project site is improved to the ultimate width. Curb, gutter, sidewalk and the bus turn-out are installed along the entire Caldwell Avenue frontage. Two drive approaches, on Caldwell Avenue and West Street, are proposed with this development as depicted on Exhibit "A". Additional on-site improvements include construction of the primary drive aisle, which will extend to the east promoting on-site vehicular access from the ARCO AM/PM gas station site to the Jack and the Box site as depicted on the overall Cameron Creek



Market Place Shopping Center Exhibit "B". Requirement to extend the on-site private driveway to the east is included as Condition No. 9.

The project will also be required to install frontage improvements along the entire West Street frontage (i.e., ARCO site and vacant Parcel 3 to the south). The frontage improvements consisting of curb, gutter and sidewalks will complete the sidewalk pattern south to Kimball Avenue thereby establishing the sidewalk connection to the neighborhood to the south. The construction of curb, gutter and sidewalk along the West Street frontage as depicted on Exhibit "A" is included as Condition No. 10 for CUP No. 2017-22.

### **Environmental Review**

An Initial Study was prepared for this project, consistent with the California Environmental Quality Act (CEQA). The Initial Study disclosed that a significant, adverse environmental impact related to construction noise may occur. The Mitigated Negative Declaration (MND) circulated for this project (see attachment) contains Construction Mitigation Measures incorporated into the project based upon a noise study. The mitigation contained in the project shall effectively reduce the environmental impact of noise to a level that is less than significant while the project site is under construction and in operation. Therefore, staff recommends that Mitigated Negative Declaration No. 2017-39 and the Mitigation Monitoring Program be adopted for this project.

On August 21, 2017, the City received correspondence from a resident to the west of the project site regarding the adequacy of the MND (Please see Exhibit G). The author Cited 19 reasons why the MND and the project should not be approved. Staff has analyzed each issue raised in the letter. In staff's analysis, the MND adequately addresses the issues raised in the letter that are directly related to the CEQA statutes. The remaining issues are project-specific considerations, not directly related to CEQA. Consequently, the MND, including the recommended mitigation measures, can be adopted.

### **RECOMMENDED FINDINGS**

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
  - a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.
  - b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.
3. An Initial Study was prepared for this project, consistent with the California Environmental Quality Act (CEQA). The Initial Study disclosed that a significant, adverse environmental impact related to construction noise may occur. The Mitigated Negative Declaration circulated for this project (see attachment) contains Construction Mitigation Measures incorporated into the project based upon a noise study. The mitigation contained in the project shall effectively reduce the environmental impact of noise to a level that is less than significant while the project site is under construction and in operation. Therefore, staff

recommends that Mitigated Negative Declaration No. 2017-39 and the Mitigation Monitoring Program be adopted for this project.

## RECOMMENDED CONDITIONS OF APPROVAL

### **Conditional Use Permit No. 2017-22**

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2016-140 incorporated herein by reference.
2. That the project be developed in substantial compliance with the site plans in Exhibits "A" & "B".
3. That the applicant complies with their operational statement, as stated in Exhibit "C". Any changes to their operation are subject to review by the City Planner, and may subsequently be required to be reviewed by the Planning Commission.
4. That the ARCO AM/PM sign program comply with the sign areas as depicted on Exhibit "D" subject to the freestanding monument sign area be revised to comply with the 35-square foot sign area as required pursuant to Section 17.48.110.C.4 of the Sign Ordinance
5. That the ARCO AM/PM gas station, convenience store and carwash be developed in substantial compliance with the elevations shown in Exhibit "E".
6. That the mitigation measures found within the Mitigation Monitoring Plan for Mitigated That the mitigation measures found within the Mitigation Monitoring Plan for Mitigated Negative Declaration No. 2017-39 are hereby incorporated as conditions of Conditional Use Permit No. 2017-22 as follows:

<b><u>Mitigation Measure</u></b>	<b><u>Responsible Party</u></b>	<b><u>Timeline</u></b>
<b>Construction Noise Impact Mitigation Measure 1.1:</b> During all project site excavation and grading on-site, construction contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturer standards.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.2:</b> The contractor shall place all stationary construction equipment so that emitted noise is directed away from the noise sensitive receptors nearest the project site.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.3:</b> Equipment shall be shut off and not left to idle when not in use.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.4:</b> The contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and sensitive receptors nearest the project site during all project construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.5:</b> The project proponent shall mandate that the construction contractor prohibit the use of music or sound amplification on the project site during construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.

<b>Construction Noise Impact Mitigation Measure 1.6:</b> The construction contractor shall limit haul truck deliveries to the same hours specified for construction equipment.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Operation Noise Impact Mitigation Measure 1.7:</b> The car wash shall be limited to the hours of 7:00 a.m. to 9:00 pm Monday thru Sunday to eliminate early morning and late night and noise impacts.	Planning Division	Noise Impact Mitigation shall be enforced as a part of the daily operation for the site as a condition of the Conditional Use Permit which provides remedies for non-compliance.

7. That the hours of operation for the car wash and vacuum cleaners shall be between the hours of 7:00 a.m. to 9:00 p.m. and that Community Noise Standards be maintained during these times.
8. That any outdoor public address system (PA system) be prohibited on the gas station. Only personal intercom systems are allowed with speakers in close proximity to the pump for communication between the customer and cashier/attendant.
9. That the ARCO AM/PM gas station, convenience store and carwash construct their portion of the centrally located primary drive aisle for the Cameron Creek Marketplace Shopping Center as depicted on Exhibit "B".
10. That the construction of curb, gutter and sidewalk along the South West Street frontage from Caldwell Avenue south to Kimball Avenue be installed with the development of the ARCO AM/PM site. A building permit final will not be issued for the ARCO AM/PM site until the curb, gutter and sidewalk has been constructed and approved by the City's Engineering Division.
11. That the fuel island canopy lighting be recessed into the canopy and shielded to prevent any significant light or glare from falling upon the adjacent residential properties.
12. That all yard lighting fixtures along West Street be brought into compliance with 0.5 lumens or less at property lines. These light fixtures may require that shields be installed to comply with the 0.5 lumens at the property line.
13. That there be no internally illuminated signage on the west exterior fascia of the fuel island canopy including any type of light strips or neon / light-emitting diode (LED) banding.
14. That illuminated building signage on the west building façade of the AM/PM convenience store and carwash building is prohibited.
15. That the park strip landscaping, within the boundaries of the ARCO AM/PM project site, be installed with this development.
16. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. Landscape and irrigation plans shall comply with the State Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
17. That all signs shall require a separate building permit.
18. That all other federal, state and city codes, ordinances and laws be met.
19. That all of the conditions and responsibilities of Conditional Use Permit No. 2017-22 and the subsequent conditions adopted with the approval of Conditional Use Permit No. 2005-22 and Parcel Map No. 2005-14 shall run with the land and subsequent owners/operators shall also be subject to all of the conditions herein, unless amended or revoked.



## APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal with applicable fees shall be in writing and shall be filed with the City Clerk at 220 N. Santa Fe Street, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website [www.ci.visalia.ca.us](http://www.ci.visalia.ca.us) or from the City Clerk.

### Attachments:

- Related Plans & Policies
- Resolution No. 2017-37 (Conditional Use Permit No. 2017-22)
- Exhibit "A" – Site Plan
- Exhibit "B" – Master Site Plan for Cameron Creek Marketplace Shopping Center
- Exhibit "C" – Operational Statement
- Exhibit "D" – Proposed Signage for ARCO AM/PM
- Exhibit "E" – Elevations
- Exhibit "F" – AM/PM Floor Plan
- Exhibit "G" – Correspondence Received, dated August 20, 2017
- Mitigated Negative Declaration No. 2017-39 w/ Noise Impact Analysis, April 27, 2017
- Site Plan Review Comments 2016-140
- General Plan Land Use Map
- Zoning Map
- Aerial Map
- Vicinity Map

## **Related Plans & Policies**

### **Conditional Use Permits**

#### **17.38.010 Purposes and powers**

In certain zones conditional uses are permitted subject to the granting of a conditional use permit. Because of their unusual characteristics, conditional uses require special consideration so that they may be located properly with respect to the objectives of the zoning ordinance and with respect to their effects on surrounding properties. In order to achieve these purposes and thus give the zone use regulations the flexibility necessary to achieve the objectives of this title, the planning commission is empowered to grant or deny applications for conditional use permits and to impose reasonable conditions upon the granting of such permits. (Prior code § 7525)

#### **17.38.020 Application procedures**

- A. Application for a conditional use permit shall be made to the planning commission on a form prescribed by the commission which shall include the following data:
  - 1. Name and address of the applicant;
  - 2. Statement that the applicant is the owner of the property or is the authorized agent of the owner;
  - 3. Address and legal description of the property;
  - 4. The application shall be accompanied by such sketches or drawings as may be necessary by the planning division to clearly show the applicant's proposal;
  - 5. The purposes of the conditional use permit and the general description of the use proposed;
  - 6. Additional information as required by the historic preservation advisory committee.
- B. The application shall be accompanied by a fee set by resolution of the city council sufficient to cover the cost of handling the application. (Prior code § 7526)

#### **17.38.030 Lapse of conditional use permit**

A conditional use permit shall lapse and shall become void twenty-four (24) months after the date on which it became effective, unless the conditions of the permit allowed a shorter or greater time limit, or unless prior to the expiration of twenty-four (24) months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site which was the subject of the permit. A permit may be renewed for an additional period of one year; provided, that prior to the expiration of twenty-four (24) months from the date the permit originally became effective, an application for renewal is filed with the planning commission. The commission may grant or deny an application for renewal of a conditional use permit. In the case of a planned residential development, the recording of a final map and improvements thereto shall be deemed the same as a building permit in relation to this section. (Ord. 2001-13 § 4 (part), 2001: prior code § 7527)

#### **17.38.040 Revocation**

Upon violation of any applicable provision of this title, or, if granted subject to a condition or conditions, upon failure to comply with the condition or conditions, a conditional use permit shall be suspended automatically. The planning commission shall hold a public hearing within sixty (60) days, in accordance with the procedure prescribed in Section 17.38.080, and if not satisfied that the regulation, general provision or condition is being complied with, may revoke the permit or take such action as may be necessary to insure compliance with the regulation, general provision or condition. Appeals of the decision of the planning commission may be made to the city council as provided in Section 17.38.120. (Prior code § 7528)



#### **17.38.050 New application**

Following the denial of a conditional use permit application or the revocation of a conditional use permit, no application for a conditional use permit for the same or substantially the same conditional use on the same or substantially the same site shall be filed within one year from the date of denial or revocation of the permit unless such denial was a denial without prejudice by the planning commission or city council. (Prior code § 7530)

#### **17.38.060 Conditional use permit to run with the land**

A conditional use permit granted pursuant to the provisions of this chapter shall run with the land and shall continue to be valid upon a change of ownership of the site or structure which was the subject of the permit application subject to the provisions of Section 17.38.065. (Prior code § 7531)

#### **17.38.065 Abandonment of conditional use permit**

If the use for which a conditional use permit was approved is discontinued for a period of one hundred eighty (180) days, the use shall be considered abandoned and any future use of the site as a conditional use will require the approval of a new conditional use permit.

#### **17.38.070 Temporary uses or structures**

- A. Conditional use permits for temporary uses or structures may be processed as administrative matters by the city planner and/or planning division staff. However, the city planner may, at his/her discretion, refer such application to the planning commission for consideration.
- B. The city planner and/or planning division staff is authorized to review applications and to issue such temporary permits, subject to the following conditions:
  1. Conditional use permits granted pursuant to this section shall be for a fixed period not to exceed thirty (30) days for each temporary use not occupying a structure, including promotional enterprises, or six months for all other uses or structures.
  2. Ingress and egress shall be limited to that designated by the planning division. Appropriate directional signing, barricades, fences or landscaping shall be provided where required. A security officer may be required for promotional events.
  3. Off-street parking facilities shall be provided on the site of each temporary use as prescribed in Section 17.34.020.
  4. Upon termination of the temporary permit, or abandonment of the site, the applicant shall remove all materials and equipment and restore the premises to their original condition.
  5. Opening and closing times for promotional enterprises shall coincide with the hours of operation of the sponsoring commercial establishment. Reasonable time limits for other uses may be set by the city planner and planning division staff.
  6. Applicants for a temporary conditional use permit shall have all applicable licenses and permits prior to issuance of a conditional use permit.
  7. Signing for temporary uses shall be subject to the approval of the city planner.
  8. Notwithstanding underlying zoning, temporary conditional use permits may be granted for fruit and vegetable stands on properties primarily within undeveloped agricultural areas. In reviewing applications for such stands, issues of traffic safety and land use compatibility shall be evaluated and mitigation measures and conditions may be imposed to ensure that the stands are built and are operated consistent with appropriate construction standards, vehicular access and off-street parking. All fruits and vegetables sold at such stands shall be grown by the owner/operator or purchased by said party directly from a grower/farmer.
- C. The applicant may appeal an administrative decision to the planning commission. (Ord. 9605 § 30 (part), 1996: prior code § 7532)

#### **17.38.080 Public hearing--Notice**

- A. The planning commission shall hold at least one public hearing on each application for a conditional use permit.
- B. Notice of the public hearing shall be given not less than ten days nor more than thirty (30) days prior to the date of the hearing by mailing a notice of the time and place of the hearing to property owners within three hundred (300) feet of the boundaries of the area occupied or to be occupied by the use which is the subject of the hearing, and by publication in a newspaper of general circulation within the city. (Prior code § 7533)

#### **17.38.090 Investigation and report**

The planning staff shall make an investigation of the application and shall prepare a report thereon which shall be submitted to the planning commission. (Prior code § 7534)

#### **17.38.100 Public hearing--Procedure**

At the public hearing the planning commission shall review the application and the statement and drawing submitted therewith and shall receive pertinent evidence concerning the proposed use and the proposed conditions under which it would be operated or maintained, particularly with respect to the findings prescribed in Section 17.38.110. The planning commission may continue a public hearing from time to time as it deems necessary. (Prior code § 7535)

#### **17.38.110 Action by planning commission**

- A. The planning commission may grant an application for a conditional use permit as requested or in modified form, if, on the basis of the application and the evidence submitted, the commission makes the following findings:
  1. That the proposed location of the conditional use is in accordance with the objectives of the zoning ordinance and the purposes of the zone in which the site is located;
  2. That the proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity.
- B. A conditional use permit may be revocable, may be granted for a limited time period, or may be granted subject to such conditions as the commission may prescribe. The commission may grant conditional approval for a permit subject to the effective date of a change of zone or other ordinance amendment.
- C. The commission may deny an application for a conditional use permit. (Prior code § 7536)

#### **17.38.120 Appeal to city council**

The decision of the City planning commission on a conditional use permit shall be subject to the appeal provisions of Section 17.02.145. (Prior code § 7537) (Ord. 2006-18 § 6, 2007)

#### **17.38.130 Effective date of conditional use permit**

A conditional use permit shall become effective immediately when granted or affirmed by the council, or upon the sixth working day following the granting of the conditional use permit by the planning commission if no appeal has been filed. (Prior code § 7539)

RESOLUTION NO. 2017-37

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2017-22, A REQUEST BY ARCO AM/PM TO CONSTRUCT A NEW ARCO AM/PM GAS STATION CONSISTING OF A 6,500 SQUARE FOOT CONVENIENCE STORE BUILDING, A 3,850 SQUARE FOOT AUTOMATED CARWASH BUILDING, AND A 6,800 SQUARE FOOT FUEL CANOPY WITH 10 FUEL DISPENSERS. THE CAMERON CREEK ARCO AM/PM IS PART OF THE CAMERON CREEK SHOPPING CENTER AND ZONED C-MU (COMMERCIAL MIXED USE). THE PARCEL FOR THE PROPOSED ARCO AM/PM GAS STATION IS LOCATED ON THE SOUTHEAST CORNER OF WEST CALDWELL AVENUE AND SOUTH WEST STREET (APN: 126-870-037)

**WHEREAS**, Conditional Use Permit No. 2017-22, is a request by ARCO AM/PM to construct a new ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers. The Cameron Creek ARCO AM/PM is part of the Cameron Creek Shopping Center and zoned C-MU (Commercial Mixed Use). The parcel for the proposed ARCO AM/PM gas station is located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037); and

**WHEREAS**, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on August 28, 2017; and

**WHEREAS**, the Planning Commission of the City of Visalia finds the Conditional Use Permit No. 2017-22, as conditioned by staff, to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

**WHEREAS**, an Initial Study was prepared, and adopted which disclosed that no significant environmental impacts would result from this project, and mitigation measures would be required.

**NOW, THEREFORE, BE IT RESOLVED**, that Mitigated Negative Declaration No. 2017-39 was prepared consistent with the California Environmental Quality Act and City of Visalia Environmental Guidelines.

**NOW, THEREFORE, BE IT FURTHER RESOLVED** that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:

- a. The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The overall approval of the Cameron Creek Marketplace Shopping Center is consistent with the Land Use Policies of the Visalia General Plan. The 2005 master site plan depicted the development of several commercial pads with associated drive-thru lanes located along the Caldwell Avenue frontage. Staff concludes the ARCO AM/PM gas station and carwash is consistent in nature and character with the original 2005 approval. Caldwell Avenue is a major arterial roadway and the future development of this 15.55-acre site will provide a wide range of commercial services and shopping opportunities to the surrounding residential neighborhoods that are not currently being served with these types of services. With the development and installation of street improvements, the catalyst for the remaining balance of the site to be developed could be initiated by this new construction.
  - b. The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The General Plan Land Use Diagram, adopted October 14, 2014, designates the 1.29-acre project area as Commercial Mixed Use. The Zoning Map, adopted on April 6, 2017, designates the site as Commercial Mixed Use (C-MU), which is consistent with the General Plan Land Use Designation of Commercial Mixed Use as identified in Table 9-1 "Consistency Between the Plan and Zoning" of the General Plan. Gas stations with drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Caldwell Avenue, which is a designated arterial roadway. Staff concludes that the proposed gas station/drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this project.
3. An Initial Study was prepared for this project, consistent with the California Environmental Quality Act (CEQA). The Initial Study disclosed that a significant, adverse environmental impact related to construction noise may occur. The Mitigated Negative Declaration circulated for this project (see attachment) contains Construction and Carwash operating hour Mitigation Measures incorporated into the project based upon a noise study. The mitigation measures contained in the project shall effectively reduce the environmental impact of noise to a level that is less than significant while the project site is under construction and in operation. Therefore, staff recommends that Mitigated Negative Declaration No. 2017-39 and the Mitigation Monitoring Program be adopted for this project.

**BE IT FURTHER RESOLVED** that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2017-1406 incorporated herein by reference.
2. That the project be developed in substantial compliance with the site plans in Exhibits "A" & "B".



3. That the applicant complies with their operational statement, as stated in Exhibit "C". Any changes to their operation are subject to review by the City Planner, and may subsequently be required to be reviewed by the Planning Commission.
4. That the ARCO AM/PM sign program comply with the sign areas as depicted on Exhibit "D" subject to the freestanding monument sign area be revised to comply with the 35-square foot sign area as required pursuant to Section 17.48.110.C.4 of the Sign Ordinance
5. That the ARCO AM/PM gas station, convenience store and carwash be developed in substantial compliance with the elevations shown in Exhibit "E".
6. That the mitigation measures found within the Mitigation Monitoring Plan for Mitigated That the mitigation measures found within the Mitigation Monitoring Plan for Mitigated Negative Declaration No. 2017-39 are hereby incorporated as conditions of Conditional Use Permit No. 2017-22 as follows:

<b>Mitigation Measure</b>	<b>Responsible Party</b>	<b>Timeline</b>
<b>Construction Noise Impact Mitigation Measure 1.1:</b> During all project site excavation and grading on-site, construction contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturer standards.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.2:</b> The contractor shall place all stationary construction equipment so that emitted noise is directed away from the noise sensitive receptors nearest the project site.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.3:</b> Equipment shall be shut off and not left to idle when not in use.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.4:</b> The contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and sensitive receptors nearest the project site during all project construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.5:</b> The project proponent shall mandate that the construction contractor prohibit the use of music or sound amplification on the project site during construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.6:</b> The construction contractor shall limit haul truck deliveries to the same hours specified for construction equipment.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.

Resolution No. 2017-37



<b>Operation Noise Impact Mitigation Measure</b> <b>1.7:</b> The car wash shall be limited to the hours of 7:00 a.m. to 9:00 pm Monday thru Sunday to eliminate early morning and late night and noise impacts.	Planning Division	Noise Impact Mitigation shall be enforced as a part of the daily operation for the site as a condition of the Conditional Use Permit which provides remedies for non-compliance.
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7. That the hours of operation for the car wash and vacuum cleaners shall be between the hours of 7:00 a.m. to 9:00 p.m. and that Community Noise Standards be maintained during these times.
8. That any outdoor public address system (PA system) be prohibited on the gas station. Only personal intercom systems are allowed with speakers in close proximity to the pump for communication between the customer and cashier/attendant.
9. That the ARCO AM/PM gas station, convenience store and carwash construct their portion of the centrally located primary drive aisle for the Cameron Creek Marketplace Shopping Center as depicted on Exhibit "B".
10. That the construction of curb, gutter and sidewalk along the South West Street frontage from Caldwell Avenue south to Kimball Avenue be installed with the development of the ARCO AM/PM site. A building permit final will not be issued for the ARCO AM/PM site until the curb, gutter and sidewalk has been constructed and approved by the City's Engineering Division.
11. That the fuel island canopy lighting be recessed into the canopy and shielded to prevent any significant light or glare from falling upon the adjacent residential properties.
12. That all yard lighting fixtures along West Street be brought into compliance with 0.5 lumens or less at property lines. These light fixtures may require that shields be installed to comply with the 0.5 lumens at the property line.
13. That there be no internally illuminated signage on the west exterior fascia of the fuel island canopy including any type of light strips or neon / light-emitting diode (LED) banding.
14. That illuminated building signage on the west building façade of the AM/PM convenience store and carwash building is prohibited.
15. That the park strip landscaping, within the boundaries of the ARCO AM/PM project site, be installed with this development.
16. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. Landscape and irrigation plans shall comply with the State Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
17. That all signs shall require a separate building permit.
18. That all other federal, state and city codes, ordinances and laws be met.

19. That all of the conditions and responsibilities of Conditional Use Permit No. 2017-22 and the subsequent conditions adopted with the approval of Conditional Use Permit No. 2005-22 and Parcel Map No. 2005-14 shall run with the land and subsequent owners/operators shall also be subject to all of the conditions herein, unless amended or revoked.

# CAMERON CREEK MARKETPLACE - ARCO AM/PM

BEING A PORTION OF THE NORTHWEST QUARTER OF THE  
NORTHWEST QUARTER OF SECTION 8, TOWNSHIP 19  
RANGE 25 EAST, MOUNT DIABLO BASE AND  
MERIDIAN, COUNTY OF VISALIA, COUNTY OF TULARE,  
STATE OF CALIFORNIA

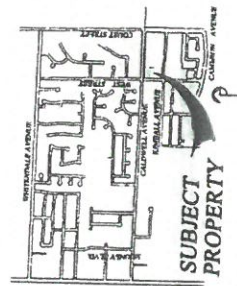
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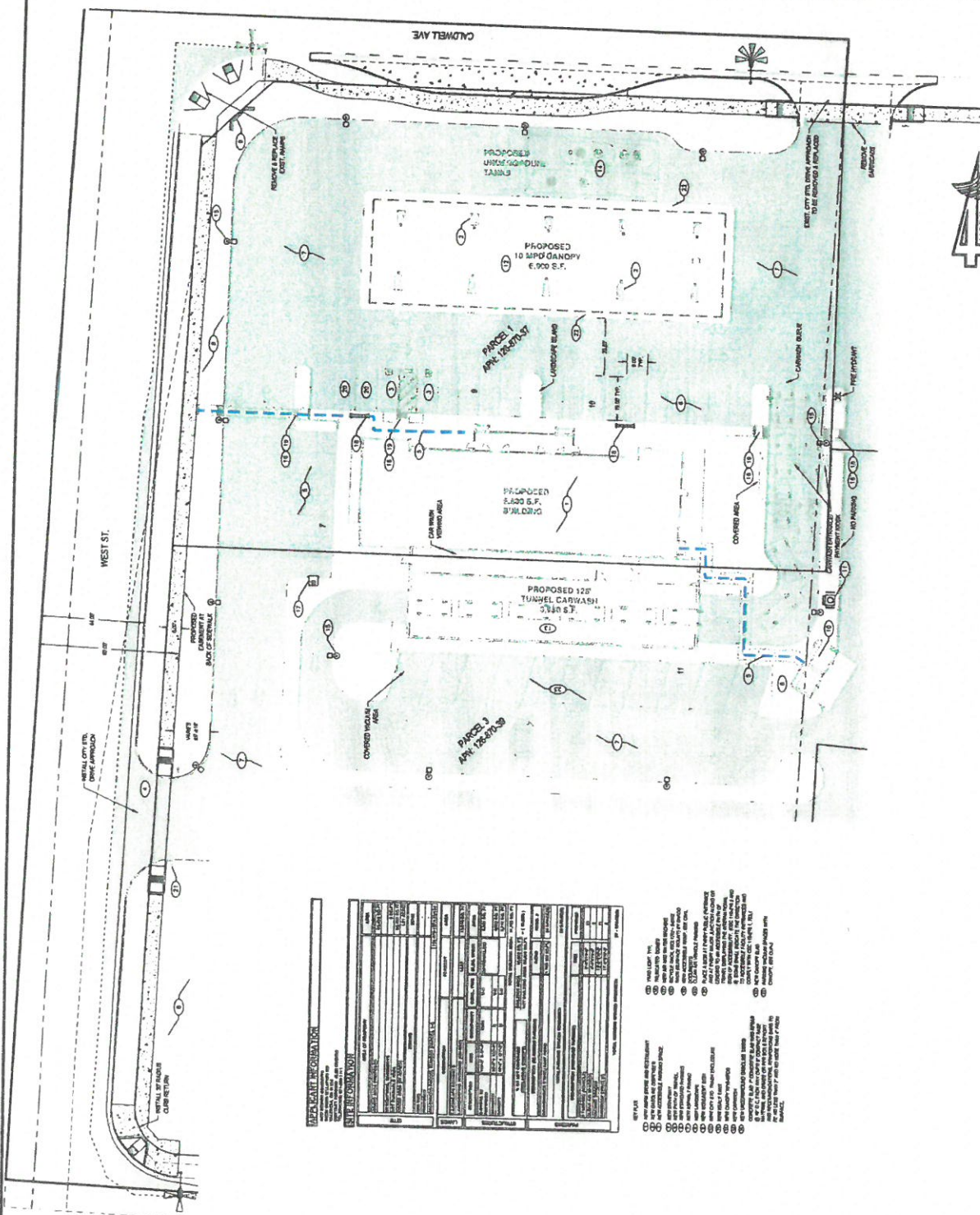
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 EXISTING USE: VACANT  
 PROPOSED ZONING: OFFICE/COMMERCIAL  
 PROPOSED USE: ON-SITE  
 SEWER SERVICE: CALIFORNIA WATER  
 WATER SERVICE: SERVICE COMPANY  
 STORM SERVICE: ON-SITE  
 GAS SERVICE: THE GAS COMPANY  
 ELEC. SERVICE: SOUTHERN CALIFORNIA  
 EDISON COMPANY  
 CITY OF VISALIA  
 ATT. X  
 REFUSE SERVICE:  
 TELEPHONE:  
 FLOOD ZONE:

## BUILDING AND PARKING:

REQUIRED PARKING:  
 EXIST. PARKING:  
 PROPOSED STALLS:  
 TOTAL:  
 ACCESSIBLE PARKING SPACES:  
 EXIST. ACCESSIBLE PARKING SPACES:  
 PROPOSED STALLS:  
 TOTAL:  
 PERCENT OF TOTAL:



VICINITY MAP  
 NOT TO SCALE



**PERMIT INFORMATION**

NO.	DATE	DESCRIPTION	BY
1	01/15/2017	PRELIMINARY	XXX
2	02/15/2017	REVISED	XXX
3	03/15/2017	REVISED	XXX
4	04/15/2017	REVISED	XXX
5	05/15/2017	REVISED	XXX
6	06/15/2017	REVISED	XXX
7	07/15/2017	REVISED	XXX
8	08/15/2017	REVISED	XXX
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96	12/15/2024	REVISED	XXX
97	01/15/2025	REVISED	XXX
98	02/15/2025	REVISED	XXX
99	03/15/2025	REVISED	XXX
100	04/15/2025	REVISED	XXX

**NOTES:**

1. ALL DIMENSIONS ARE IN FEET AND INCHES.
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# CAMERON CREEK MARKETPLACE OVERALL SITE PLAN

BEING A PORTION OF THE NORTHWEST QUARTER OF THE  
NORTHWEST QUARTER OF SECTION 8, TOWNSHIP 19  
SOUTH, RANGE 23 EAST, MOUNT DIABLO BASE AND  
MERIDIAN, IN THE CITY OF VISALIA, COUNTY OF TULARE,  
STATE OF CALIFORNIA

CITY OF VISALIA BENCHMARK: 1567, ELEV. 214.27  
CONCRETE OF INTERSECTION OF COURT AND CALDWELL

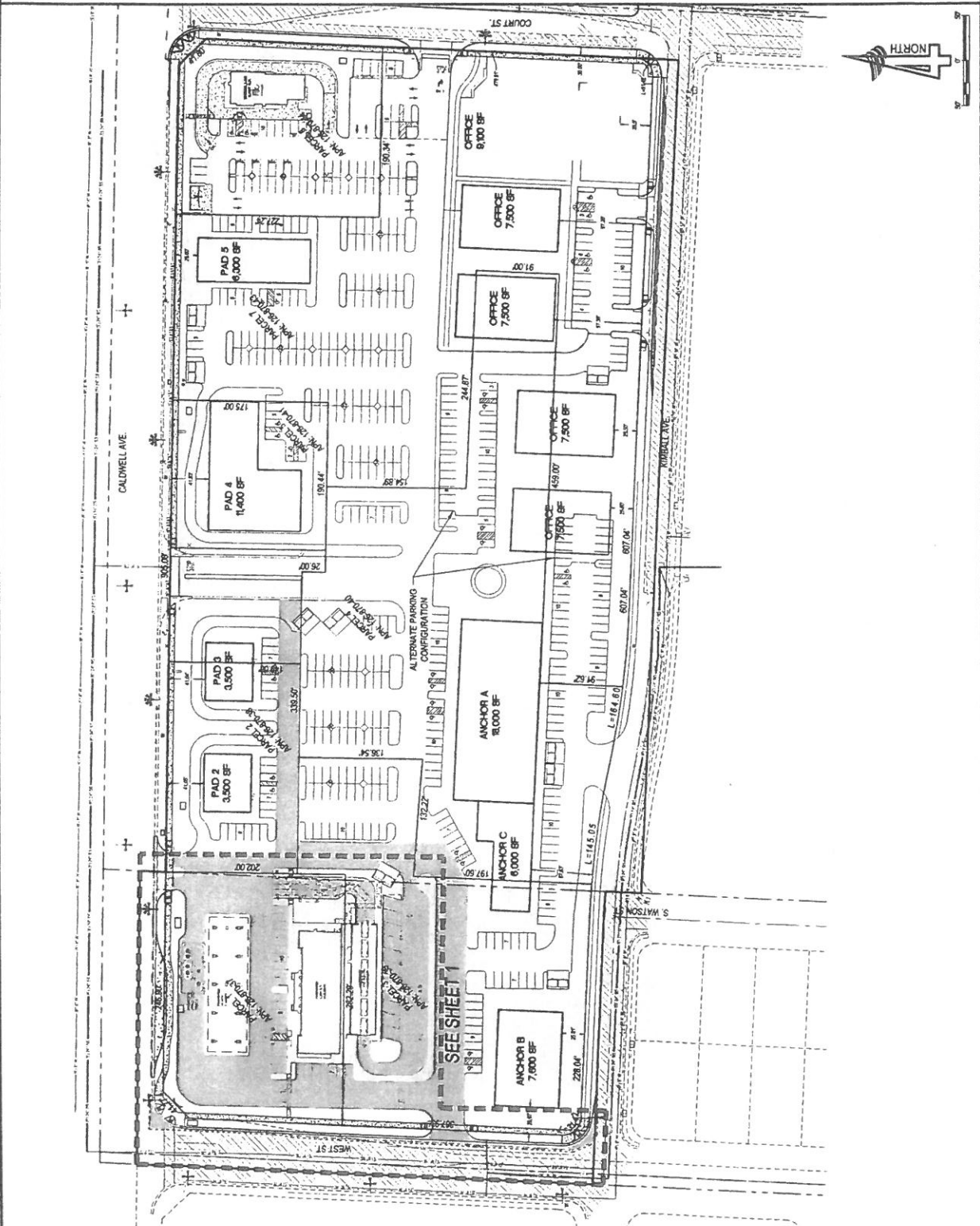
MARCH 16, 2017

PREPARED FOR: CAMERON CREEK MARKETING, LLC  
121 N. GARDEN ST., STE. 110  
VISALIA, CA 93276  
PREPARED BY: 4CREKS  
214 E. MAIN ST., STE. 200  
VISALIA, CA 93276

Exhibit "B"



2 OF 2





**PROPOSED PROJECT: CAMERON CREEK ARCO AMPM @ Caldwell & West, Visalia**  
**DEVELOPER / OWNER: NACHHATTAR SINGH CHANDI / CHANDI GROUP USA**

**OPERATIONAL STATEMENT FOR CUP SUBMITTAL**

- Owner will construct an ARCO am/pm gas station with c-store and express tunnel car wash at the site.
- The business will sell vehicle fuel (auto and diesel) along with typical convenience store goods, including quick-serve food, snacks, and beverages, including coffee, sodas, beer and wine. The car wash is a state-of-the-art self-service express tunnel car wash, with vacuum canopy for self-service vacuum.
- The site is currently vacant, raw land.
- Business is not seasonal and will be open year-round. Gas station and c-store will be open 24/7. Car wash will be open 7 days a week, from 6 am – 7 pm.
- Based on other similarly owned businesses, we anticipate that the store would have an average of 10 – 20 customers at most during peak times. (at any one given time)
- We anticipate that we will have 18 -24 employees, between the gas station, c-store and car wash operations.
- Other than the fuel supplier truck, which would be a standard 18-wheeler type, the typical merchandise delivery vehicles are box truck type and we generally have about five key suppliers making deliveries of this merchandise.
- The business operates with typical fuel station and convenience store equipment and does not produce any hazardous waste material.
- The owner/developer is an experienced owner-operator and currently owns over twenty similar gas station sites, the last several which have been ground-up construction projects comparable to this proposed site.

---

Chandi Group USA, Inc. ♦ 42270 Spectrum St. ♦ Indio ♦ California ♦ 92203

(760) 396-9260: Business Offices ♦ (760) 396-5245: Fax



### Exhibit "D"

**1 PRELIMINARY ELEVATION (C-STORE & CAR WASH)**

**2 PRELIMINARY ELEVATION (C-STORE & CAR WASH)**

**3 PRELIMINARY ELEVATION (C-STORE & CAR WASH)**

**4 PRELIMINARY ELEVATION (C-STORE & CAR WASH)**

**5 PRELIMINARY ELEVATION (CANOPY)**

**6 PRELIMINARY POLE SIGN**

**B1 BUILDING SIGN**

**DISPENSER**

**BUILDING AWNING FASCIA DECAL**

**CANOPY SPARK**

**PROPOSED SIGN SUMMARY**

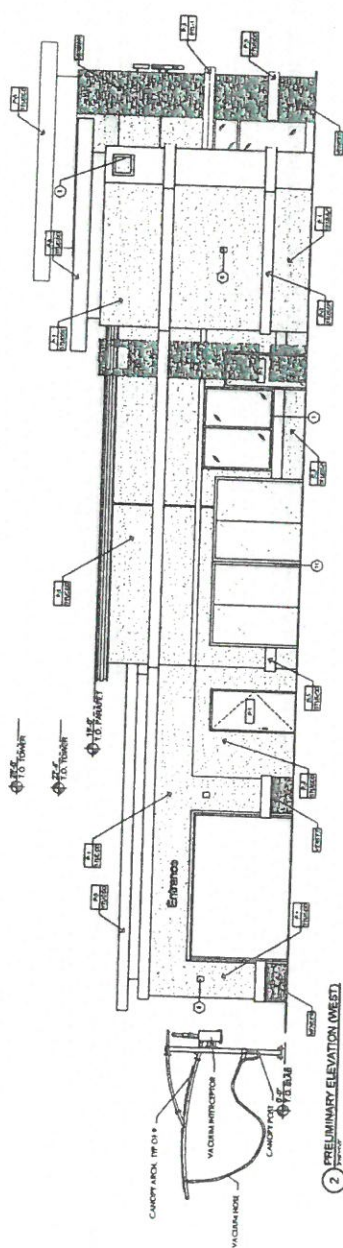
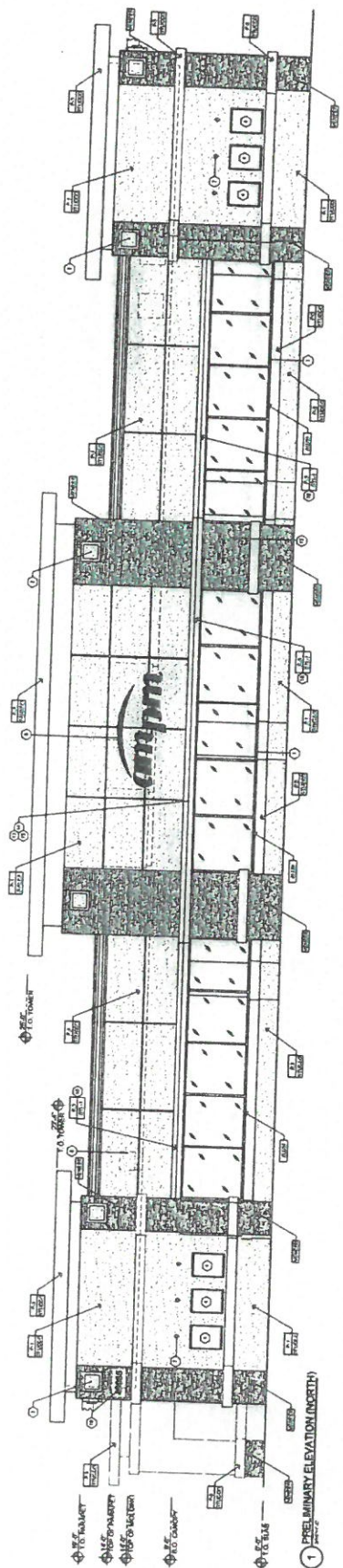
NAME	DESCRIPTION	STATUS	SIZE (H x W x D)	MTS (SQ. FT.)	MTS (SQ. FT.)	TOTAL MTS
(1)	BUILDING (AMP/PM 31" LETTERS)	NEW	17'-0" x 12'-0" x 1'-0"	8.48	1	8.48
(2)	AMP/PM TOWER SIGN	NEW	7'-0" x 7'-0"	8.3	6	49.8
(3)	CANOPY (AMP/PM LETTERS)	NEW	1'-0" x 6'-0"	10	2	20
(4)	CANOPY DECAL (W/VEHICLE L&P)	NEW	30'-0" x 1'-0"	30.0	1	30.0
(5)	CANOPY SPARK	NEW	34' x 34'	7.08	3	21.24
(6)	CANOPY (AMP/PM LETTERS)	NEW	2'-0" x 6'-0"	2.0	2	4.0
(7)	CANOPY LETTERS	NEW	8'-0" x 1'-0"	14.12	1	14.12
(8)	MONUMENT SIGN	NEW	8'-0" x 4'-0"	16.73	1	16.73
<b>TOTAL MTG SIGN AREA (SQ. FT.)</b>						<b>708.14</b>

1. SIGNAGE AND/OR SIGNAGE POINT BY SIGN INSTALLER  
2. SIGNAGE TO BE PROVIDED AND SUPPLIED BY GC

**7 PRELIMINARY SITE PLAN**



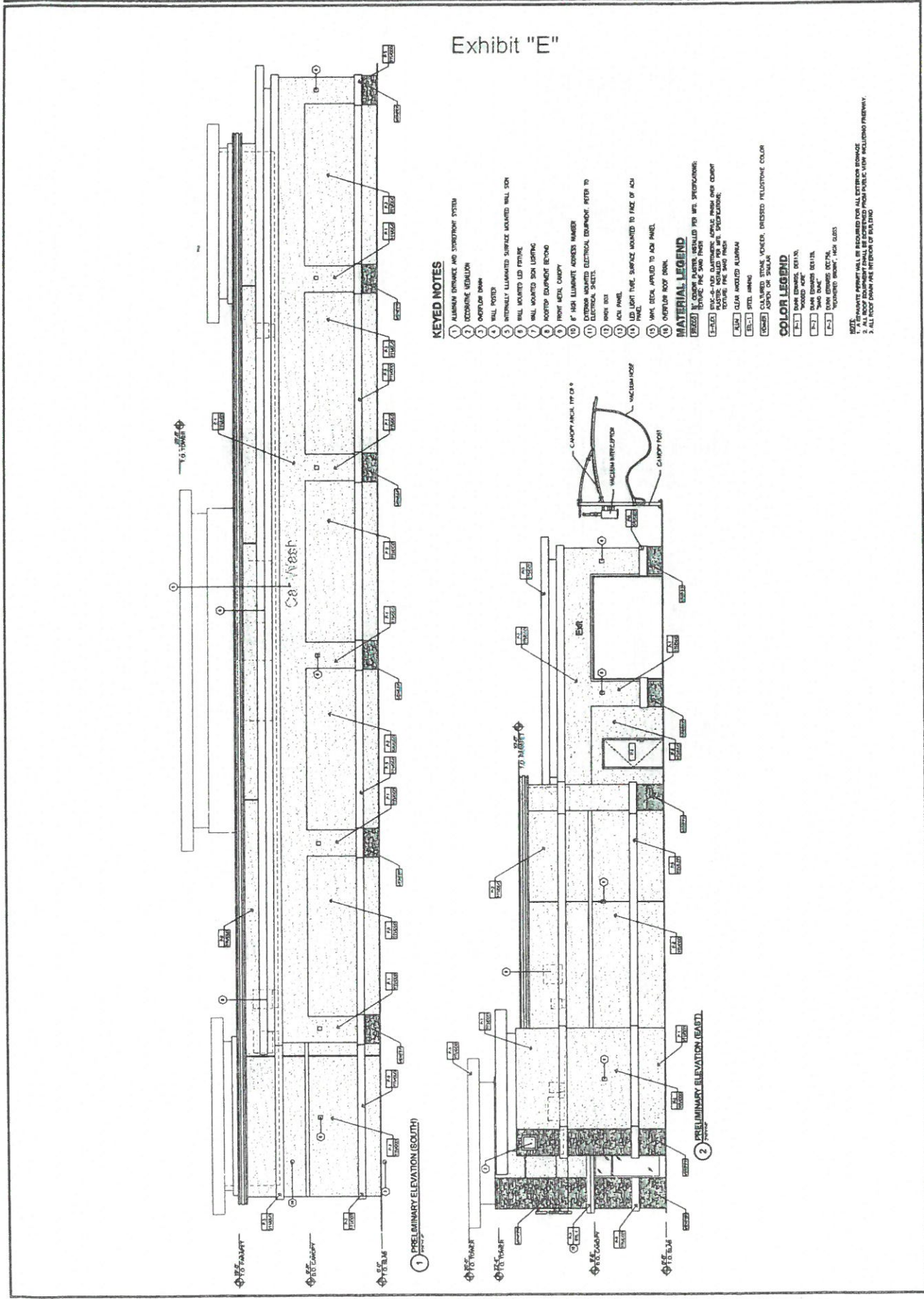
## Exhibit "E"



- [illegible]

**NOTE:**

1. A SEPARATE PERMIT WILL BE REQUIRED FOR ALL EXTERIOR SIGNAGE
2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FRIGIDARY.
3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING





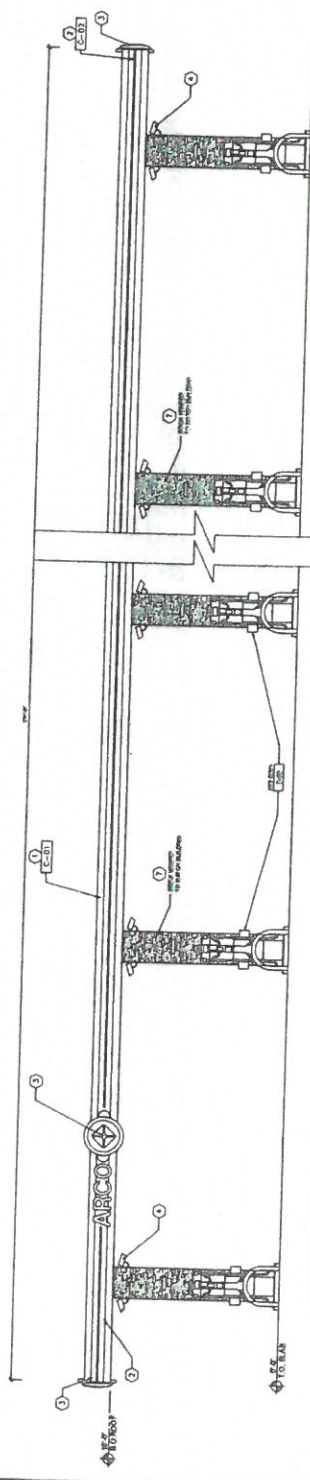
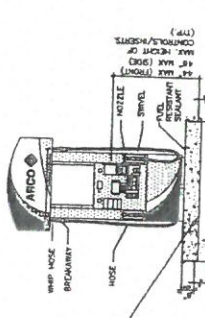
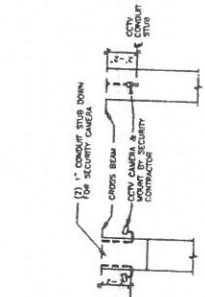
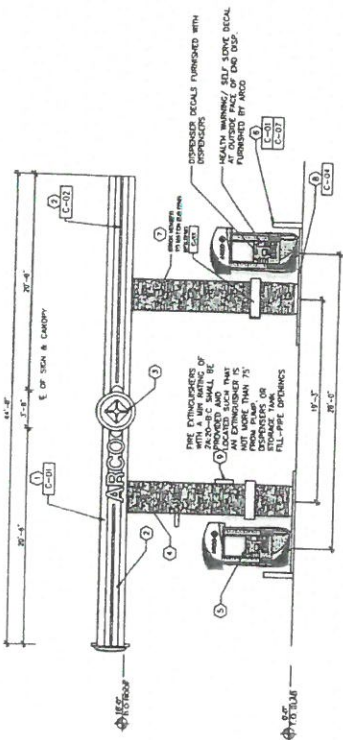
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PROJECT NO.	03/07/17
DESIGNED BY	AGC
CHECKED BY	AGC
DATE	03/07/17
SCALE	AS SHOWN
PROJECT	NEW GAS STATION @ CAMERAON CREEK
CLIENT	AGC
PROJECT	AGC

CUP-5

Exhibit "E"

- KEYED NOTES:**
1. ALL FACIAL
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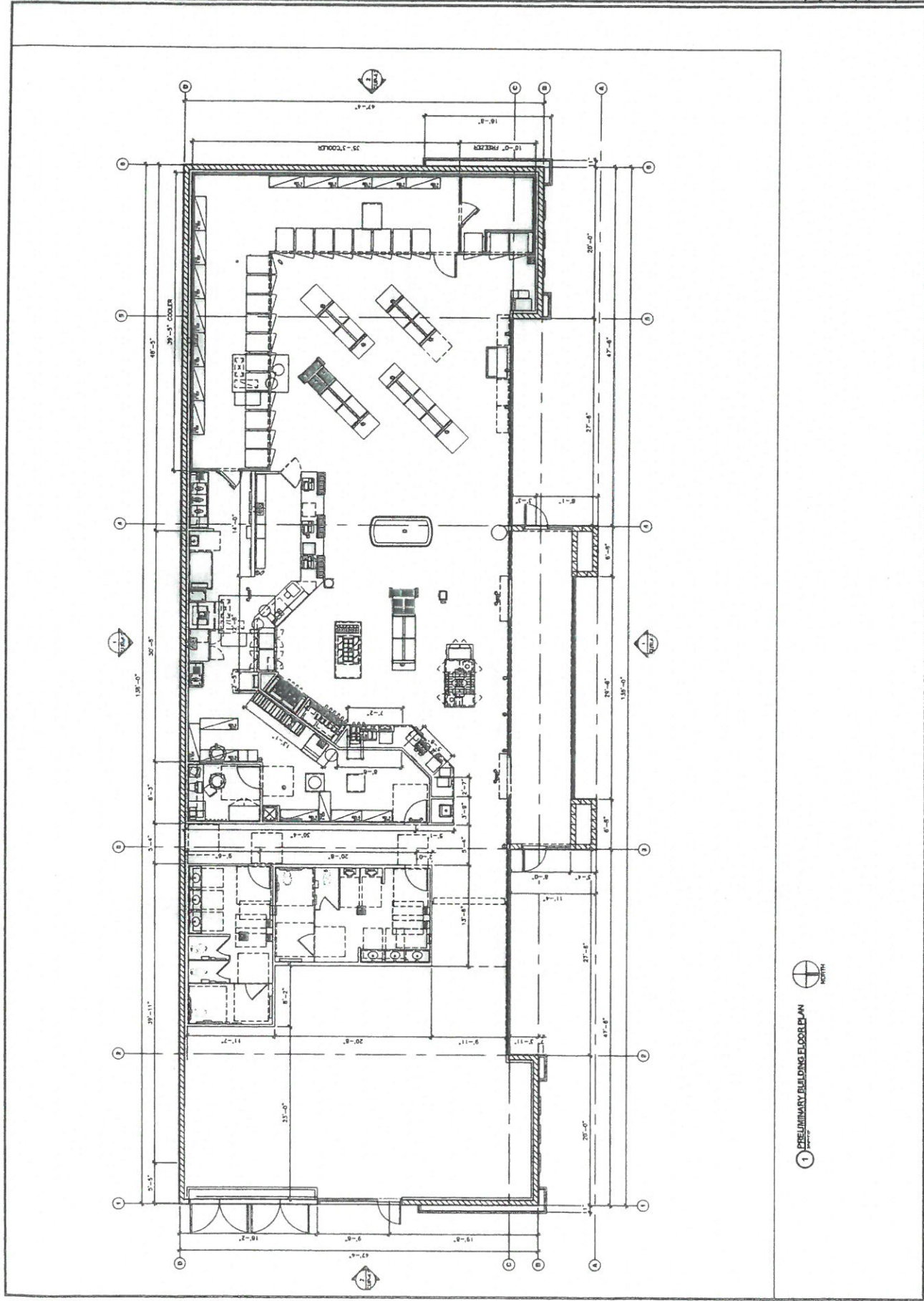




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NEW GAS STATION @ CAMERON CREEK  
 S.E. CALDWELL AVENUE AND S. WEST ST. VISALIA, CA  
 PRELIMINARY BUILDING FLOOR PLAN

Project No.	CUP-2
Client	AGC DESIGN CONCEPT, INC.
Design By	AGC DESIGN CONCEPT, INC.
Check By	AGC DESIGN CONCEPT, INC.
Drawn By	AGC DESIGN CONCEPT, INC.
Scale	AS SHOWN
Notes	SEE SHEET CUP-1



1 PRELIMINARY BUILDING FLOOR PLAN  
 NORTH

August 20, 2017

City of Visalia Planning Division

315 East Acequia Ave

Visalia CA 93291

Subject: Mitigated Negative Declaration

(Conditional Use Permit No. 2017-22)



Reasons this Mitigated Negative Declaration should not be approved:

1. Excessive lighting to existing home owners in affect area.
2. Hours of operation will diminish property values in addition to the increased noise level and street traffic from autos, customers, and trucks used to fill the tanks. Further, deliveries made to the Arco in various times and days will disrupt normal sleeping habits and create congestion to the existing owners affected by this location.
3. Significantly reduce resale and existing home owner values.
4. Existing bus stop for small children is across the street on West Street this could increase the risk of injury to parents and children.
5. Liquor sales and consumption in the immediate area will increase risks of accidents and injury to the affected area.
6. Added traffic will increase exhaust emission to affect area.
7. Sound barriers must be established to reduce noise and enhance "quite enjoyment of existing homes". Sound travels through wooden fences in far greater decibels than from a brick fence or other sound resistant material.
8. Combined future projects contribution is cumulatively considerable
9. Project is not within zoning, plans, and land use controls.
10. Greenhouse gas hazards and hazardous material will exist at the proposed location.

11. West Street from Coldwell and Kimball Ave has a high degree of excessive speed from motorist traveling this route.
12. Has an EIR been conducted and has CEQA been contacted to verify the site.
13. CEQA policies prefer EIRs, the law effectively protects those who prepare them by applying the "substantial evidence" standard of judicial review.
14. The California Environment Quality Act (CEQA) is the primary state law that requires public agencies and their decision-makers to understand and evaluate the environmental consequences of their discretionary decisions before making them.
15. Quantitative thresholds, such as traffic intersection level of standards as well as noise thresholds must be maintained.
16. The local agency handling this has not provided "substantial evidence" to support their claim for Mitigated Negative Declaration.
17. Existing issues have not been adequately addressed. CEQA defines adequately addressed as " they cannot be mitigated to avoid or substantially lessen the significant impacts despite the project proponent's willingness to accept all feasible mitigation measures, and the only purpose of including analysis of such effects in another environmental impact report would be to put the agency in a position to adopt a statement of overriding considerations with respect to the effects"
18. In an MND, reduction of impacts must be "clearly" to a less than significant level.
19. Adequate Mitigation, must include (a) physically modifying the project, such as design changes to avoid or substantially reduce an effect. (b) Limiting project activities, such as operational limits like seasonal or daily time restrictions. (c) Constructing improvements intended for mitigation such as BMPs. (d) Compensatory actions, such as replacement of lost habitat through restoration or enhancement.

On these grounds and further arguments presented to the Visalia Planning Commission in the future I strongly urge the Agency to reconsider their proposed adoption of the "Mitigated Negative Declaration"



Regards

A handwritten signature in dark ink, appearing to read 'William Reeser', with a stylized, cursive script.

William Reeser

William Reeser

508 W Kimball Ave

Visalia CA 93277

Breaser55@aol.com

(559) 308-8716

CITY OF VISALIA  
315 E. ACEQUIA STREET  
VISALIA, CA 93291

**NOTICE OF A PROPOSED  
MITIGATED NEGATIVE DECLARATION**

Project Title: Conditional Use Permit No. 2017-22

Project Description: The project applicant is requesting to develop a 1.29-acre parcel (i.e. Parcel 1 of Parcel Map 5141) within the Cameron Creek Market Place Shopping Center with an ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers (see Exhibit "A"). The Cameron Creek ARCO project site is zoned C-MU (Commercial Mixed Use) and located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

The ARCO gas station site will be developed with 26 parking spaces, excluding the 20 fueling station spaces located under the fueling island canopy. In addition, 11 vacuum parking stations will be provided south of the carwash building. The ARCO gas station site will be accessible by two new drive approaches which will be installed along both the Caldwell Avenue and West Street frontages. In addition, curb, gutter, sidewalk and park strip landscaping will be installed along the West Street frontage providing pedestrian access to the residential neighborhood to the south of the ARCO gas station project site. Additional on-site improvements include construction of the primary drive aisle which will extend to the east which will allow on-site vehicular access from the ARCO gas station site to the Jack and the Box site as depicted on the overall Cameron Creek Market Place Shopping Center Exhibit "B".

No other development entitlements beyond the conditional use permit have been filed for the Cameron Creek Marketplace Shopping Center.

Project Location: The 1.29-acre project site is located within the planned Cameron Creek Marketplace Shopping Center and is located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

Contact Person: Paul Bernal, Principal Planner

Phone: (559) 713-4025

Pursuant to City Ordinance No. 2388, the Environmental Coordinator of the City of Visalia has reviewed the proposed project described herein and has found that the project will not result in any significant effect upon the environment because of the reasons listed below:

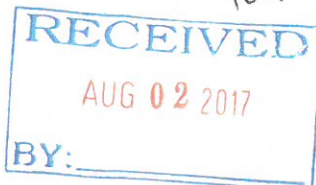
Reasons for Mitigated Negative Declaration: Initial Study No. 2017-39 has identified environmental impact(s) that may occur because of the project; however, with the implementation of mitigation measures identified, impact(s) will be reduced to a level that is less than significant. Copies of the initial study and other documents relating to the subject project may be examined by interested parties at the Planning Division in City Hall East, at 315 East Acequia Avenue, Visalia, CA.

Comments on this proposed Mitigated Negative Declaration will be accepted from August 3, 2017 to August 23, 2017.

Date: 8-2-17

Signed: \_\_\_\_\_

Paul Scheibel, AICP  
Environmental Coordinator  
City of Visalia



## MITIGATED NEGATIVE DECLARATION

**Project Title:** Conditional Use Permit No. 2017-22

**Project Description:** The project applicant is requesting to develop a 1.29-acre parcel (i.e. Parcel 1 of Parcel Map 5141) within the Cameron Creek Market Place Shopping Center with an ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers (see Exhibit "A"). The Cameron Creek ARCO project sited is zoned C-MU (Commercial Mixed Use) and located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

The ARCO gas station site will be developed with 26 parking spaces, excluding the 20 fueling station spaces located under the fueling island canopy. In addition, 11 vacuum parking stations will be provided south of the carwash building. The ARCO gas station site will be accessible by two new drive approaches which will be installed along both the Caldwell Avenue and West Street frontages. In addition, curb, gutter, sidewalk and park strip landscaping will be installed along the West Street frontage providing pedestrian access to the residential neighborhood to the south of the ARCO gas station project site. Additional on-site improvements include construction of the primary drive aisle which will extend to the east which will allow on-site vehicular access from the ARCO gas station site to the Jack and the Box site as depicted on the overall Cameron Creek Market Place Shopping Center Exhibit "B".

No other development entitlements beyond the conditional use permit have been filed for the Cameron Creek Marketplace Shopping Center.

**Project Location:** The 1.29-acre project site is located within the planned Cameron Creek Marketplace Shopping Center and is located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

**Project Facts:** Refer to Initial Study for project facts, plans and policies, and discussion of environmental effects.

### Attachments:

Initial Study	(X)
Environmental Checklist	(X)
Maps	(X)
Mitigation Measures	(X)
Noise Study	(X)
Traffic Impact Study	( )

### DECLARATION OF NO SIGNIFICANT EFFECT:

This project will not have a significant effect on the environment for the following reasons:

- The project does not have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory.
- The project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.
- The project does not have environmental effects which are individually limited but cumulatively considerable. Cumulatively considerable means that the incremental effects of an individual project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.



- (d) The environmental effects of the project will not cause substantial adverse effects on human beings, either directly or indirectly.

This Mitigated Negative Declaration has been prepared by the City of Visalia Planning Division in accordance with the California Environmental Quality Act of 1970, as amended. A copy may be obtained from the City of Visalia Planning Division Staff during normal business hours.

APPROVED  
Paul Scheibel, AICP  
Environmental Coordinator

By: 

Date Approved: 8-2-17

Review Period: 20 days

## INITIAL STUDY

### I. GENERAL

**A. Conditional Use Permit No. 2017-22:** The project applicant is requesting to develop a 1.29-acre parcel (i.e. Parcel 1 of Parcel Map 5141) within the Cameron Creek Market Place Shopping Center with an ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers (see Exhibit "A"). The Cameron Creek ARCO project sited is zoned C-MU (Commercial Mixed Use) and located on the southeast corner of West Caldwell Avenue and South West Street (APN: 126-870-037).

The ARCO gas station site will be developed with 26 parking spaces, excluding the 20 fueling station spaces located under the fueling island canopy. In addition, 11 vacuum parking stations will be provided south of the carwash building. The ARCO gas station site will be accessible by two new drive approaches which will be installed along both the Caldwell Avenue and West Street frontages. In addition, curb, gutter, sidewalk and park strip landscaping will be installed along the West Street frontage providing pedestrian access to the residential neighborhood to the south of the ARCO gas station project site. Additional on-site improvements include construction of the primary drive aisle which will extend to the east which will allow on-site vehicular access from the ARCO gas station site to the Jack and the Box site as depicted on the overall Cameron Creek Market Place Shopping Center Exhibit "B".

No other development entitlements beyond the conditional use permit have been filed for the Cameron Creek Marketplace Shopping Center.

### B. Identification of the Environmental Setting:

The overall Cameron Creek Marketplace Shopping Center is located on the south side of West Caldwell Avenue and is bounded by West Kimball Avenue to the south, South Court Street to the east and South West Street to the west. The ARCO gas station project site (1.29 ac.) is proposed to be constructed on the southeast corner of West Caldwell Avenue and South West Street on Parcel 1 of Parcel Map No. 5141.

The surrounding uses, Zoning, and General Plan for the 1.29-acre project site are as follows:

	General Plan (2014 Land Use)	Zoning (1993)	Existing uses
North:	West Caldwell Avenue (Arterial Street)	N/A	West Caldwell Avenue (Arterial Street)
South:	Commercial Mixed Use	C-MU – Commercial Mixed Use	Vacant property (Parcel 2 and 3 of PM 5141) / part of the Cameron Creek Market Place Shopping Center
East:	Commercial Mixed Use	C-MU – Commercial Mixed Use	Vacant property (Parcel 3 of PM 5141) / part of the Cameron Creek Market Place Shopping Center
West:	South West Street (Collector Street)	N/A	South West Street (Collector Street)

Fire and police protection services, street maintenance of public streets, refuse collection, and wastewater treatment will be provided by the City of Visalia upon annexation and the development of the project area.

**C. Plans and Policies:** The General Plan Land Use Diagram, adopted October 14, 2014, designates the project site as Commercial Mixed Use. The Zoning Map, adopted on April 6, 2017, designates 1.29-acre site as Commercial Mixed Use. The land use designation and zoning is consistent with the General Plan Land Use Designation of Commercial Mixed Use and C-MU zoning as noted in Table 9-1 "Consistency Between the Plan and Zoning of the General Plan".

## **II. ENVIRONMENTAL IMPACTS**

No significant adverse environmental impacts have been identified for this project. The City of Visalia Land Use Element, Circulation Element, Zoning and Subdivision Ordinances contain policies and regulations that are designed to mitigate impacts to a level of non-significance.

## **III. MITIGATION MEASURES**

The following mitigation measures will reduce potential environmental impacts related to Noise Impacts to a less than significant level as shown below:

**Noise – A Noise Impact Analysis prepared for the proposed project (ref.: Cameron Creek Noise Impact Analysis, Kunzman Associates, Inc., April 27, 2017) has concluded that an exterior noise level in excess of the daytime 65 dB DNL standard for noise-sensitive land uses, specified in the City's Noise Element, will occur during the construction of the ARCO gas station site unless mitigated. In addition, interior noise levels, when adjusted for ambient noise levels, will be below the applicable noise standards for interior noise. To ensure that community noise standards are met for noise-sensitive land uses adjacent to the site, the Noise Analysis identifies construction practice Mitigation Measures that shall be followed. To attenuate the noise, the Construction Mitigation Measures listed on page 26 of the Noise Impact Analysis shall be implemented.**

**Therefore, to ensure that community noise standards are met for the proposed project, the project site shall be developed and shall operate in substantial compliance with the Mitigation Measures 1.1 through 1.7.**

Therefore, to ensure that these requirements are met for the proposed project, the project shall be developed and shall operate in substantial compliance with the Construction Noise Impact Mitigation Measure 1.1 through 1.6. These mitigation measures are included as an attachment to the Mitigated Negative Declaration.

The City of Visalia Zoning Ordinance contains guidelines, criteria, and requirements for the mitigation of potential impacts related to light/glare, visibility screening, noise, and traffic/parking to eliminate and/or reduce potential impacts to a level of non-significance.

## **IV. MITIGATION MONITORING PROGRAM**

<b><u>Mitigation Measure</u></b>	<b><u>Responsible Party</u></b>	<b><u>Timeline</u></b>
<b>Construction Noise Impact Mitigation Measure 1.1:</b> During all project site excavation and grading on-site, construction contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturer standards.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.2:</b> The contractor shall place all stationary construction equipment so that emitted noise is directed away from the noise sensitive receptors nearest the project site.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.3:</b> Equipment shall be shut off and not left to idle when not in use.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.



<b>Construction Noise Impact Mitigation Measure 1.4:</b> The contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and sensitive receptors nearest the project site during all project construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.5:</b> The project proponent shall mandate that the construction contractor prohibit the use of music or sound amplification on the project site during construction.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Construction Noise Impact Mitigation Measure 1.6:</b> The construction contractor shall limit haul truck deliveries to the same hours specified for construction equipment.	Project Applicant	Noise Impact Mitigation for specified equipment shall be enforced through the building permit and construction inspection.
<b>Operation Noise Impact Mitigation Measure 1.7:</b> The car wash shall be limited to the hours of 7:00 a.m. to 9:00 pm Monday thru Sunday to eliminate early morning and late night and noise impacts.	Planning Division	Noise Impact Mitigation shall be enforced as a part of the daily operation for the site as a condition of the Conditional Use Permit which provides remedies for non-compliance.

#### **V. PROJECT COMPATIBILITY WITH EXISTING ZONES AND PLANS**

The project is compatible with the General Plan, Zoning and Subdivision Ordinances as the project relates to surrounding properties.


#### **VI. SUPPORTING DOCUMENTATION**

The following documents are hereby incorporated into this Negative Declaration and Initial Study by reference:

- Visalia General Plan Update. Dyett & Bhatia, October 2014.
- Visalia City Council Resolution No. 2014-38 (Certifying the Visalia General Plan Update), passed and adopted October 14, 2014.
- Visalia General Plan Update Final Environmental Impact Report (SCH No. 2010041078). Dyett & Bhatia, June 2014.
- Visalia General Plan Update Draft Environmental Impact Report (SCH No. 2010041078). Dyett & Bhatia, March 2014.
- Visalia City Council Resolution No. 2014-37 (Certifying the EIR for the Visalia General Plan Update), passed and adopted October 14, 2014.
- Visalia Municipal Code, including Title 17 (Zoning Ordinance).
- California Environmental Quality Act Guidelines.
- City of Visalia, California, Climate Action Plan, Draft Final. Strategic Energy Innovations, December 2013.
- Visalia City Council Resolution No. 2014-36 (Certifying the Visalia Climate Action Plan), passed and adopted October 14, 2014.
- City of Visalia Storm Water Master Plan. Boyle Engineering Corporation, September 1994.
- City of Visalia Sanitary Sewer Master Plan. City of Visalia, 1994.
- Cameron Creek Noise Impact Analysis, Kunzman Associates, Inc., April 27, 2017

#### **VII. NAME OF PERSON WHO PREPARED INITIAL STUDY**

  
Paul Bernal  
Principal Planner

  
Paul Scheibel, AICP  
Environmental Coordinator

INITIAL STUDY  
ENVIRONMENTAL CHECKLIST

Name of Proposal	Conditional Use Permit No. 2017-22		
NAME OF PROPONENT:	Chandi Group USA, Inc.	NAME OF AGENT:	Steven Macias – 4Creeks, Inc.
Address of Proponent:	42270 Spectrum St. Indio CA 92203	Address of Agent:	324 S. Santa Fe St. Ste. A Visalia CA 93292
Telephone Number:	(760) 396-9260	Telephone Number:	(559) 802-3052
Date of Review	July 27, 2017	Lead Agency:	City of Visalia

The following checklist is used to determine if the proposed project could potentially have a significant effect on the environment. Explanations and information regarding each question follow the checklist.

1 = No Impact                      2 = Less Than Significant Impact  
3 = Less Than Significant Impact with Mitigation Incorporated                      4 = Potentially Significant Impact

I. AESTHETICS

Would the project:

- 2 a) Have a substantial adverse effect on a scenic vista?
- 1 b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?
- 2 c) Substantially degrade the existing visual character or quality of the site and its surroundings?
- 2 d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

II. AGRICULTURAL RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

- 2 a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency to non-agricultural use?
- 2 b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?
- 1 c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?
- 1 d) Result in the loss of forest land or conversion of forest land to non-forest use?
- 1 e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to nonagricultural use?

III. AIR QUALITY

Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

- 2 a) Conflict with or obstruct implementation of the applicable air quality plan?
- 2 b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?
- 2 c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?
- 1 d) Expose sensitive receptors to substantial pollutant concentrations?
- 1 e) Create objectionable odors affecting a substantial number of people?

IV. BIOLOGICAL RESOURCES

Would the project:

- 2 a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 2 b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 2 c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?
- 2 d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?



- 2 e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?
- 1 f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

#### V. CULTURAL RESOURCES

Would the project:

- 1 a) Cause a substantial adverse change in the significance of a historical resource as defined in Public Resources Code Section 15064.5?
- 1 b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Public Resources Code Section 15064.5?
- 1 c) Directly or indirectly destroy a unique paleontological resource or site, or unique geologic feature?
- 1 d) Disturb any human remains, including those interred outside of formal cemeteries?

#### VI. GEOLOGY AND SOILS

Would the project:

- 1 a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
  - 1 i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?
  - 1 ii) Strong seismic ground shaking?
  - 1 iii) Seismic-related ground failure, including liquefaction?
  - iv) Landslides?
- 1 b) Result in substantial soil erosion or loss of topsoil?
- 1 c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?
- 1 d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?
- 1 e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

#### VII. GREENHOUSE GAS EMISSIONS

Would the project:

- 2 a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?
- 2 b) Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

#### VIII. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- 1 a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

- 1 b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?
- 1 c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?
- 1 d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?
- 3 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?
- 1 f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?
- 1 g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?
- 1 h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

#### IX. HYDROLOGY AND WATER QUALITY

Would the project:

- 2 a) Violate any water quality standards of waste discharge requirements?
- 2 b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?
- 2 c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?
- 2 d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?
- 2 e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?
- 1 f) Otherwise substantially degrade water quality?
- 1 g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?
- 1 h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?
- 1 i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?
- 1 j) Inundation by seiche, tsunami, or mudflow?



#### X. LAND USE AND PLANNING

Would the project:

- 1 a) Physically divide an established community?
- 1 b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?
- 1 c) Conflict with any applicable habitat conservation plan or natural community conservation plan?

#### XI. MINERAL RESOURCES

Would the project:

- 1 a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
- 1 b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

#### XII. NOISE

Would the project:

- 3 a) Cause exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?
- 3 b) Cause exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?
- 2 c) Cause a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?
- 2 d) Cause a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?
- 1 f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

#### XIII. POPULATION AND HOUSING

Would the project:

- 2 a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?
- 1 b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?
- 1 c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

#### XIV. PUBLIC SERVICES

Would the project:

- 1 a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically

altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

- 1 i) Fire protection?
- 1 ii) Police protection?
- 2 iii) Schools?
- 1 iv) Parks?
- 1 v) Other public facilities?

#### XV. RECREATION

Would the project:

- 1 a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?
- 1 b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

#### XVI. TRANSPORTATION / TRAFFIC

Would the project:

- 1 a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?
- 2 b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?
- 1 c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?
- 1 d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?
- 1 e) Result in inadequate emergency access?
- 1 f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?

#### XVII. UTILITIES AND SERVICE SYSTEMS

Would the project:

- 1 a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
- 2 b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 2 c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 1 d) Have sufficient water supplies available to service the project from existing entitlements and resources, or are new or expanded entitlements needed?

- 1 e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?
- 1 f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?
- 1 g) Comply with federal, state, and local statutes and regulations related to solid waste?

#### XVIII. TRIBAL CULTURAL RESOURCES

Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

- 1 a) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or
- 1 b) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

#### XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- 3 a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?
- 2 b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?
- 3 c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Note: Authority cited: Sections 21083 and 21083.05, Public Resources Code. Reference: Section 65088.4, Gov. Code; Sections 21080(c), 21080.1, 21080.3, 21083, 21083.05, 21083.3, 21093, 21094, 21095, and 21151, Public Resources Code; *Sundstrom v. County of Mendocino*, (1988) 202 Cal.App.3d 296; *Leonoff v. Monterey Board of Supervisors*, (1990) 222 Cal.App.3d 1337; *Eureka Citizens for Responsible Govt. v. City of Eureka* (2007) 147 Cal.App.4th 357; *Protect the Historic Amador Waterways v. Amador Water Agency* (2004) 116 Cal.App.4th at 1109; *San Franciscans Upholding the Downtown Plan v. City and County of San Francisco* (2002) 102 Cal.App.4th 656.

Revised 2009

## DISCUSSION OF ENVIRONMENTAL EVALUATION

### I. AESTHETICS

- a. This project will not adversely affect the view of any scenic vistas. The Sierra Nevada mountain range may be considered a scenic vista, but views of the range will not be adversely impacted or significantly by the project. Gas stations and drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Caldwell Avenue, which is a designated arterial roadway. The City's General Plan Land Use Map designates the site as Commercial Mixed Use. Staff believes that the proposed gas station and drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this project.
- b. There are no scenic resources on the site.
- c. The City has development standards related to landscaping and other amenities that will ensure that the visual character of the area is not degraded upon any future development. The proposed project in itself will not change the visual character or quality of the site.
- d. The project will facilitate for the future development of the entire Cameron Creek Marketplace Shopping center. New sources of light would be created upon the site's future development that is typical of urban development. The City's existing development standards require that light be directed and/or shielded so it does not fall upon adjacent properties upon the site's future development.

### II. AGRICULTURAL RESOURCES

- a. The project is located on property that is not identified as farmland based on maps prepared by the California Department of Conservation and contained within the Visalia General Plan, Figure 6-4.

The Visalia General Plan Update Environmental Impact Report (EIR) has already considered the environmental impacts of the conversion of properties within the Planning Area, which includes the subject property, into non-agriculture uses. Overall, the General Plan results in the conversion of over 14,000 acres of Important Farmland to urban uses, which is considered significant and unavoidable. Aside from preventing development altogether the conversion of Important Farmland to urban uses cannot be directly mitigated. However, the General Plan contains multiple policies that together work to limit conversion only to the extent needed to accommodate long-term growth. The General Plan policies identified under Impact 3.5-1 of the EIR serve as the mitigation, which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area. These policies include the implementation of a three-tier growth boundary system that assists in protecting open space around the City fringe and maintaining compact development within the City limits.

The project is within the Urban Development Tier 1 Boundary. Development of residential, commercial, and industrial lands in Tier 1 may occur at any time.

- b. The project site is zoned C-MU (Commercial Mixed Use) and is part of the Cameron Creek Marketplace Shopping Center. The project is bordered by Caldwell Avenue to the north, West Street to the west and vacant property to the east and south that is zoned and planned for commercial development.
- c. There is no forest or timber land currently located on the site.
- d. There is no forest or timber land currently located on the site.
- e. The project will not involve any changes that would promote or result in the conversion of farmland to non-agriculture use. The subject property is currently designated for an urban development rather than agricultural land use. Properties that are vacant may develop in a way that is consistent with their zoning and land use designated at any time. The adopted Visalia General Plan's implementation of a three-tier growth boundary system further assists in protecting open space around the City fringe to ensure that premature conversion of farmland to non-agricultural uses does not occur.

### III. AIR QUALITY

- a. The project site is located in an area that is under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The project in itself does not disrupt implementation of the San Joaquin Regional Air Quality Management Plan, and will therefore be a less than significant impact.
- b. Development under the Visalia General Plan will result in emissions that will exceed thresholds established by the SJVAPCD for PM10 and PM2.5. The project will contribute to a net increase of criteria pollutants and will therefore contribute to exceeding the thresholds. Also the project could result in short-term air quality impacts related to dust generation and exhaust due to construction and grading activities. This site was evaluated in the Visalia General Plan Update EIR for conversion into urban development. Development under the General Plan will result in increases of construction and operation-related criteria pollutant impacts, which are considered significant and unavoidable. General Plan policies identified under Impacts 3.3-1 and 3.3-2 serve as the mitigation which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area.

The project is required to adhere to requirements administered by the SJVAPCD to reduce emissions to a level of compliance consistent with the District's grading regulations. Compliance with the SJVAPCD's rules and regulations will reduce potential impacts associated with air quality standard violations to a less than significant level.



In addition, development of the project will be subject to the SJVAPCD Indirect Source Review (Rule 9510) procedures that became effective on March 1, 2006. The Applicant will be required to obtain permits demonstrating compliance with Rule 9510, or payment of mitigation fees to the SJVAPCD.

- c. Tulare County is designated non-attainment for certain federal ozone and state ozone levels. The project will result in a net increase of criteria pollutants. This site was evaluated in the Visalia General Plan Update EIR for conversion into urban development. Development under the General Plan will result in increases of construction and operation-related criteria pollutant impacts, which are considered significant and unavoidable. General Plan policies identified under Impacts 3.3-1, 3.3-2, and 3.3-3 serve as the mitigation, which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area.

The project is required to adhere to requirements administered by the SJVAPCD to reduce emissions to a level of compliance consistent with the District's grading regulations. Compliance with the SJVAPCD's rules and regulations will reduce potential impacts associated with air quality standard violations to a less than significant level.

In addition, development of the project will be subject to the SJVAPCD Indirect Source Review (Rule 9510) procedures that became effective on March 1, 2006. The Applicant will be required to obtain permits demonstrating compliance with Rule 9510, or payment of mitigation fees to the SJVAPCD.

- d. Residences located near the proposed project may be intermittently exposed to pollutant concentrations due to construction activities. The use of construction equipment will be temporary and is subject to SJVAPCD rules and regulations. The impact is considered as less than significant.
- e. The proposed project will not involve the generation of objectionable odors that would affect a substantial number of people.

#### IV. BIOLOGICAL RESOURCES

- a. City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR) for conversion to urban use. In addition, staff had conducted an on-site visit to the site on July 21, 2017 to observe biological conditions and did not observe any evidence or symptoms that would suggest the presence of a sensitive, candidate, or special species.

Based on the above, the site has no known species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. The project would therefore not have a substantial adverse effect on a sensitive, candidate, or special species.

- b. The project is not located within or adjacent to an identified sensitive riparian habitat or other natural community.

City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that certain sensitive natural communities may be directly or indirectly affected by future development within the General Plan Planning Area, particularly valley oak woodlands and valley oak riparian woodlands. Such effects would be considered significant. However, the General Plan contains multiple policies, identified under Impact 3.8-2 of the EIR, that together work to reduce the potential for impacts on woodlands located within in the Planning Area. With implementation of these policies, impacts on woodlands will be less than significant.

- c. The project is not located within or adjacent to federally protected wetlands as defined by Section 404 of the Clean Water Act.

City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that certain protected wetlands and other waters may be directly or indirectly affected by future development within the General Plan Planning Area. Such effects would be considered significant. However, the General Plan contains multiple policies, identified under Impact 3.8-3 of the EIR, that together work to reduce the potential for impacts on wetlands and other waters located within in the Planning Area. With implementation of these policies, impacts on wetlands will be less than significant.

- d. City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that the movement of wildlife species may be directly or indirectly affected by future development within the General Plan Planning. Such effects would be considered significant. However, the General Plan contains multiple policies, identified under Impact 3.8-4 of the EIR, that together work to reduce the potential for impacts on wildlife movement corridors located within in the Planning Area. With implementation of these policies, impacts on wildlife movement corridors will be less than significant.

- e. The City has a municipal ordinance in place to protect valley oak trees. There are no valley oak trees on-site.
- f. There are no local or regional habitat conservation plans for the area.

#### V. CULTURAL RESOURCES

- a. There are no known historical resources located within the project area. If some potentially historical or cultural resource is unearthed during development all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- b. There are no known archaeological resources located within the project area. If some archaeological resource is unearthed during development all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.

- c. There are no known unique paleontological resources or geologic features located within the project area.
- d. There are no known human remains buried in the project vicinity. If human remains are unearthed during development all work should cease until the proper authorities are notified and a qualified professional archaeologist can evaluate the finding and make any necessary mitigation recommendations.

**VI. GEOLOGY AND SOILS**

- a. The State Geologist has not issued an Alquist-Priolo Earthquake Fault Map for Tulare County. The project area is not located on or near any known earthquake fault lines. Therefore, the project will not expose people or structures to potential substantial adverse impacts involving earthquakes.
- b. The development of this site will require movement of topsoil. Existing City Engineering Division standards require that a grading and drainage plan be submitted for review to the City to ensure that off- and on-site improvements will be designed to meet City standards.
- c. The project area is relatively flat and the underlying soil is not known to be unstable. Soils in the Visalia area have few limitations with regard to development. Due to low clay content and limited topographic relief, soils in the Visalia area have low expansion characteristics.
- d. Due to low clay content, soils in the Visalia area have an expansion index of 0-20, which is defined as very low potential expansion.
- e. The project does not involve the use of septic tanks or alternative wastewater disposal systems since sanitary sewer lines are used for the disposal of wastewater at this location.

**VII. GREENHOUSE GAS EMISSIONS**

- a. The project is expected to generate Greenhouse Gas (GHG) emissions in the short-term as a result of the construction of residences and long-term as a result of day-to-day operation of the proposed residences.

The City has prepared and adopted a Climate Action Plan (CAP), which includes a baseline GHG emissions inventories, reduction measures, and reduction targets consistent with local and State goals. The CAP was prepared concurrently with the proposed General Plan and its impacts are also evaluated in the Visalia General Plan Update EIR.

The Visalia General Plan and the CAP both include policies intended to reduce the level of GHG emissions emitted in association with buildout conditions under the General Plan. Although emissions will be generated as a result of the project, implementation of the General Plan and CAP policies will result in fewer emissions than would be associated with a continuation of baseline conditions. Thus, the impact to GHG emissions will be less than significant.

- b. The State of California has enacted the Global Warming Solutions Act of 2006 (AB 32), which included provisions for reducing the GHG emission levels to 1990 "baseline" levels by 2020.

The proposed project will not impede the State's ability to meet the GHG emission reduction targets under AB 32. Current and probable future state and local GHG reduction measures will continue to reduce the project's contribution to climate change. As a result, the project will not contribute significantly, either individually or cumulatively, to GHG emissions.

**VIII. HAZARDS AND HAZARDOUS MATERIALS**

- a. No hazardous materials are anticipated with the project.
- b. Construction activities associated with development of the project may include maintenance of on-site construction equipment, which could lead to minor fuel and oil spills. The use and handling of any hazardous materials during construction activities would occur in accordance with applicable federal, state, regional, and local laws. Therefore, impacts are considered to be less than significant.
- c. There are no schools located within one-quarter mile from the project site. There is no reasonably foreseeable condition or incident involving the project that could affect existing or proposed school sites or areas within one-quarter mile of school sites.
- d. The project area does not include any sites listed as hazardous materials sites pursuant to Government Code Section 65692.5.
- e. The project area is not located within any airport land use plan or within 2 miles of a public airport.
- f. The project area is not within the vicinity of any private airstrip.
- g. The project will not interfere with the implementation of any adopted emergency response plan or evacuation plan.
- h. There are no wild lands within or near the project area.

**IX. HYDROLOGY AND WATER QUALITY**

- a. The project will not violate any water quality standards of waste discharge requirements. The site, when developed, will meet the City's improvement standards for directing storm water runoff to existing City storm water drainage systems; consistent with the City's adopted City Storm Drain Master Plan.
- b. The project will not substantially deplete groundwater supplies in the project vicinity. The project will be served by a water lateral for domestic, irrigation, and fire protection use. The project area overlies the southern portion of the San Joaquin unit of the Central Valley groundwater aquifer. The project will result in an increase of impervious surfaces on the project site, which might affect the amount of precipitation that is recharged to the aquifer. However, as the City of Visalia is already largely developed and covered by impervious surfaces, the increase of impervious surfaces through this project will be small by comparison. The project therefore might affect the amount of precipitation that is recharged to the aquifer. The City of Visalia's water conservation measures and explorations for surface water use over groundwater extraction will assist in offsetting the loss in groundwater recharge.

- c. Development of the site has the potential to affect drainage patterns in the short term due to erosion and sedimentation during construction activities and in the long term through the expansion of impervious surfaces. Impaired storm water runoff may then be intercepted and directed to a storm drain or water body, unless allowed to stand in a detention area. The City's existing standards may require the preparation and implementation of a Storm Water Pollution Prevention Plan (SWPPP) in accordance with the SWRCB's General Construction Permit process, which would address erosion control measures.

The Visalia General Plan contains multiple policies, identified under Impact 3.6-1 of the EIR, that together work to reduce the potential for erosion. With implementation of these policies and the existing City standards, impacts to erosion will be less than significant.

- d. Development of the site will create additional impervious surfaces. However, existing and planned improvements to storm water drainage facilities as required through the Visalia General Plan policies will reduce any potential impacts to a less than significant level.

Policies identified under Impact 3.6-2 of the EIR will reduce any potential impacts to a less than significant level. With implementation of these policies and the existing City standards, impacts to groundwater supplies will be less than significant.

- e. Development of the site will create additional impervious surfaces. However, existing and planned improvements to storm water drainage facilities as required through the Visalia General Plan policies will reduce any potential impacts to a less than significant level.

Policies identified under Impact 3.6-2 of the EIR will reduce any potential impacts to a less than significant level. With implementation of these policies and the existing City standards, impacts to groundwater supplies will be less than significant.

Furthermore, the project will be required to meet the City's improvement standards for directing storm water runoff to the existing City storm water drainage system, consistent with the City's adopted City Storm Drain Master Plan.

- f. There are no reasonably foreseeable reasons why the project would result in the degradation of water quality. As previously discussed, the Visalia General Plan contains multiple policies, identified under Impact 3.6-2 and 3.9-3 of the EIR, that together work to reduce the potential for impacts to water quality. With implementation of these policies and the existing City standards, impacts to water quality will be less than significant.
- g. The project area is located within Zone X02, which indicates an area that is outside of the 100-year flood hazard area.
- h. The project area is located within Zone X02, which indicates an area that is outside of the 100-year flood hazard area.
- i. The project would not expose people or structures to risks from failure of levee or dam. The project is located downstream from the Terminus Dam; in the case of dam failure, there will be 4 hours of warning to evacuate the site.

- j. Seiche and tsunami impacts do not occur in the Visalia area. The site is relatively flat, which will contribute to the lack of impacts by mudflow occurrence.

#### X. LAND USE AND PLANNING

- a. The project will not physically divide an established community. The General Plan Land Use Diagram, adopted October 14, 2014, designates the 1.29-acre project area as Commercial Mixed Use. The Zoning Map, adopted on April 6, 2017, designates the site as Commercial Mixed Use (C-MU), which is consistent with the General Plan Land Use Designation of Commercial Mixed Use as identified in Table 9-1 "Consistency Between the Plan and Zoning" of the General Plan. The project will not physically divide an established community. Gas stations with drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Caldwell Avenue, which is a designated arterial roadway. Staff concludes that the proposed gas station/drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this project.

The Visalia General Plan contains multiple policies, identified under Impact 3.1-2 of the EIR, that together work to reduce the potential for impacts to the development of land as designated by the General Plan. With implementation of these policies and the existing City standards, impacts to land use development consistent with the General Plan will be less than significant.

- b. The project does not conflict with any land use plan, policy or regulation of the City of Visalia. The recently adopted General Plan did not rezone or otherwise disrupt residential communities or commercial areas, and provides additional space to accommodate any potentially displaced residents or businesses.

The City's General Plan Land Use Map designates the site as Commercial Mixed Use. Staff concludes that the proposed ARCO AM/PM gas station and drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this project. The 1.29-acre parcel is part of the Cameron Creek Marketplace Shopping Center. The Cameron Creek Marketplace Shopping Center development consisted of a total building area of 106,132 square feet. The commercial center, as proposed, consisted of two large anchor pads and various retail buildings along the southern portion of the site. The remaining balance of the site includes the development of out-pads buildings with drive-thru lanes along the Caldwell Avenue street frontage. Thus, Staff finds that the development of the site with the proposed use at this time is consistent with the overall master plan.

- c. The project does not conflict with any applicable habitat conservation plan or natural community conservation plan as it is located on a vacant dirt lot with no significant natural habitat present.

#### XI. MINERAL RESOURCES

- a. No mineral areas of regional or statewide importance exist within the Visalia area.



- b. There are no mineral resource recovery sites delineated in the Visalia area.

## **XII. NOISE**

- a. The project will result in noise generation typical of urban development. The Visalia Noise Element and City Ordinance contain criterion for acceptable noise levels inside and outside residential living spaces. This standard is 65 dB DNL for outdoor activity areas associated with residences and 45 dB DNL for indoor areas.
- b. Noise Impact Analysis prepared for the proposed project (ref.: Cameron Creek Noise Impact Analysis, Kunzman Associates, Inc., April 27, 2017) has concluded that an exterior noise level in excess of the daytime 65 dB DNL standard for noise-sensitive land uses, specified in the City's Noise Element, will occur during the construction of the ARCO gas station site unless mitigated. Furthermore, in the event the measured ambient noise level without the alleged offensive source in operations exceeds an applicable noise level standard in any category, the applicable standard or standards shall be adjusted so as to equal the ambient noise level. The noise level data presented in the Noise Impact Analysis identified that ambient noise levels generally exceed the performance standards identified in the Noise Ordinance. In addition, interior noise levels, when adjusted for ambient noise levels, will be below the applicable noise standards for interior noise. To ensure that community noise standards are met for noise-sensitive land uses adjacent to the site, the Noise Analysis identifies construction practice Mitigation Measures that shall be followed. To attenuate the noise, the Construction Mitigation Measures listed on page 26 of the Noise Impact Analysis shall be implemented.

Therefore, to ensure that community noise standards are met for the proposed project, the project site shall be developed and shall operate in substantial compliance with the Mitigation Measures 1.1 through 1.7.

- c. Ground-borne vibration or ground-borne noise levels may occur as part of construction activities associated with the project. Construction activities will be temporary and will not expose persons to such vibration or noise levels for an extended period of time. Therefore, to ensure that community noise standards are met for the proposed project, the project site shall be developed and shall operate in substantial compliance with the Mitigation Measures 1.1 through 1.7.
- d. Ambient noise levels will increase beyond current levels as a result of the project, however these levels will be typical of noise levels associated with urban development and not in excess of standards established in the City of Visalia's General Plan or Noise Ordinance. Noise associated with the establishment of new urban uses was previously evaluated with the General Plan for the conversion of land to urban uses.

Furthermore, the Visalia General Plan contains multiple policies, identified under Impact N-P-3 through N-P-5, that work to reduce the potential for noise impacts to sensitive land uses. With implementation of Noise Impact Policies and existing City Standards, noise impacts to new noise sensitive lands uses would be less than significant.

- e. Noise levels will increase during the construction of the project but shall remain within the limits defined by the City of Visalia Noise Ordinance. Temporary increase in ambient noise levels is considered to be less than significant. Therefore, to ensure that community noise standards are met for the proposed project, the project site shall be developed and shall operate in substantial compliance with the Mitigation Measures 1.1 through 1.7.
- f. The project area is not within an airport land use plan, nor is it within 2 miles of a public airport.
- g. There is no private airstrip near the project area.

## **XIII. POPULATION AND HOUSING**

- a. The project will not directly induce substantial population growth that is in excess of that planned in the General Plan.
- b. Development of the site will not displace any housing on the site.
- c. Development of the site will not displace any people on the site.

## **XIV. PUBLIC SERVICES**

- a.
  - i. Fire protection facilities are located at the Visalia Station 52 can adequately serve the site without a need for alteration. Impact fees will be paid to mitigate the project's proportionate impact on these facilities.
  - ii. Current police protection facilities can adequately serve the site without a need for alteration. Impact fees will be paid to mitigate the project's proportionate impact on these facilities.
  - iii. The project will not generate new students for which existing schools in the area may accommodate. In addition, to address direct impacts, the future development of the site will be required to pay residential impact fees. These fees are considered to be conclusive mitigation for direct impacts. The project includes residential units that will create a need for park facilities.
  - iv. Other public facilities can adequately serve the site without a need for alteration.

## **XV. RECREATION**

- a. The proposed project does not include recreational facilities or require the construction or expansion of recreational facilities within the area that might have an adverse physical effect on the environment.
- b. The proposed project does not include recreational facilities or require the construction or expansion of recreational facilities within the area that might have an adverse physical effect on the environment.

## **XVI. TRANSPORTATION AND TRAFFIC**

- a. Development and operation of the project is not anticipated to conflict with applicable plans, ordinances, or policies establishing measures of effectiveness of the City's circulation system. The project will result in an increase in traffic levels on arterial and collector roadways, although the City of Visalia's Circulation Element has been prepared to address this increase in traffic.

- b. Development of the site will result in increased traffic in the area, but will not cause a substantial increase in traffic on the city's existing circulation pattern. This site was evaluated in the Visalia General Plan Update Environmental Impact Report (EIR) for urban use.
- c. The project will not result in nor require a need to change air traffic patterns.
- d. There are no planned designs that are considered hazardous.
- e. The project will not result in inadequate emergency access.
- f. The project will not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities.

**XVII. UTILITIES AND SERVICE SYSTEMS**

- a. The project will be connecting to existing City sanitary sewer lines, consistent with the City Sewer Master Plan. The Visalia wastewater treatment plant has a current rated capacity of 22 million gallons per day, but currently treats an average daily maximum month flow of 12.5 million gallons per day. With the completed project, the plant has more than sufficient capacity to accommodate impacts associated with the proposed project. The proposed project will therefore not cause significant environmental impacts.
- b. The project will not result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.
- c. There are currently stubbed storm drain lines in South West Street and West Kimball Avenue. The project, when developed, will be required to connect to the master planned on-site storm drain lines that are part of the shopping center development. The on-site master storm drain lines on-site are/shall be connected to the existing City storm water drainage lines that handle on-site and street runoff. Usage of these lines shall be consistent with the City Storm Drain Master Plan. These improvements will not cause significant environmental impacts.
- d. California Water Service Company has determined that there are sufficient water supplies to support the site, and that service can be extended to the site.
- e. The City has determined that there is adequate capacity existing to serve the site's projected wastewater treatment demands at the City wastewater treatment plant.
- f. Current solid waste disposal facilities can adequately serve the site without a need for alteration.

- g. The project will be able to meet the applicable regulations for solid waste. Removal of debris from construction will be subject to the City's waste disposal requirements.

**XVIII. TRIBAL CULTURAL RESOURCES**

The proposed project would not cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

- a. The site is not listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or
- b. The site has been determined to not be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Further, the EIR (SCH 2010041078) for the 2014 General Plan update included a thorough review of sacred lands files through the California Native American Heritage Commission. The sacred lands file did not contain any known cultural resources information for the Visalia Planning Area.


**XIX. MANDATORY FINDINGS OF SIGNIFICANCE**

- a. The project will not affect the habitat of a fish or wildlife species or a plant or animal community. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia's General Plan Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- b. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia General Plan Update for the area's conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- c. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia General Plan Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.

## DETERMINATION OF REQUIRED ENVIRONMENTAL DOCUMENT

On the basis of this initial evaluation:

- ☐ I find that the proposed project **COULD NOT** have a significant effect on the environment. **A NEGATIVE DECLARATION WILL BE PREPARED.**
- ☒ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on the attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION WILL BE PREPARED.**
- ☐ I find the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- ☐ I find that the proposed project **MAY** have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- ☐ I find that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37 adopted on October 14, 2014. **THE PROGRAM ENVIRONMENTAL IMPACT REPORT WILL BE UTILIZED.**

  
Paul Scheibel, AICP  
Environmental Coordinator

August 2, 2017  
Date





KUNZMAN ASSOCIATES, INC.

**CAMERON CREEK**

**NOISE IMPACT ANALYSIS**

**April 27, 2017**

Traffic Engineering | Transportation Planning | Parking | Noise/Vibration | Expert Witness  
Air Quality | Global Climate Change | Health Risk Assessment





# **CAMERON CREEK**

## **NOISE IMPACT ANALYSIS**

**April 27, 2017**

Prepared by:

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## **I. INTRODUCTION AND SETTING**

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### **A. Purpose and Objectives**

The purpose of this noise impact analysis is to provide an assessment of the impacts resulting from development of the proposed Cameron Creek project car wash and to identify mitigation measures that may be necessary to reduce those impacts. The noise issues related to the proposed land use and development have been evaluated in the context of the California Environmental Quality Act.

Although this is a technical report, every effort has been made to write the report clearly and concisely. To assist the reader with those terms unique to noise analysis, a glossary of terms is provided in Appendix A of this report.

### **B. Project Location**

The project site is proposed to be located at the southeast corner of the Caldwell Avenue and West Street intersection in the City of Visalia. A vicinity map showing the project location is provided on Figure 1.

### **C. Project Description**

The proposed project consists of developing the approximately 1.91 acre project site with a 6,000 square foot convenience store with fast-food restaurant, a 4,933 square foot carwash, and a 6,775 square foot canopy covering a 20 fueling position service station. The project site would also include 13,300 square feet of landscaping and 38 parking spaces. Figure 2 illustrates the site plan.



**Figure 1**  
**Project Location Map**

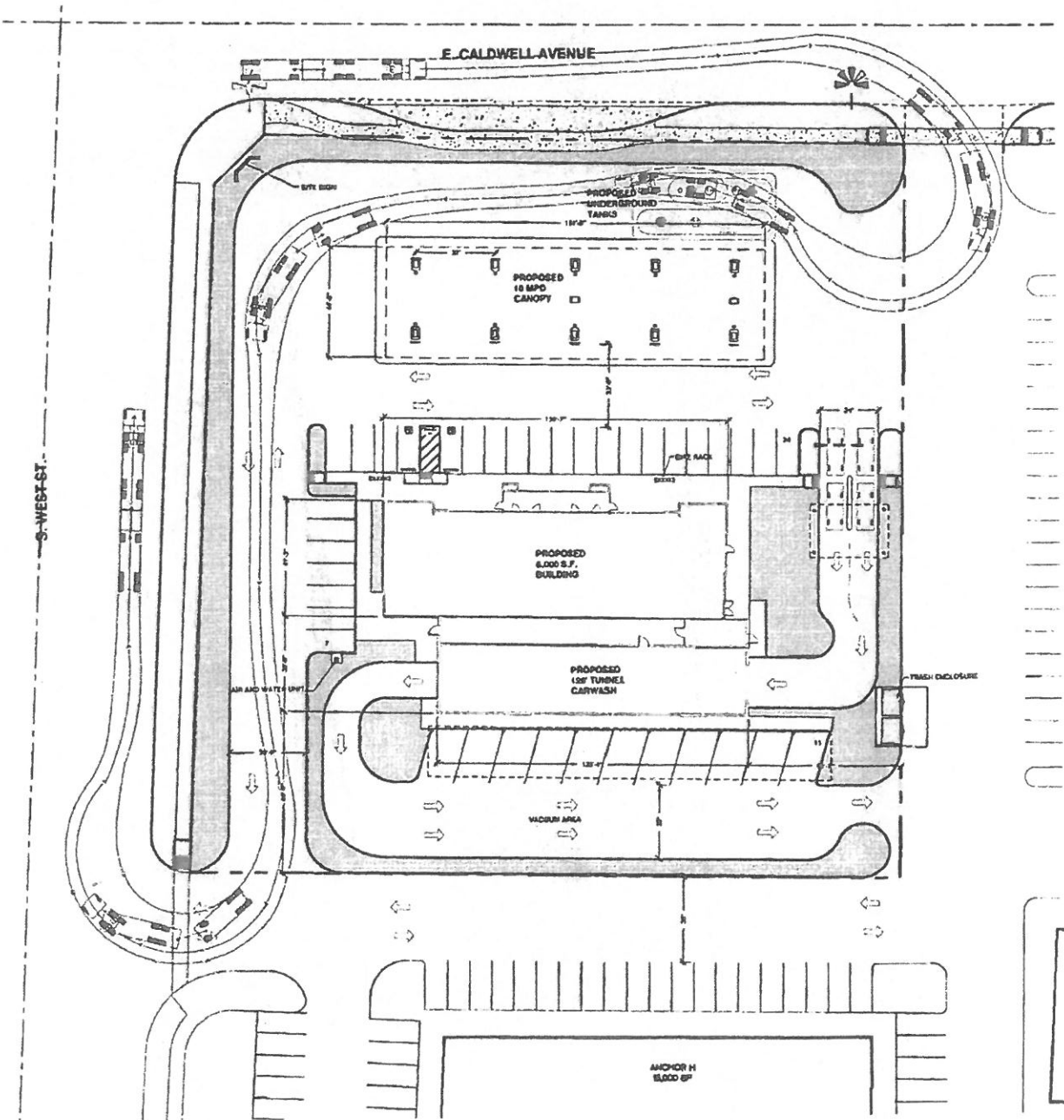


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### Figure 2 Site Plan



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## II. NOISE FUNDAMENTALS

### A. Noise Fundamentals

Sound is a pressure wave created by a moving or vibrating source that travels through an elastic medium such as air. Noise is defined as unwanted or objectionable sound. The effects of noise on people can include general annoyance, interference with speech communication, sleep disturbance, and in extreme circumstances, hearing impairment.

Commonly used noise terms are presented in Appendix B. The unit of measurement used to describe a noise level is the decibel (dB). The human ear is not equally sensitive to all frequencies within the sound spectrum. Therefore, the "A-weighted" noise scale, which weights the frequencies to which humans are sensitive, is used for measurements. Noise levels using A-weighted measurements are written dB(A) or dBA.

From the noise source to the receiver, noise changes both in level and frequency spectrum. The most obvious is the decrease in noise as the distance from the source increases. The manner in which noise reduces with distance depends on whether the source is a point or line source as well as ground absorption, atmospheric effects and refraction, and shielding by natural and manmade features. Sound from point sources, such as air conditioning condensers, radiates uniformly outward as it travels away from the source in a spherical pattern. The noise drop-off rate associated with this geometric spreading is 6 dBA per each doubling of the distance (dBA/DD). Transportation noise sources such as roadways are typically analyzed as line sources, since at any given moment the receiver may be impacted by noise from multiple vehicles at various locations along the roadway. Because of the geometry of a line source, the noise drop-off rate associated with the geometric spreading of a line source is 3 dBA/DD.

Decibels are measured on a logarithmic scale, which quantifies sound intensity in a manner similar to the Richter scale used for earthquake magnitudes. Thus, a doubling of the energy of a noise source, such as a doubled traffic volume, would increase the noise levels by 3 dBA; halving of the energy would result in a 3 dBA decrease.

Figure 3 shows the relationship of various noise levels to commonly experienced noise events.

Average noise levels over a period of minutes or hours are usually expressed as  $\text{dBA}_{\text{Leq}}$ , or the equivalent noise level for that period of time. For example,  $\text{Leq}(3)$  would represent a 3-hour average. When no period is specified, a one-hour average is assumed.

Noise standards for land use compatibility are stated in terms of the Community Noise Equivalent Level (CNEL) and the Day-Night Average Noise Level ( $L_{\text{dn}}$ ). CNEL is a 24-hour weighted average measure of community noise. CNEL is obtained by adding five decibels to sound levels in the evening (7:00 PM to 10:00 PM), and by adding ten decibels to sound levels at night (10:00 PM to 7:00 AM). This weighting accounts for the increased human sensitivity to noise during the evening and nighttime hours.  $L_{\text{dn}}$  is a very similar 24-hour average measure that weights only the nighttime hours.



It is widely accepted that the average healthy ear can barely perceive changes of 3 dBA; that a change of 5 dBA is readily perceptible, and that an increase (decrease) of 10 dBA sounds twice (half) as loud. This definition is recommended by the California Department of Transportation's Technical Noise Supplement to the Traffic Noise Analysis Protocol (2013).

## **B. Vibration Fundamentals**

The way in which vibration is transmitted through the earth is called propagation. Propagation of earthborn vibrations is complicated and difficult to predict because of the endless variations in the soil through which waves travel. There are three main types of vibration propagation: surface, compression and shear waves. Surface waves, or Raleigh waves, travel along the ground's surface. These waves carry most of their energy along an expanding circular wave front, similar to ripples produced by throwing a rock into a pool of water. Compression waves, or P-waves, are body waves that carry their energy along an expanding spherical wave front. The particle motion in these waves is longitudinal (i.e., in a "push-pull" fashion). P-waves are analogous to airborne sound waves. Shear waves, or S-waves, are also body waves that carry energy along an expanding spherical wave front. However, unlike P-waves, the particle motion is transverse or "side-to-side and perpendicular to the direction of propagation".

As vibration waves propagate from a source, the energy is spread over an ever-increasing area such that the energy level striking a given point is reduced with the distance from the energy source. This geometric spreading loss is inversely proportional to the square of the distance. Wave energy is also reduced with distance as a result of material damping in the form of internal friction, soil layering, and void spaces. The amount of attenuation provided by material damping varies with soil type and condition as well as the frequency of the wave.

Construction operations generally include a wide range of activities that can generate groundborne vibration. Vibratory compactors or rollers, pile drivers, and pavement breakers can generate perceptible amounts of vibration at up to 200 feet. Heavy trucks can also generate groundborne vibrations, which can vary depending on vehicle type, weight, and pavement conditions. Potholes, pavement joints, discontinuities, or the differential settlement of pavement all increase the vibration levels from vehicles passing over a road surface. Construction vibration is normally of greater concern than vibration from normal traffic flows on streets and freeways with smooth pavement conditions.

Typically, particle velocity or acceleration (measured in gravities) is used to describe vibration. Table 1 shows the peak particle velocities (PPV) of some common construction equipment and Table 2 shows typical human reactions to various levels of PPV as well as the effect of PPV on buildings.

**Table 1****Vibration Source Levels for Construction Equipment<sup>1</sup>**

Equipment	Peak Particle Velocity (inches/second) at 25 feet	Approximate Vibration Level LV (dVB) at 25 feet
Pile driver (impact)	1.518 (upper range)	112
	0.644 (typical)	104
Pile driver (sonic)	0.734 upper range	105
	0.170 typical	93
Clam shovel drop (slurry wall)	0.202	94
Hydromill	0.008 in soil	66
(slurry wall)	0.017 in rock	75
Vibratory Roller	0.21	94
Hoe Ram	0.089	87
Large bulldozer	0.089	87
Caisson drill	0.089	87
Loaded trucks	0.076	86
Jackhammer	0.035	79
Small bulldozer	0.003	58

<sup>1</sup> Source: Transit Noise and Vibration Impact Assessment, Federal Transit Administration, May 2006.

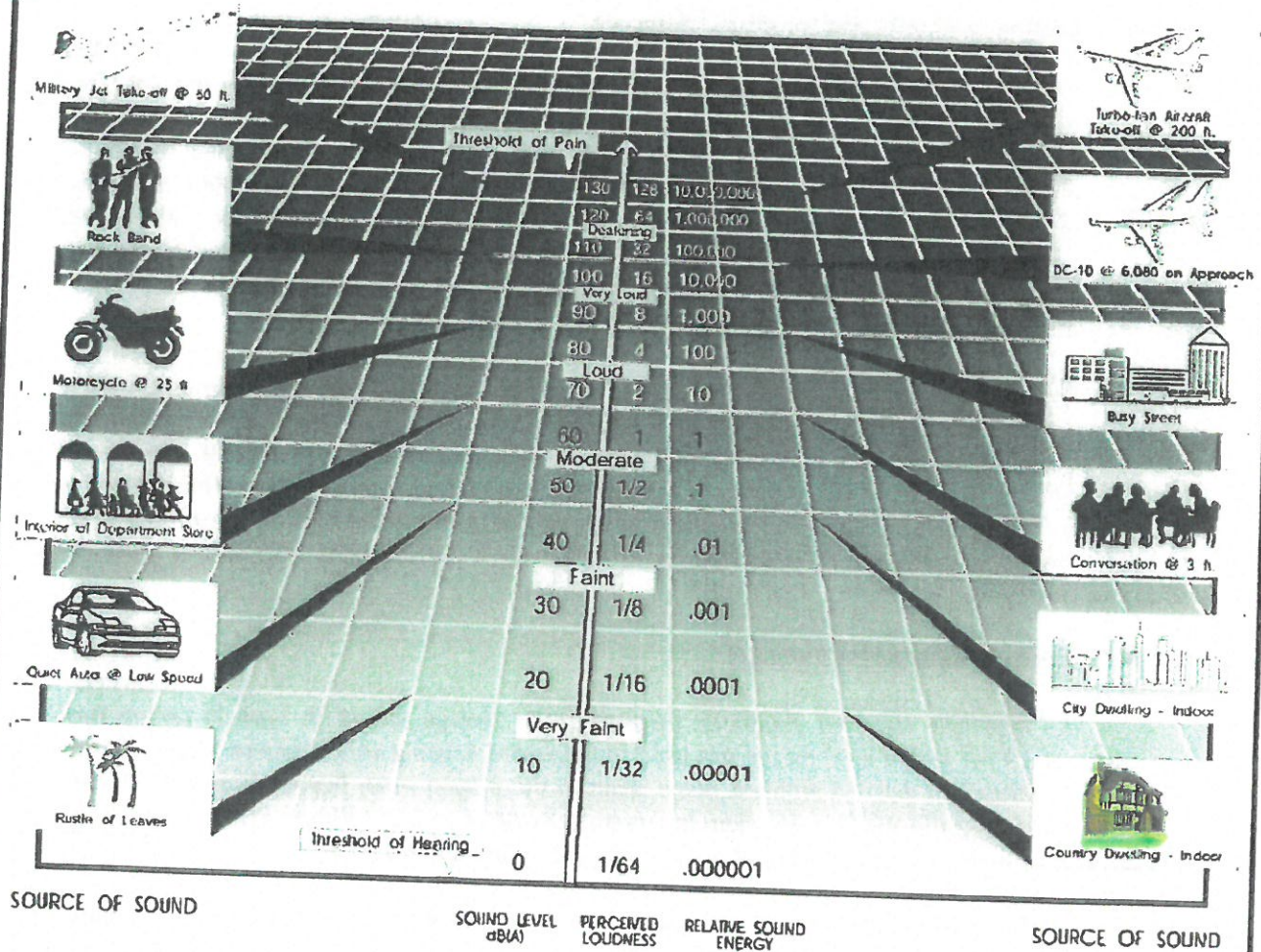
**Table 2****Typical Human Reaction and Effect on Buildings Due to Groundborne Vibration<sup>1</sup>**

Vibration Level Peak Particle Velocity (PPV)	Human Reaction	Effect on Buildings
0.006–0.019 in/sec	Threshold of perception, possibility of intrusion	Vibrations unlikely to cause damage of any type
0.08 in/sec	Vibrations readily perceptible	Recommended upper level of vibration to which ruins and ancient monuments should be subjected
0.10 in/sec	Level at which continuous vibration begins to annoy people	Virtually no risk of “architectural” (i.e., not structural) damage to normal buildings
0.20 in/sec	Vibrations annoying to people in buildings	Threshold at which there is a risk to “architectural” damage to normal dwelling – houses with plastered walls and ceilings
0.4–0.6 in/sec	Vibrations considered unpleasant by people subjected to continuous vibrations and unacceptable to some people walking on bridges	Vibrations at a greater level than normally expected from traffic, but would cause “architectural” damage and possibly minor structural damage

<sup>1</sup> Source: California Department of Transportation, 2002.



**Figure 3**  
**Common Noise Sources and Noise Levels**



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### **III. EXISTING NOISE ENVIRONMENT**

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#### **A. Existing Land Uses and Sensitive Receptors**

The project site is bordered by Caldwell Avenue to the north, West Street to the west, and vacant land to the south and east. Land uses surrounding the site consist of vacant land, single-family detached residential dwelling units, and multi-family attached residential dwelling units to the south (across Kimball Avenue), single-family detached residential dwelling units to the west (across West Street), commercial uses to the north (across Caldwell Avenue), and vacant land to the east. Noise sensitive land uses include the single-family detached residential dwelling units to the west and south and the multi-family attached residential dwelling units to the south and northeast.

The State of California defines sensitive receptors as those land uses that require serenity or are otherwise adversely affected by noise events or conditions. Schools, libraries, churches, hospitals, single and multiple-family residential, including transient lodging, motels and hotel uses make up the majority of these areas. Sensitive receptors that may be affected by project generated noise include the single-family detached residential dwelling units to the west and south and the multi-family attached residential dwelling units to the south and northeast.

#### **B. Ambient Noise Measurements**

An American National Standards Institute (ANSI Section S14 1979, Type 1) Larson Davis model LxT sound level meter was used to document existing ambient noise levels. In order to document existing ambient noise levels in the project area, five (5) 10-minute daytime noise measurements were taken between 11:22 AM and 12:37 PM on April 17, 2017. Field worksheets and measurement output data are included in Appendix C.

As shown on Figure 4, the noise measurements were taken near existing sensitive receptors along Kimball Avenue and West Street. Table 3 provides a summary of the short-term ambient noise data. Ambient noise levels measured between 51.8 and 64.1 dBA  $L_{eq}$  during the daytime (7:00 AM to 10:00 PM). Estimated ambient nighttime noise levels are anticipated to decrease by 5 dBA to between 46.8 and 59.1 dBA  $L_{eq}$  during nighttime hours. The dominant noise sources were from vehicles traveling on Kimball Avenue and West Street.

**Table 3**

**Short-Term Noise Measurement Summary (dBA)<sup>1,2</sup>**

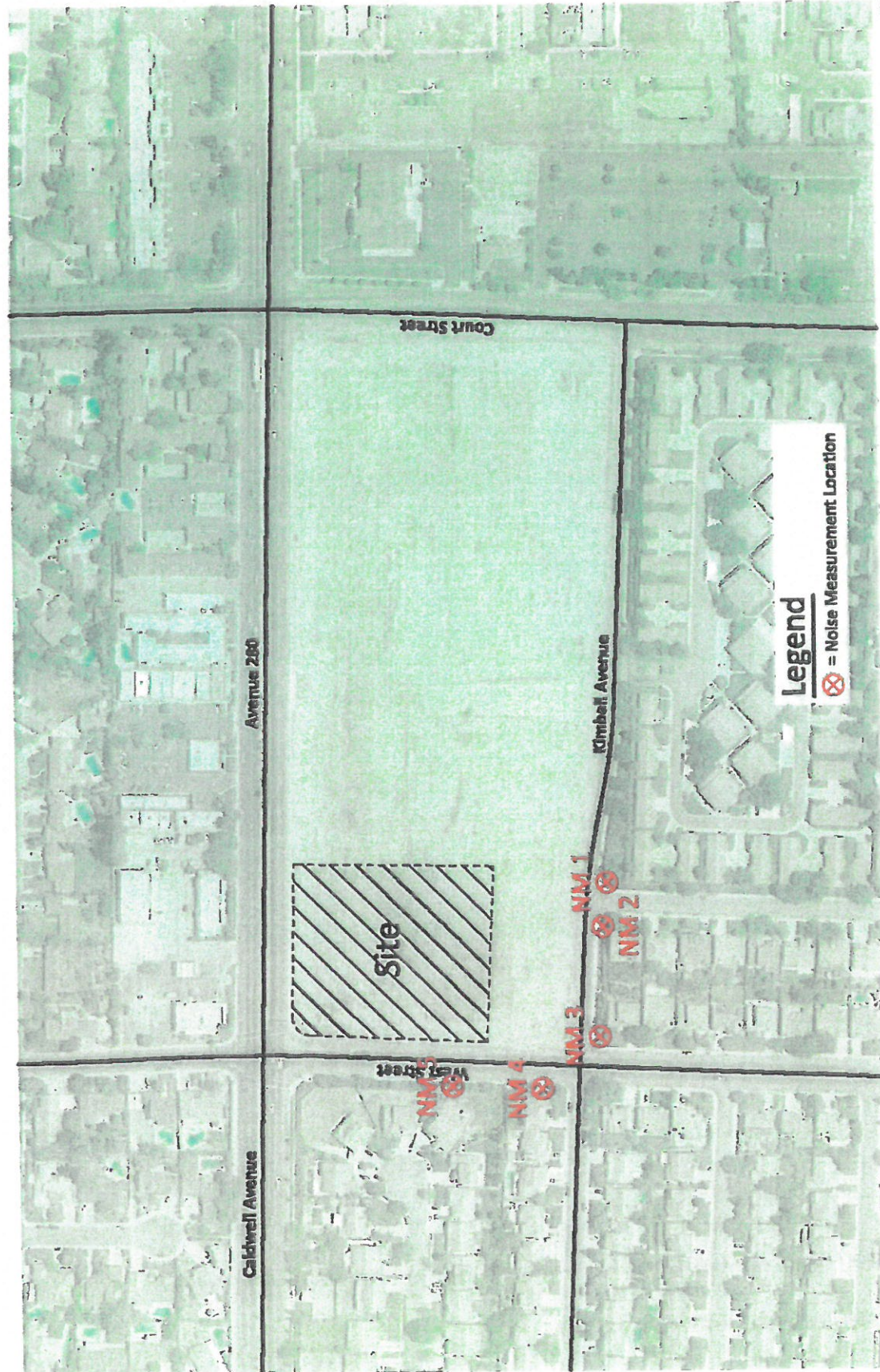
Site Location	Time Started	Leq	Lmax
NM1	11:22 AM	51.8	71.8
NM2	11:38 AM	59.5	72.8
NM3	11:54 AM	64.1	79.3
NM4	12:09 PM	62.8	77.3
NM5	12:37 PM	62.8	77.8

<sup>1</sup> See Figure 4 for noise measurement locations. Each noise measurement was performed over a 10-minute duration.

<sup>2</sup> Noise measurements performed on February 28, 2017.



**Figure 4**  
**Noise Measurement Location Map**



## **IV. REGULATORY SETTING**

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### **A. Federal Regulations**

#### **1. Federal Noise Control Act of 1972**

The U.S. Environmental Protection Agency (EPA) Office of Noise Abatement and Control was originally established to coordinate federal noise control activities. After its inception, EPA's Office of Noise Abatement and Control issued the Federal Noise Control Act of 1972, establishing programs and guidelines to identify and address the effects of noise on public health, welfare, and the environment. In response, the EPA published Information on Levels of Environmental Noise Requisite to Protect Public Health and Welfare with an Adequate Margin of Safety (Levels of Environmental Noise). The Levels of Environmental Noise recommended that the Ldn should not exceed 55 dBA outdoors or 45 dBA indoors to prevent significant activity interference and annoyance in noise-sensitive areas.

In addition, the Levels of Environmental Noise identified five (5) dBA as an "adequate margin of safety" for a noise level increase relative to a baseline noise exposure level of 55 dBA Ldn (i.e., there would not be a noticeable increase in adverse community reaction with an increase of five dBA or less from this baseline level). The EPA did not promote these findings as universal standards or regulatory goals with mandatory applicability to all communities, but rather as advisory exposure levels below which there would be no risk to a community from any health or welfare effect of noise.

In 1981, EPA administrators determined that subjective issues such as noise would be better addressed at lower levels of government. Consequently, in 1982 responsibilities for regulating noise control policies were transferred to State and local governments. However, noise control guidelines and regulations contained in EPA rulings in prior years remain in place by designated Federal agencies, allowing more individualized control for specific issues by designated Federal, State, and local government agencies.

### **B. State Regulations**

#### **1. State of California General Plan Guidelines 2003**

Though not adopted by law, the State of California General Plan Guidelines 2003, published by the California Governor's Office of Planning and Research (OPR) (OPR Guidelines), provide guidance for the compatibility of projects within areas of specific noise exposure. The OPR Guidelines identify the suitability of various types of construction relative to a range of outdoor noise levels and provide each local community some flexibility in setting local noise standards that allow for the variability in community preferences. Findings presented in the Levels of Environmental Noise Document (EPA 1974) influenced the recommendations of the OPR Guidelines, most importantly in the choice of noise exposure metrics (i.e., Ldn or CNEL) and in the upper limits for the Normally Acceptable outdoor exposure of noise-



sensitive uses. The City of Visalia has adopted General Plan objectives and policies to ensure land use compatibility.

**C. Local Regulations**

**1. City of Visalia General Plan**

Applicable policies and standards governing environmental noise in the City are set forth in the General Plan Safety and Noise Element. The applicable objective and policy are presented below:

***Objective***

**N-O-1** Strive to achieve an acceptable noise environment for present and future residents of Visalia.

***Policy***

**N-P-4** Where new development of industrial, commercial or other noise generating land uses (including roadways, railroads, and airports) may result in noise levels that exceed applicable noise level exposure criteria established in Section 8.36 of the Municipal Code, require a noise study to determine impacts, and require developers to mitigate these impacts in conformance with Municipal Code as a condition of permit approval through appropriate means.

**2. City of Visalia Municipal Ordinance**

**Section 8.36.040. Exterior noise standards - fixed noise sources.** It is unlawful for any person at any location within the city to create any noise, or to allow the creation of any noise, on property owned, leased, occupied or otherwise controlled by such person which causes the exterior noise level, when measured at the property line of any affected noise sensitive land use, to exceed any of the categorical noise level standards as set forth in Table 4.

**Section 8.36.060. Residential interior noise standards.** It is unlawful for any person, at any location within the city, to operate or cause to be operated, any source of sound or to allow the creation of any noise which causes the noise level when measured inside a dwelling unit to exceed any of the categorized noise level standards as set forth in Table 5. These standards are presented as Lns or the noise level exceeded for a certain period of time. For example, the standard prohibiting the noise level from exceeding 50 dB for more than 30 minutes during any one-hour time period can be written as a standard of 50 dBA Leq(<sub>50</sub>), because thirty minutes is equal to fifty percent of one-hour. If project operations do not exceed the L<sub>50</sub> or the L<sub>max</sub>, then it is highly unlikely that they will exceed the noise level standards in between.

**Section 8.36.070. Noise source exemptions.** Noise sources associated with the collection of waste or garbage from commercially zoned or industrially zoned property by the city or its authorized franchisee shall be exempted from the provisions of this chapter.



**Table 4**

**Exterior Noise Level Standards, dBA**

Category	Cumulative Number of Minutes in any One-Hour Time Period	Evening and Daytime 6:00 AM to 7:00 PM	Nighttime 7:00 PM to 6:00 AM
1	30	50	45
2	15	55	50
3	5	60	55
4	1	65	60
5	0	70	65

**Notes:**

- A. It is unlawful for any person, at any location within the city, to operate or cause to be operated, any source of sound or to allow the creation of any noise which causes the noise level, when measured at the property line of any affected noise sensitive land use, to exceed any of the categorical noise level standards as set forth in the following table:
- B. In the event the measured ambient noise level without the alleged offensive source in operation exceeds an applicable noise level standard in any category above, the applicable standard or standards shall be adjusted so as to equal the ambient noise level. **The noise standards above have been adjusted per measured ambient noise levels. The adjusted noise standards are shown in Table 6.**
- C. Each of the noise level standards specified above shall be reduced by five dB for pure tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises.
- D. If the intruding noise source is continuous and cannot reasonably be discontinued or stopped for a time period whereby the ambient noise level without the source can be measured, the noise level measured while the source is in operation shall be compared directly to the noise level standards.

**Table 5****Interior Noise Level Standards, dBA**

Category	Cumulative Number of Minutes in any One-Hour Time Period	Evening and Daytime 6:00 AM to 7:00 PM	Nighttime 7:00 PM to 6:00 AM
1	5	45	35
2	1	50	40
3	0	55	45

Notes:

A. It is unlawful for any person, at any location within the city, to operate or cause to be operated, any source of sound or to allow the creation of any noise which causes the noise level when measured inside a dwelling unit to exceed any of the categorized noise level standards as set forth in the following table:

B. In the event the measured ambient noise level without the alleged offensive source in operation exceeds an applicable noise level standard in any category above, the applicable standard or standards shall be adjusted so as to equal the ambient noise level.

C. Each of the noise level standards specified above shall be reduced by five dB for pure tone noises, noises consisting primarily of speech or music, or four recurring impulsive noises.

D. If the intruding noise source is continuous and cannot reasonably be discontinued or stopped for a time period whereby the ambient noise level without the source can be measured, the noise level measured while the source is in operation shall be compared directly to the noise level standards.

E. If the source of noise is of an impulse or voluntary nature such as shouting, loud conversation, playing or operation of audio and video equipment and is audible in a dwelling unit, the noise level in the above table shall conclusively be determined to be exceeded

## **V. ANALYTICAL METHODOLOGY AND MODEL PARAMETERS**

### **A. Noise Modeling and Input**

#### **1. Road Construction Model**

A worst-case construction noise scenario was modeled using a version of the Federal Highway Administration's Roadway Construction Noise Model (RCNM). RCNM utilizes standard noise emission levels for many different types of equipment and includes utilization percentage, impact, and shielding parameters. Noise modeling input parameters and output are provided in Appendix D.

#### **2. SoundPLAN**

SoundPLAN acoustical modeling software was utilized to model project operational worst-case stationary noise impacts from the proposed project to adjacent sensitive uses (e.g., residences). SoundPLAN is capable of evaluating stationary noise sources (e.g., parking lots, drive-thru menus, car wash equipment, vacuums, etc.) and much more. The SoundPLAN software utilizes algorithms (based on the inverse square law) to calculate noise level projections. The software allows the user to input specific noise sources, spectral content, sound barriers, building placement, topography, and sensitive receptor locations. Noise modeling input and outputs assumptions are indicated in Appendix E.



## VI. IMPACT ANALYSIS

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### A. Noise Impacts

This impact discussion analyzes the potential for project construction noise and operational noise to cause an exposure of person to or generation of noise levels in excess of established City of Visalia noise standards related to construction noise, transportation, and stationary related noise impacts caused by the proposed project.

#### 1. Construction Noise

Construction noise is considered a short-term impact and would be considered significant if construction activities are undertaken outside the allowable times as described by the City's Municipal Code Section 8.36.050. Existing single-family detached residential dwelling units to the west and south and multi-family attached residential dwelling units to the south and northeast may be temporarily affected by short-term noise impacts associated with the transport of workers, the movement of construction materials to and from the project site, ground clearing, excavation, grading, and building activities. The noise analysis reviews the construction noise levels during the various phases of the project.

Typical noise sources and noise levels associated with the site grading phase of construction are shown in Table 2. Site grading is expected to produce the highest sustained construction noise levels. Typical operating cycles for these types of construction equipment may involve one or two minutes of full power operation followed by three to four minutes at lower power settings. A likely worst-case construction noise scenario during grading assumes the use of a grader, a dozer, a water truck (modeled as a dump truck), a backhoe, and an excavator operating between 125 and 300 feet from the nearest sensitive receptors (single-family detached residential dwelling units to the west). Assuming a usage factor of 40 percent for each piece of equipment, unmitigated noise levels have the potential to reach 74.5 dBA  $L_{eq}$  and 77.0 dBA  $L_{max}$  at the nearest sensitive receptor during grading. RCNM worksheets are provided as Appendix D.

Section 8.36.050 of the City of Visalia Municipal Ordinance limits construction to between the hours of 6:00 AM and 7:00 PM during the weekday and between the hours of 9:00 AM and 7:00 PM during the weekend. Construction is anticipated to occur during the permissible hours according to the City's Municipal Code. Construction noise will have a temporary or periodic increase in the ambient noise levels above the existing within the project vicinity. As stated earlier, any construction activities that occur outside the allowable time would be considered significant. Adherence to these hours for construction activities, and implementation of the measures presented in Section VII of this report, will reduce construction noise impacts.

2. Noise Impacts to Off-site Receptors Due to On-Site Operational Noise

Sensitive receptors that may be affected by project operational noise include the single-family detached residential dwelling units to the north and west and the multi-family attached residential dwelling units to the south and northeast. The worst-case stationary noise was modeled utilizing the SoundPLAN model. Modeled noise sources include parking lot noise, car wash noise, and vacuum noise.

Parking Lot Noise

Parking lot noise typically includes periods of conversation, doors slamming, engines starting and stopping and vehicle passage. Parking lot noise varies depending upon size and type of the proposed land use. These activities can generate noise levels between 49 dBA (tire squeals) and 74 dBA (car alarms) at a distance of 50 feet. The number of parking movements associated with peak hour trip generation was input to SoundPLAN. The software utilizes this information to calculate the projected  $L_{eq}$  from each parking area. Per the San Diego Association of Governments Brief Guide on Vehicular Traffic Generation Rates (SANDAG 2002), a car wash is expected to generate an average of approximately 900 vehicle trips per day. Furthermore, peak hour trips are expected to reach approximately 81 vehicle trips. The proposed project's parking lot was modeled with 38 total parking spaces during the peak hour.

Gas Pump Noise

Newer gas pumps include video screens and speakers to entertain customers while they pump gas. The gas pump area was modeled as an area source with a sound level of 65 dBA. The area source calculation assumes that there is a point source in every square meter.

Car Wash Noise

Car wash related noise sources, including equipment near the entrance and exit of the car wash and vacuum hose ends and parking movements, were modeled in SoundPLAN. Acoustically significant noise sources were modeled within the equipment room and within the car wash tunnel. The following is a list of equipment and noise source levels provided by Belanger Inc. (the car wash retailer), as they were modeled in SoundPLAN:

Equipment	dBA at 3 feet
<u>Enclosed within the Building</u>	
Prep Jet Pump	83
DF10 Pump	88
DF15 Pump	88
Air Compressor	87
Pneumatic Panels	88
<u>Car Wash Tunnel</u>	
Prep Jet Pump	70
DF10 Pump	75

DF15 Pump	75
Air Compressor	67
Pneumatic Panels	73
AirCannon Dryers	99
Durascrubber	67
Buff'N Dry	69
Topwheel	69
Gyro's	71
Lowsides	69
FullSides	69
Vacuum hose ends	65
Eurovac	94

#### Combined Noise at Sensitive Receptors

A total of eleven (11) sensitive receptors were modeled to accurately evaluate the proposed project's operational noise impact. A receptor is denoted by a yellow dot.

Figure 5 shows the future unmitigated operational noise levels at backyards and nearby sensitive receptors. They are expected range between 48.1 to 67.1 dBA Leq at the nearest sensitive receptors. Figure 6 shows how unmitigated operational noise will propagate. Table 6 shows how the project is not expected to exceed City Municipal Code standards after they are adjusted to ambient noise level measurement per 8.36.040(B). No mitigation is required for operations that occur between 6:00 AM and 7:00 PM.

Operation of the car wash between the hours of 7:00 PM and 6:00 AM will result in a violation of the noise ordinance. Due to the loudness of car wash equipment, a wall of a reasonable height (less than 13 feet) would not suffice to bring operational noise levels down to 45 dB. However, it may be in the best interest of nearby residents to allow an adjustment to the hours of operation from 6:00 AM to 7:00 PM to 8:00 AM to 9:00 PM as these hours may better represent waking hours at this location. This, however, would be up to the discretion of the City.

Normal residential building construction provides 20 dB with windows closed. Interior noise levels at nearby sensitive receptors are not expected to exceed 45 dBA Leq<sub>(50)</sub>.

As stated previously, parking lot noise can generate noise levels between 49 dBA (tire squeals) and 74 dBA (car alarms) at a distance of 50 feet. Figure 7 shows that noise levels associated a car alarm operating in the parking area closest to the nearest sensitive receptors would not exceed the City's maximum allowed noise level (70 dBA).

#### **B. Vibration Impacts**

This impact discussion analyzes the potential for the proposed project to cause an exposure of persons to or generation of excessive groundborne vibration or groundborne noise



levels. Vibration levels in the project area may be influenced by construction. A vibration impact would generally be considered significant if it involves any construction-related or operations-related impacts in excess of 0.2 +inches per second (in/sec) PPV.

1. Construction Vibration

Construction activity can result in varying degrees of ground vibration, depending on the equipment used on the site. Operation of construction equipment causes ground vibrations that spread through the ground and diminish in strength with distance. Buildings respond to these vibrations with varying results ranging from no perceptible effects at the low levels to slight damage at the highest levels. Table 2 gives approximate vibration levels for particular construction activities. This data provides a reasonable estimate for a wide range of soil conditions.

The nearest existing structure to the project site is located approximately 105 feet to the west of the project site.

As shown in

Table 2, the threshold at which there may be a risk of architectural damage to normal houses with plastered walls and ceilings is 0.20 PPV in/second. Primary sources of vibration during construction would be bulldozers and vibratory rollers. As shown in Table 2, a large bulldozer could produce up to 0.089 PPV at 25 feet and a vibratory roller could produce up to 0.210 at 25 feet.

At a distance of 105 feet a bulldozer would yield a worst-case 0.0103 PPV (in/sec) which is well below the threshold of perception and below any risk or architectural damage. Temporary vibration levels associated with project construction would be less than significant.

Table 6

## Adjusted Daytime Exterior Noise Level Standards and Project Impact Evaluation (dBA)

Category	Receptors	NM Location	Cumulative Number of minutes in any one-hour time period	Evening and Daytime (6:00 AM to 7:00 PM) Noise Standard	Adjusted Evening and Daytime (6:00 AM to 7:00 PM) Noise Standard	Operational Noise Level at Sensitive Receptor	Exceeds Standard?
1	n/a	n/a	30	50	n/a	n/a	n/a
	1	4			62.8	53.4	No
	2	5			62.8	61.2	No
	3	5			62.8	59.9	No
	4	5			62.8	57.0	No
	5	4			63.8	53.4	No
	6	3			64.1	50.7	No
	7	2			59.5	50.8	No
	8	1			51.8	50.0	No
	9	1			51.8	48.4	No
	10	5			62.8	56.9	No
	11	5			62.8	54.6	No



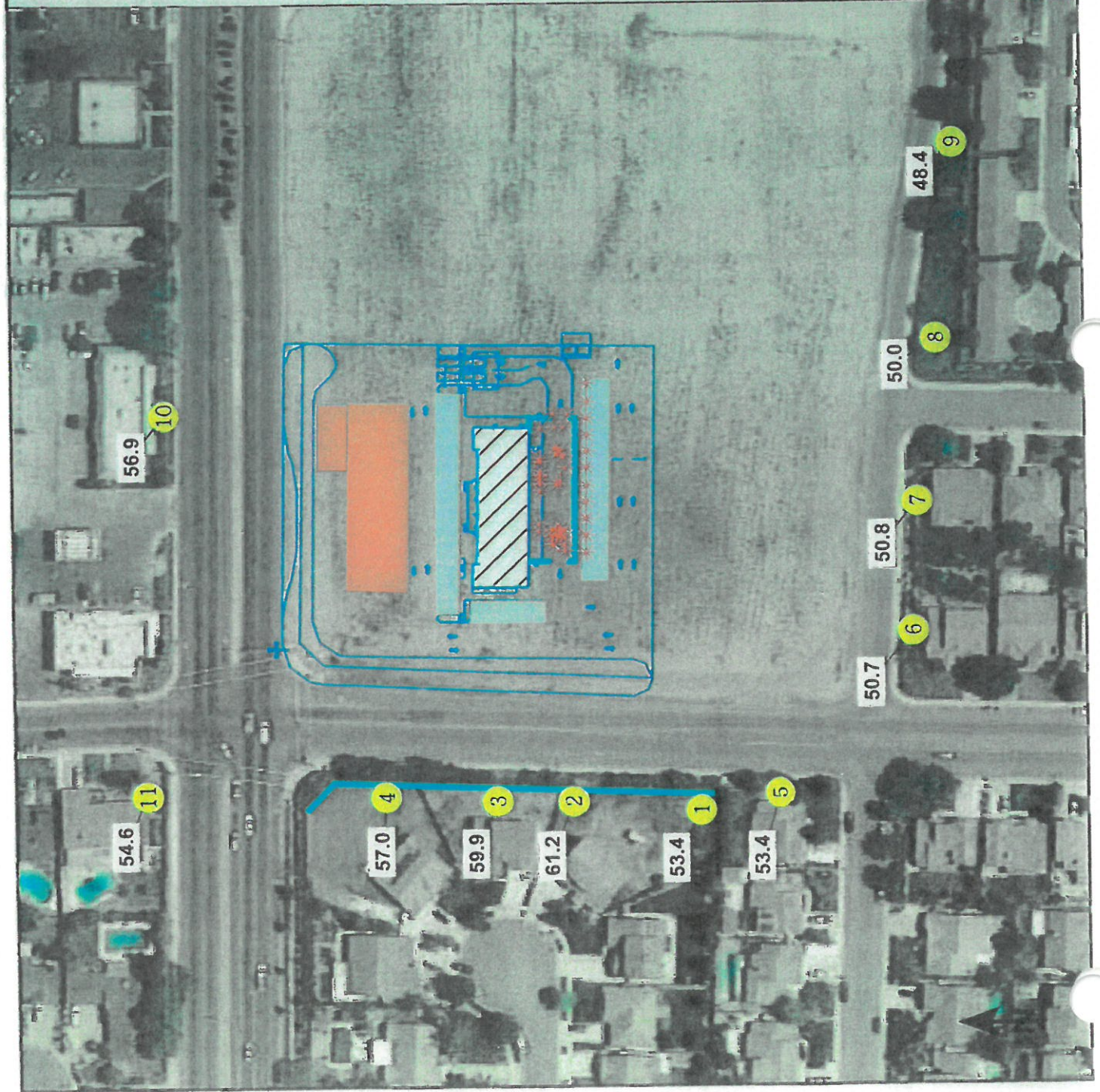


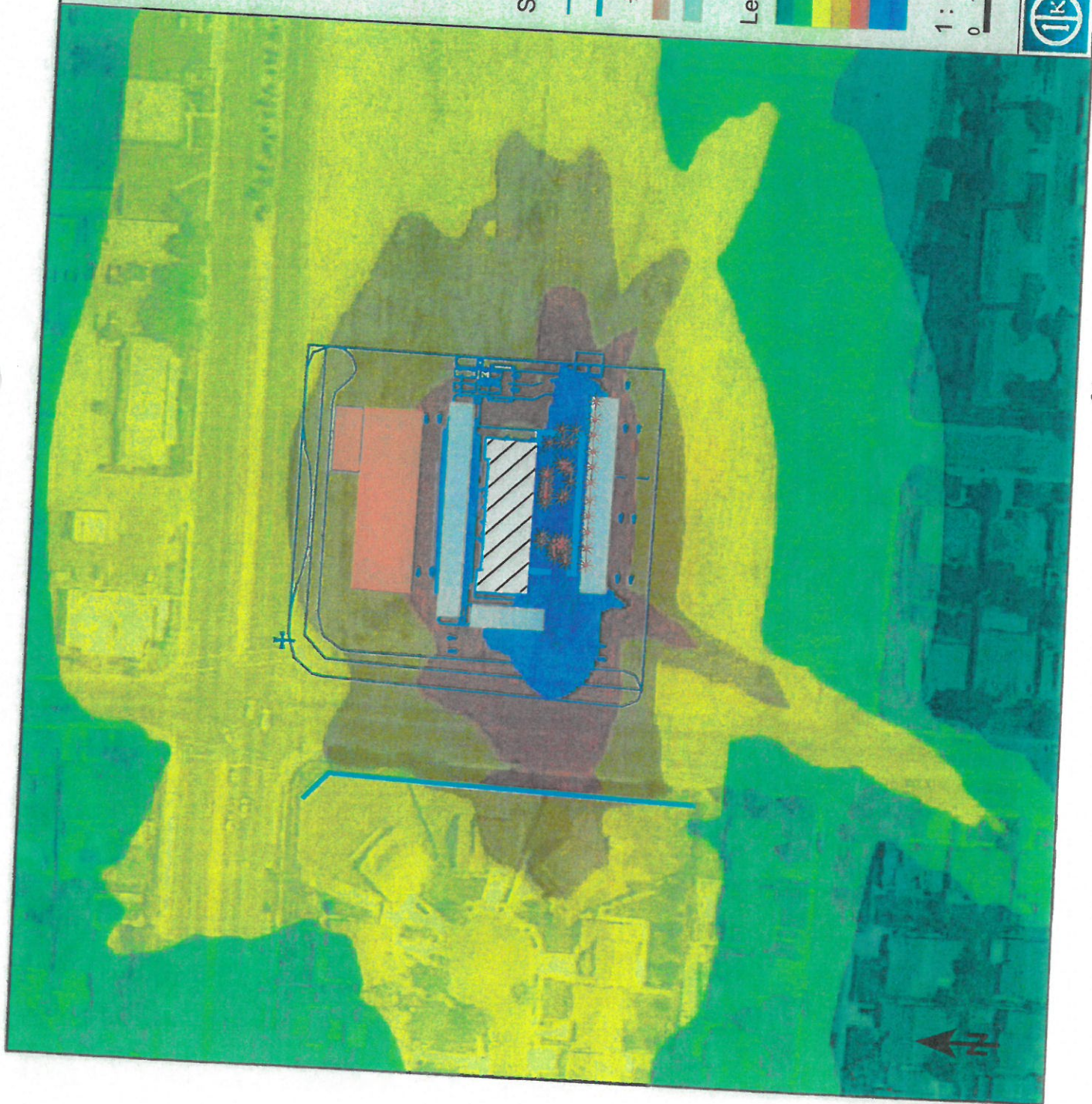
Figure 5

Operational Noise Levels (Leq)

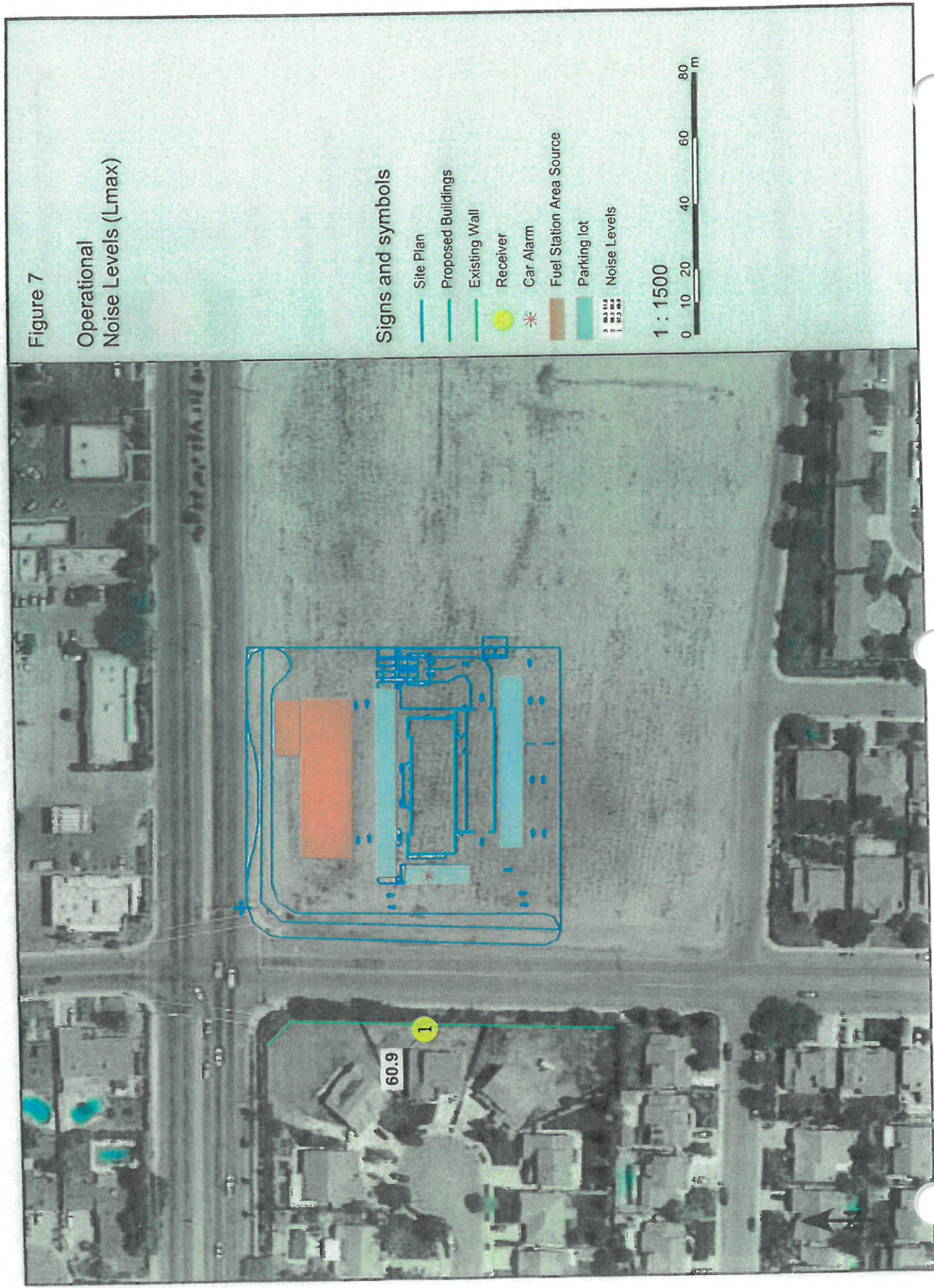


Figure 6

Operational Noise Level Contours  
(Leq)









## **VII. MEASURES TO REDUCE IMPACTS**

### **A. Construction Mitigation Measures**

In addition to adherence to the City of Visalia's policies found in the Safety and Noise Element and Municipal Code limiting the construction hours of operation, the following measures are recommended to reduce construction noise emanating from the proposed project:

1. During all project site excavation and grading on-site, construction contractors shall equip all construction equipment, fixed or mobile, with properly operating and maintained mufflers, consistent with manufacturer standards.
2. The contractor shall place all stationary construction equipment so that emitted noise is directed away from the noise sensitive receptors nearest the project site.
3. Equipment shall be shut off and not left to idle when not in use.
4. The contractor shall locate equipment staging in areas that will create the greatest distance between construction-related noise sources and sensitive receptors nearest the project site during all project construction.
5. The project proponent shall mandate that the construction contractor prohibit the use of music or sound amplification on the project site during construction.
6. The construction contractor shall limit haul truck deliveries to the same hours specified for construction equipment.

## **APPENDICES**

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**Appendix A – List of Acronyms**

**Appendix B – Definitions of Acoustical Terms**

**Appendix C – Noise Monitoring Field Worksheets**

**Appendix D – RCNM Noise Modeling Output**

**Appendix E – SoundPLAN Input and Output**

**APPENDIX A**

**List of Acronyms**



Term	Definition
ADT	Average Daily Traffic
ANSI	American National Standard Institute
CEQA	California Environmental Quality Act
CNEL	Community Noise Equivalent Level
D/E/N	Day / Evening / Night
dB	Decibel
dBA or dB(A)	Decibel "A-Weighted"
dBA/DD	Decibel per Double Distance
dBA $L_{eq}$	Average Noise Level over a Period of Time
EPA	Environmental Protection Agency
FHWA	Federal Highway Administration
$L_{02}, L_{08}, L_{50}, L_{90}$	A-weighted Noise Levels at 2 percent, 8 percent, 50 percent, and 90 percent, respectively, of the time period
$L_{dn}$	Day-Night Average Noise Level
$L_{eq(x)}$	Equivalent Noise Level for "x" period of time
$L_{eq}$	Equivalent Noise Level
$L_{max}$	Maximum Level of Noise (measured using a sound level meter)
$L_{min}$	Minimum Level of Noise (measured using a sound level meter)
LOS C	Level of Service C
OPR	California Governor's Office of Planning and Research
PPV	Peak Particle Velocities
RCNM	Road Construction Noise Model
REMEL	Reference Energy Mean Emission Level
RMS	Root Mean Square

## **APPENDIX B**

### **Definitions of Acoustical Terms**

Term	Definition
Decibel, dB	A logarithmic unit of noise level measurement that relates the energy of a noise source to that of a constant reference level; the number of decibels is 10 times the logarithm (to the base 10) of this ratio.
Frequency, Hertz	In a function periodic in time, the number of times that the quantity repeats itself in one second (i.e., the number of cycles per second).
A-Weighted Sound Level, dBA	The sound level obtained by use of A-weighting. The A-weighting filter de-emphasizes the very low and very high frequency components of the sound in a manner similar to the frequency response of the human ear.
Root Mean Square (RMS)	A measure of the magnitude of a varying noise source quantity. The name derives from the calculation of the square root of the mean of the squares of the values. It can be calculated from either a series of lone values or a continuous varying function.
Fast/Slow Meter Response	The fast and slow meter responses are different settings on a sound level meter. The fast response setting takes a measurement every 100 milliseconds, while a slow setting takes one every second.
$L_{02}$ , $L_{08}$ , $L_{50}$ , $L_{90}$	The A-weighted noise levels that are equaled or exceeded by a fluctuating sound level, 2 percent, 8 percent, 50 percent, and 90 percent of a stated time period, respectively.
Equivalent Continuous Noise Level, $L_{eq}$	A level of steady state sound that in a stated time period, and a stated location, has the same A-weighted sound energy as the time-varying sound.
$L_{max}$ , $L_{min}$	$L_{max}$ is the RMS (root mean squared) maximum level of a noise source or environment measured on a sound level meter, during a designated time interval, using fast meter response. $L_{min}$ is the minimum level.
Ambient Noise Level	The all-encompassing noise environment associated with a given environment, at a specified time, usually a composite of sound from many sources, at many directions, near and far, in which usually no particular sound is dominant.
Offensive/Offending/Intrusive Noise	The noise that intrudes over and above the existing ambient noise at a given location. The relative intrusiveness of sound depends on its amplitude, duration, frequency, and time of occurrence, and tonal information content as well as the prevailing ambient noise level.



**APPENDIX C**

**Noise Monitoring Field Worksheets**

Noise Measurement  
Field Data

Project Name: Visalia CA Site 1 Date: 17 April 2017

Project #: 6939

Noise Measurement #: NM1 3099 LxT\_Data173 .xlsx Technician: Ian Edward Gallagher

Nearest Address or Cross Street: S Watson Street & W Kimball Ave

Site Description (Type of Existing Land Use and any other notable features): Empty lot . Surrounding : Residential / commercial

Weather: Cloudy, overcast with thin cloud, veiled sunshine Settings: SLOW FAST (Circle one)

Temperature: 73 deg F Wind: Calm - 3mph Humidity: 50% Terrain: FLAT

Start Time: 11:22 AM End Time: 11:32 AM Run Time: 10mins

Leq: 51.8 dB Primary Noise Source: Traffic, occasional vehicle along W Kimball Ave

Lmax: 71.8 dB

L2: 61.3 dB Secondary Noise Sources: Traffic ambience along S West Street, West Caldwell Ave & S Court Street

L8: 51.7 dB Overhead aircraft, bird song, residential ambience

L25: 47.1 dB

L50: 45.1 dB

NOISE METER: SoundTrack LxT Class 1 CALIBRATOR: Larson Davis CAL250 Acoustic Calibrator

MAKE: Larson Davis MAKE: Larson Davis

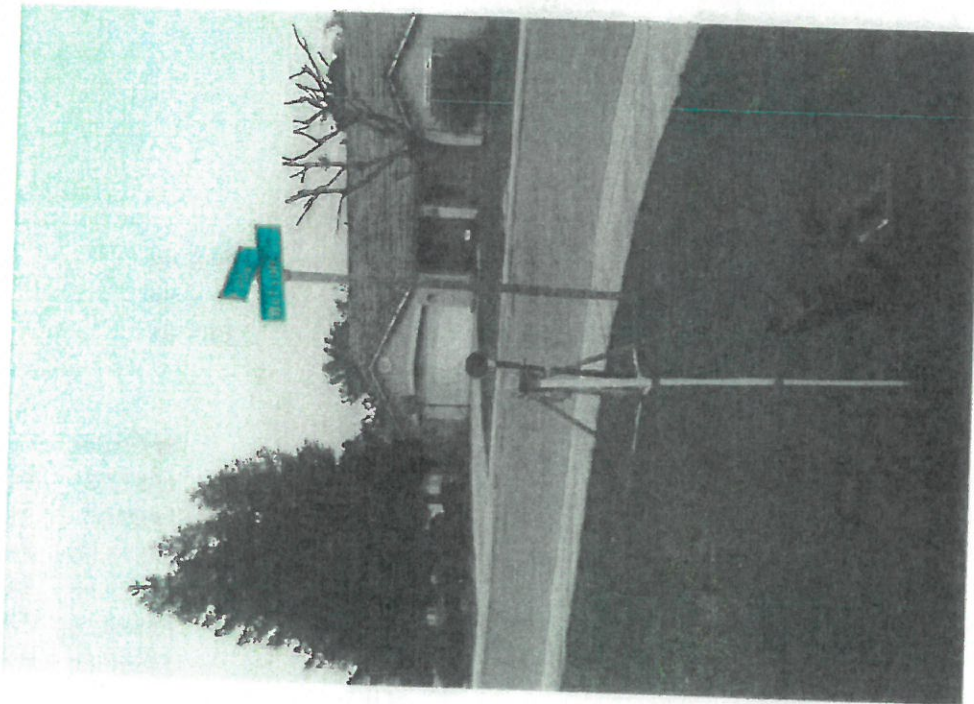
MODEL: LxT1 MODEL: Cal250

SERIAL NUMBER: 3099 SERIAL NUMBER: 2723

FACTORY CALIBRATION DATE: 11/4/2014 FACTORY CALIBRATION DATE: 11/3/2014

FIELD CALIBRATION DATE: 4/17/2017

Additional Notes/Sketch



NM1 looking South down S Watson Street



NM1 Looking East down W Kimball Ave. Height of wall ~ 6 feet





**Summary**

File Name	LxT_Data.173
Serial Number	0003099
Model	SoundTrack LxT®
Firmware Version	2.301
User	Ian Edward Gallagher
Start	2017-04-17 11:22:52
Stop	2017-04-17 11:32:52
Duration	0:10:00.0
Run Time	0:10:00.0
Pause	0:00:00.0

Pre Calibration	2017-04-17 11:16:51
Post Calibration	None
Calibration Deviation	---

**Overall Settings**

RMS Weight	A Weighting
Peak Weight	Z Weighting
Detector	Slow
Preamp	PRMLxT1L
Microphone Correction	Off
Integration Method	Linear
OBA Range	Low
OBA Bandwidth	1/1 and 1/3
OBA Freq. Weighting	Z Weighting
OBA Max Spectrum	Bin Max
Overload	121.8 dB

**Results**

LAeq	51.8 dB	
LAE	79.6 dB	
EA	10.158 $\mu\text{Pa}^2\text{h}$	
EA8	487.560 $\mu\text{Pa}^2\text{h}$	
EA40	2.438 $\text{mPa}^2\text{h}$	
LZpeak (max)	2017-04-17 11:27:13	89.6 dB
LASmax	2017-04-17 11:27:14	71.8 dB
LASmin	2017-04-17 11:29:25	39.5 dB
SEA	-99.9 dB	

LCeq	60.9 dB	<b>Statistics</b>	
LAeq	51.8 dB	LAS2.00	61.3 dB
LCeq - LAeq	9.1 dB	LAS8.00	51.7 dB
LAleq	54.0 dB	LAS25.00	47.1 dB
LAeq	51.8 dB	LAS50.00	45.1 dB
LAleq - LAeq	2.1 dB	LAS66.60	44.2 dB
# Overloads	0	LAS90.00	42.3 dB
Overload Duration	0.0 s		

# Noi. Measurement Field Data

Project Name:

Visalia CA Site 1

Project #:

6939

Date:

17 April 2017

Noise Measurement #:

NM2

3099 LxT\_Data174 .xlsx

Nearest Address or Cross Street:

S Watson Street & W Kimball Ave

Technician: Ian Edward Gallagher

Site Description (Type of Existing Land Use and any other notable features)

Empty lot. Surrounding: Residential / commercial

Weather: Cloudy, overcast with thin cloud, veiled sunshine

Temperature: 73 deg F

Settings:

☒ SLOW

FAST (Circle one)

Start Time: 11:38 AM

Wind: Calm - 3mph

Humidity: 50%

Terrain:

FLAT

Leq: 59.5 dB

End Time: 11:48 AM

Run Time:

10mins

Lmax 72.8 dB

Primary Noise Source:

Traffic, occasional vehicle along W Kimball Ave

L2 68.9 dB

Secondary Noise Sources:

Traffic ambience along S West Street, West Caldwell Ave & S Court Street

L8 65.5 dB

Overhead aircraft, bird song, residential ambience

L25 57.9 dB

L50 50.9 dB

NOISE METER:

SoundTrack LxT Class 1

MAKE:

Larson Davis

CALIBRATOR:

Larson Davis CAL250 Acoustic Calibrator

MODEL:

LxT1

MAKE:

Larson Davis

SERIAL NUMBER:

3099

MODEL:

Cal250

SERIAL NUMBER:

2723

FACTORY CALIBRATION DATE:

11/4/2014

FACTORY CALIBRATION DATE:

11/3/2014

FIELD CALIBRATION DATE:

4/17/2017



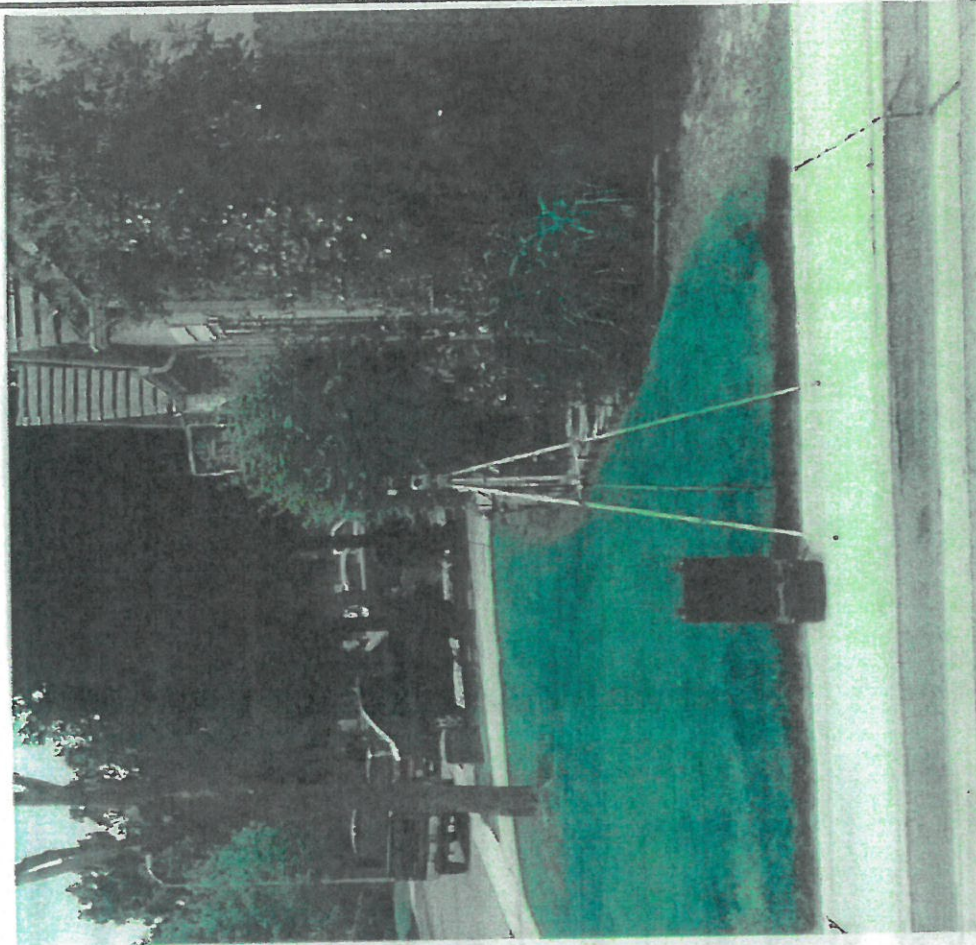


Noise Measurement  
Field Data

Additional Notes/Sketch



NM2 looking East down W Kinball Avenue



NM2 looking South down S Watson Street



# Summary

File Name

Serial Number

Model

Firmware Version

User

Location

Start

Stop

Duration

Run Time

Pause

Pre Calibration

Post Calibration

Calibration Deviation

## Overall Settings

RMS Weight

Peak Weight

Detector

Preamplifier

Microphone Correction

Integration Method

OBA Range

OBA Bandwidth

OBA Freq. Weighting

OBA Max Spectrum

Overload

## Results

L<sub>Aeq</sub>

L<sub>AE</sub>

E<sub>A</sub>

E<sub>A8</sub>

E<sub>A40</sub>

L<sub>Zpeak</sub> (max)

L<sub>ASmax</sub>

L<sub>ASmin</sub>

SEA

L<sub>Ceq</sub>

L<sub>Aeq</sub>

L<sub>Ceq</sub> - L<sub>Aeq</sub>

L<sub>Aleq</sub>

L<sub>Aeq</sub>

L<sub>Aleq</sub> - L<sub>Aeq</sub>

# Overloads

LxT\_Data.174

0003099

SoundTrack LxT®

2.301

Ian Edward Gallagher

NM2 JN6939

2017-04-17 11:38:06

2017-04-17 11:48:06

0:10:00.0

0:10:00.0

0:00:00.0

2017-04-17 11:37:40

None

---

A Weighting

Z Weighting

Slow

PRMLxT1L

Off

Linear

Low

1/1 and 1/3

Z Weighting

Bin Max

122.0 dB

59.5 dB

87.3 dB

59.615  $\mu\text{Pa}^2\text{h}$

2.862  $\text{mPa}^2\text{h}$

14.308  $\text{mPa}^2\text{h}$

2017-04-17 11:47:14

94.5 dB

2017-04-17 11:40:45

72.8 dB

2017-04-17 11:45:06

41.6 dB

99.9 dB

69.4 dB

59.5 dB

9.9 dB

61.7 dB

59.5 dB

2.2 dB

0

## Statistics

L<sub>AS2.00</sub> 68.9 dB

L<sub>AS8.00</sub> 65.5 dB

L<sub>AS25.00</sub> 57.9 dB

L<sub>AS50.00</sub> 50.9 dB

L<sub>AS66.60</sub> 48.8 dB

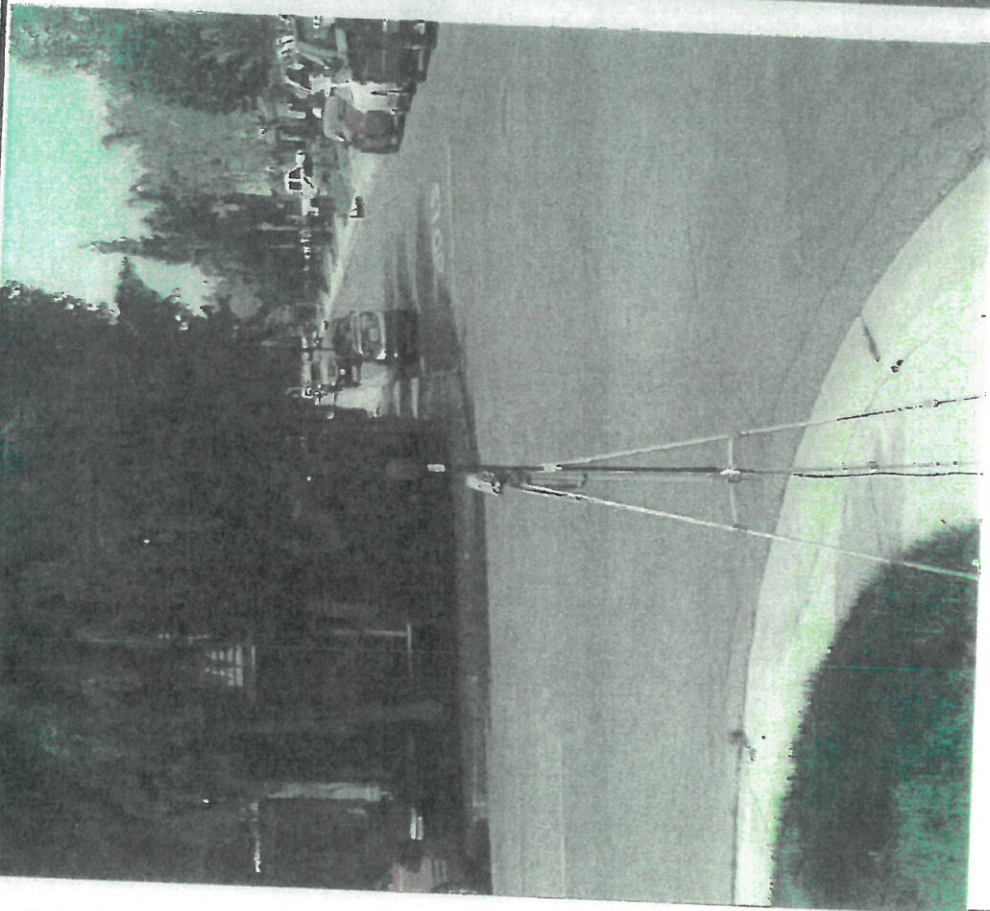
L<sub>AS90.00</sub> 45.4 dB

# Noise Measurement Field Data

Project Name: <u>Visalia CA Site 1</u>		Date: <u>17 April 2017</u>
Project #: <u>6939</u>		
Noise Measurement #: <u>NM3</u>	<u>3099 LxT_Data175</u> .xlsx	Technician: <u>Ian Edward Gallagher</u>
Nearest Address or Cross Street: <u>S West Street &amp; W Kimball Ave</u>		
Site Description (Type of Existing Land Use and any other notable features) <u>Empty lot - Surrounding : Residential / commercial</u>		
Weather: <u>Cloudy, overcast with thin cloud, veiled sunshine</u>		Settings: <span style="border: 1px solid black; padding: 2px;">SLOW</span> FAST (Circle one)
Temperature: <u>73 deg F</u>	Wind: <u>Calm - 3mph</u>	Humidity: <u>50%</u>
Terrain: <u>FLAT</u>		
Start Time: <u>11:54 AM</u>	End Time: <u>12:04 PM</u>	Run Time: <u>10mins</u>
Leq: <u>64.1 dB</u> Primary Noise Source: <u>Traffic along S West Street.</u>		
Lmax <u>79.3 dB</u>		
L2 <u>73.2 dB</u> Secondary Noise Sources: <u>Traffic along W Kimball Ave</u>		
L8 <u>68.8 dB</u> <u>Traffic ambulance along West Caldwell Ave &amp; S Court Street</u>		
L25 <u>63.4 dB</u> <u>Overhead aircraft, bird song, residential ambience</u>		
L50 <u>56.4 dB</u>		
NOISE METER: <u>SoundTrack LxT Class 1</u>	CALIBRATOR: <u>Larson Davis CAL250 Acoustic Calibrator</u>	
MAKE: <u>Larson Davis</u>	MAKE: <u>Larson Davis</u>	
MODEL: <u>LxT1</u>	MODEL: <u>Cal250</u>	
SERIAL NUMBER: <u>3099</u>	SERIAL NUMBER: <u>2723</u>	
FACTORY CALIBRATION DATE: <u>11/4/2014</u>	FACTORY CALIBRATION DATE: <u>11/3/2014</u>	
FIELD CALIBRATION DATE: <u>4/17/2017</u>		



Additional Notes/Sketch



NM3 looking West across S West Street down W Kimball Ave



NM3 looking North up S West Street across W Kimball Ave



**Summary**  
**File Name** LxT\_Data.175  
**Serial Number** 0003099  
**Model** SoundTrack LxT®  
**Firmware Version** 2.301  
**User** Ian Edward Gallagher  
**Location** NM3 JN6939  
**Start** 2017-04-17 11:54:21  
**Stop** 2017-04-17 12:04:21  
**Duration** 0:10:00.0  
**Run Time** 0:10:00.0  
**Pause** 0:00:00.0

**Pre Calibration** 2017-04-17 11:53:50  
**Post Calibration** None  
**Calibration Deviation** ---

**Overall Settings**  
**RMS Weight** A Weighting  
**Peak Weight** Z Weighting  
**Detector** Slow  
**Preamp** PRMLxT1L  
**Microphone Correction** Off  
**Integration Method** Linear  
**OBA Range** Low  
**OBA Bandwidth** 1/1 and 1/3  
**OBA Freq. Weighting** Z Weighting  
**OBA Max Spectrum** Bin Max  
**Overload** 122.0 dB

**Results**  
**LAeq** 64.1 dB  
**LAE** 91.9 dB  
**EA** 172.001  $\mu\text{Pa}^2\text{h}$   
**EA8** 8.256  $\text{mPa}^2\text{h}$   
**EA40** 41.280  $\text{mPa}^2\text{h}$   
**LZpeak (max)** 2017-04-17 11:55:01 104.6 dB  
**LASmax** 2017-04-17 11:55:02 79.3 dB  
**LASmin** 2017-04-17 11:56:00 42.9 dB  
**SEA** -99.9 dB

<b>LCeq</b>	76.9 dB	<b>Statistics</b>	
<b>LAeq</b>	64.1 dB	<b>LAS2.00</b>	73.2 dB
<b>LCeq - LAeq</b>	12.8 dB	<b>LAS8.00</b>	68.8 dB
<b>LAleq</b>	65.6 dB	<b>LAS25.00</b>	63.4 dB
<b>LAeq</b>	64.1 dB	<b>LAS50.00</b>	56.4 dB
<b>LAleq - LAeq</b>	1.5 dB	<b>LAS66.60</b>	51.7 dB
<b># Overloads</b>	0	<b>LAS90.00</b>	46.0 dB

Noi. Measurement  
Field Data

Project Name:

Visalia CA Site 1

Project #:

6939

Date:

17 April 2017

Noise Measurement #:

NM4

3099 LxT\_Data176 .xlsx

Nearest Address or Cross Street:

S West Street & W Kimball Ave

Technician:

Ian Edward Gallagher

Site Description (Type of Existing Land Use and any other notable features)

Empty lot . Surrounding : Residential / commercial

Weather: Cloudy, overcast with thin cloud, veiled sunshine

Temperature: 74 deg F

Wind: Calm - 3mph

Settings:

☒ SLOW

☐ FAST (Circle one)

Start Time:

12:09 PM

End Time:

12:19 PM

Humidity: 50%

Terrain:

FLAT

Leq:

62.8 dB

Run Time:

10mins

Lmax

77.3 dB

Primary Noise Source:

Traffic along S West Street.

L2

72.3 dB

Secondary Noise Sources:

Traffic along W Kimball Ave

L8

68.6 dB

Traffic along West Caldwell AVE & S Court Street

L25

61.8 dB

Overhead aircraft, bird song, residential ambience

L50

52.3 dB

Child playing in back yard of nearest residence, not too loud

NOISE METER:

SoundTrack LxT Class 1

MAKE:

Larson Davis

CALIBRATOR:

Larson Davis CAL250 Acoustic Calibrator

MODEL:

LxT1

MAKE:

Larson Davis

SERIAL NUMBER:

3099

MODEL:

Cal250

FACTORY CALIBRATION DATE:

11/4/2014

SERIAL NUMBER:

2723

FIELD CALIBRATION DATE:

4/17/2017

FACTORY CALIBRATION DATE:

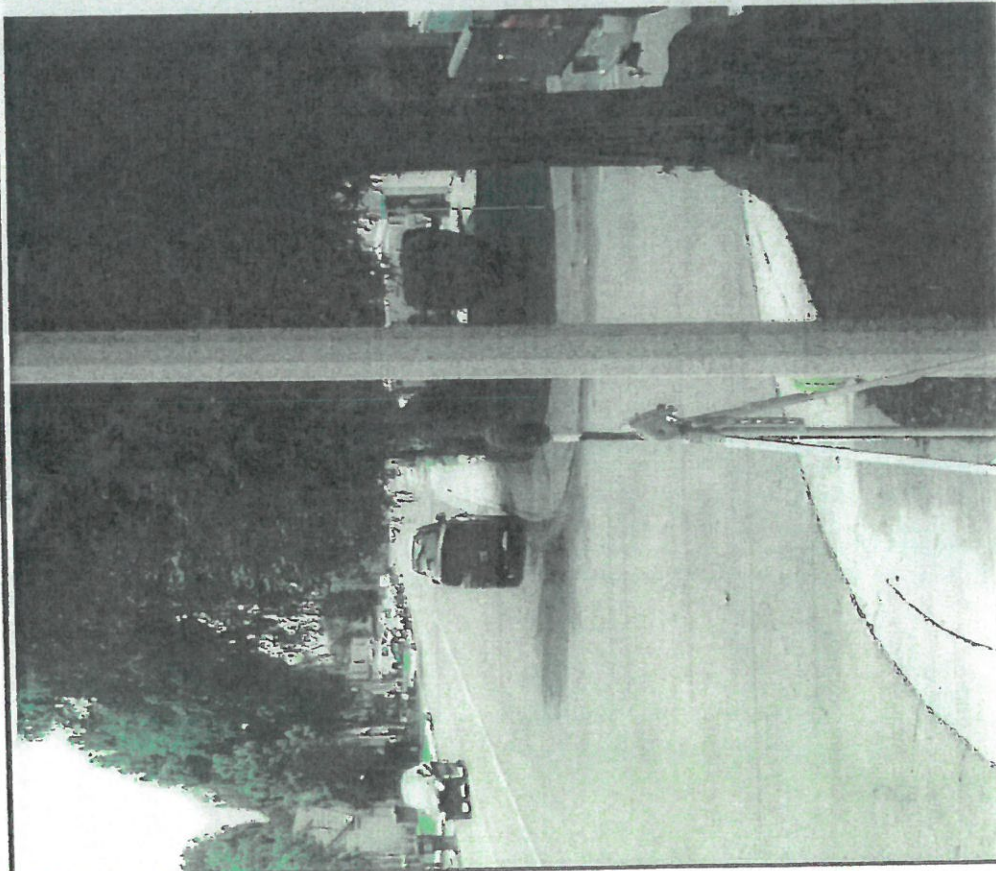
11/3/2014





Noise Measurement  
Field Data

Additional Notes/Sketch



NM4 looking South down S West Street across W Kimball Ave



NM4 looking East down W Kimball Ave across S West Street



# Summary

File Name LxT\_Data.176  
 Serial Number 0003099  
 Model SoundTrack LxT®  
 Firmware Version 2.301  
 User Ian Edward Gallagher  
 Location NM4 JN6939  
 Start 2017-04-17 12:09:43  
 Stop 2017-04-17 12:19:43  
 Duration 0:10:00.0  
 Run Time 0:10:00.0  
 Pause 0:00:00.0

Pre Calibration 2017-04-17 12:08:40  
 Post Calibration None  
 Calibration Deviation ---

## Overall Settings

RMS Weight A Weighting  
 Peak Weight Z Weighting  
 Detector Slow  
 Preamp PRMLxT1L  
 Microphone Correction Off  
 Integration Method Linear  
 OBA Range Low  
 OBA Bandwidth 1/1 and 1/3  
 OBA Freq. Weighting Z Weighting  
 OBA Max Spectrum Bin Max  
 Overload 122.1 dB

## Results

LAeq 62.8 dB  
 LAE 90.6 dB  
 EA 127.197  $\mu\text{Pa}^2\text{h}$   
 EA8 6.105  $\text{mPa}^2\text{h}$   
 EA40 30.527  $\text{mPa}^2\text{h}$   
 LZpeak (max) 2017-04-17 12:17:31 101.3 dB  
 LASmax 2017-04-17 12:17:32 77.3 dB  
 LASmin 2017-04-17 12:17:06 42.6 dB  
 SEA -99.9 dB

## LCeq

LAeq

LCeq - LAeq

LAeq

LAeq

LAeq - LAeq

# Overloads

69.1 dB

62.8 dB

6.3 dB

64.8 dB

62.8 dB

2.0 dB

0

## Statistics

LAS2.00 72.3 dB  
 LAS8.00 68.6 dB  
 LAS25.00 61.8 dB  
 LAS50.00 52.3 dB  
 LAS66.60 49.2 dB  
 LAS90.00 46.2 dB

# Noise Measurement Field Data

Project Name: Visalia CA Site 1 Date: 17 April 2017

Project #: 6939

Noise Measurement #: NM5 3099 LxT\_Data177 .xlsx Technician: Ian Edward Gallagher

Nearest Address or Cross Street: S West Street & W Kimball Ave

Site Description (Type of Existing Land Use and any other notable features) Empty lot . Surrounding : Residential / commercial

Weather: Cloudy, overcast with thin cloud, veiled sunshine Settings: ☒ SLOW ☐ FAST (Circle one)

Temperature: 74 deg F Wind: Calm - 3mph Humidity: 50% Terrain: FLAT

Start Time: 12:37 PM End Time: 12:47 PM Run Time: 10mins

Leq: 62.8 dB Primary Noise Source: Traffic along S West Street.

Lmax: 77.8 dB

L2: 71.4 dB Secondary Noise Sources: Traffic ambulance along W Kimball Ave, West Caldwell Ave & S Court Street

L8: 67.8 dB Overhead aircraft, bird song, residential ambience

L25: 62.1 dB

L50: 57.1 dB

NOISE METER: SoundTrack LxT Class 1 CALIBRATOR: Larson Davis CAL250 Acoustic Calibrator

MAKE: Larson Davis MAKE: Larson Davis

MODEL: LxT1 MODEL: Cal250

SERIAL NUMBER: 3099 SERIAL NUMBER: 2723

FACTORY CALIBRATION DATE: 11/4/2014 FACTORY CALIBRATION DATE: 11/3/2014

FIELD CALIBRATION DATE: 4/17/2017



Additional Notes/Sketch



NM5 looking North up S West Street



NM5 Looking south down S West Street





**Summary**

File Name	LxT_Data.177
Serial Number	0003099
Model	SoundTrack LxT®
Firmware Version	2.301
User	Ian Edward Gallagher
Location	NM5 JN6939
Start	2017-04-17 12:37:41
Stop	2017-04-17 12:47:41
Duration	0:10:00.0
Run Time	0:10:00.0
Pause	0:00:00.0

Pre Calibration	2017-04-17 12:25:55
Post Calibration	None
Calibration Deviation	---

**Overall Settings**

RMS Weight	A Weighting
Peak Weight	Z Weighting
Detector	Slow
Preamp	PRMLxT1L
Microphone Correction	Off
Integration Method	Linear
OBA Range	Low
OBA Bandwidth	1/1 and 1/3
OBA Freq. Weighting	Z Weighting
OBA Max Spectrum	Bin Max
Overload	122.1 dB

**Results**

LAeq	62.8 dB
LAE	90.6 dB
EA	127.576 $\mu\text{Pa}^2\text{h}$
EA8	6.124 $\text{mPa}^2\text{h}$
EA40	30.618 $\text{mPa}^2\text{h}$
LZpeak (max)	2017-04-17 12:47:35 99.7 dB
LASmax	2017-04-17 12:47:36 77.8 dB
LASmin	2017-04-17 12:43:51 41.4 dB
SEA	-99.9 dB

LCeq	70.6 dB	<b>Statistics</b>	
LAeq	62.8 dB	LAS2.00	71.4 dB
LCeq - LAeq	7.7 dB	LAS8.00	67.8 dB
LAleq	64.5 dB	LAS25.00	62.1 dB
LAeq	62.8 dB	LAS50.00	57.1 dB
LAleq - LAeq	1.7 dB	LAS66.60	54.1 dB
# Overloads	0	LAS90.00	49.6 dB

**APPENDIX D**

**RCNM Noise Modeling Output**

# Roadway Construction Noise Model (RCNM), Version 1.1

Report date: 3/27/2017  
Case Description: 6939 Cameron Creek

		---- Receptor #1 ----					
		Baselines (dBA)					
Description	Land Use	Daytime	Evening	Night			
Residential	Residential	65	65	45			
		Equipment					
Description		Impact		Spec	Actual	Receptor	Estimated
		Device	Usage(%)	Lmax (dBA)	Lmax (dBA)	Distance (feet)	Shielding (dBA)
Grader		No	40	85		125	0
Dozer		No	40		81.7	175	0
Dump Truck		No	40		76.5	200	0
Backhoe		No	40		77.6	250	0
Excavator		No	40		80.7	300	0

		Calculated (dBA)		Results
				Day
Equipment		*Lmax	Leq	Lmax
Grader		77	73.1	N/A
Dozer		70.8	66.8	N/A
Dump Truck		64.4	60.4	N/A
Backhoe		63.6	59.6	N/A
Excavator		65.1	61.2	N/A
Total		77	74.5	N/A

\*Calculated Lmax is the Loudest value.



**APPENDIX E**

**SoundPLAN Input and Output**

## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP				
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	
	dB(A)				dB(A)				
1	F1	53.1	-38.6	-38.6	0.0	50.8	-41.5	-41.5	0.0
1		38.8	0.0	0.0	0.0	37.6	0.0	0.0	0.0
2		35.4	0.0	0.0	0.0	30.3	0.0	0.0	0.0
3		38.1	0.0	0.0	0.0	35.2	0.0	0.0	0.0
Air Compressor 1		17.1	-54.9	-54.9	0.0	12.4	-59.6	-59.6	0.0
Air Compressor 2		17.2	-54.8	-54.8	0.0	12.5	-59.5	-59.5	0.0
Aircannon 1		35.6	-68.4	-68.4	0.0	35.5	-68.5	-68.5	0.0
Aircannon 2		36.6	-67.4	-67.4	0.0	36.5	-67.5	-67.5	0.0
Aircannon 3		36.4	-67.6	-67.6	0.0	36.4	-67.6	-67.6	0.0
Aircannon 4		35.6	-68.4	-68.4	0.0	35.6	-68.4	-68.4	0.0
Aircannon 5		36.9	-67.1	-67.1	0.0	36.8	-67.2	-67.2	0.0
Aircannon 6		36.5	-67.5	-67.5	0.0	36.5	-67.5	-67.5	0.0
Aircannon 7		52.1	-51.9	-51.9	0.0	48.9	-55.1	-55.1	0.0
Buff and Dry 1		22.3	-51.7	-51.7	0.0	18.3	-55.7	-55.7	0.0
Buff and Dry 2		4.9	-69.1	-69.1	0.0	4.8	-69.2	-69.2	0.0
Buff and Dry 3		3.4	-70.6	-70.6	0.0	3.3	-70.7	-70.7	0.0
Component Pneumatic Panel 1		27.3	-65.7	-65.7	0.0	27.3	-65.7	-65.7	0.0
Component Pnuematic Panel 2		27.3	-65.7	-65.7	0.0	27.3	-65.7	-65.7	0.0
Dura Scrubber 1		5.2	-66.8	-66.8	0.0	5.2	-66.8	-66.8	0.0
Dura Scrubber 2		0.0	-72.0	-72.0	0.0	0.0	-72.0	-72.0	0.0
Duroflo Pump Station 1		25.2	-54.8	-54.8	0.0	20.5	-59.5	-59.5	0.0
Duroflo Pump Station 2		25.3	-54.7	-54.7	0.0	20.6	-59.4	-59.4	0.0
Eurovac		22.1	-52.9	-52.9	0.0	15.8	-59.2	-59.2	0.0
Eurovac		20.2	-54.8	-54.8	0.0	14.6	-60.4	-60.4	0.0
Gyro Wrap 1		20.4	-55.6	-55.6	0.0	15.8	-60.2	-60.2	0.0
Gyro Wrap 2		20.5	-55.5	-55.5	0.0	15.9	-60.1	-60.1	0.0
Gyro Wrap 3		5.5	-70.5	-70.5	0.0	5.4	-70.6	-70.6	0.0
Gyro Wrap 4		6.0	-70.0	-70.0	0.0	6.0	-70.0	-70.0	0.0
Hydraulic Unit		27.5	-52.5	-52.5	0.0	21.8	-58.2	-58.2	0.0
Hydraulic Unit1		23.3	-56.7	-56.7	0.0	19.7	-60.3	-60.3	0.0
Hydraulic Unit2		20.7	-59.3	-59.3	0.0	18.4	-61.6	-61.6	0.0
Low Side Washer 1		18.5	-55.5	-55.5	0.0	13.9	-60.1	-60.1	0.0
Low Side Washer 2		18.5	-55.5	-55.5	0.0	13.8	-60.2	-60.2	0.0
Prep Jet Pump Station		23.5	-64.5	-64.5	0.0	23.5	-64.5	-64.5	0.0
Prep Jet Wand 1		18.1	-69.9	-69.9	0.0	18.1	-69.9	-69.9	0.0
Prep Jet Wand 2		15.6	-72.4	-72.4	0.0	15.6	-72.4	-72.4	0.0
Vacuum Hose End 1		28.4	-51.6	-51.6	0.0	21.9	-58.1	-58.1	0.0
Vacuum Hose End 2		30.3	-49.7	-49.7	0.0	24.1	-55.9	-55.9	0.0
Vacuum Hose End 3		29.9	-50.1	-50.1	0.0	23.9	-56.1	-56.1	0.0
Vacuum Hose End 4		29.5	-50.5	-50.5	0.0	27.8	-52.2	-52.2	0.0
Vacuum Hose End 5		29.1	-50.9	-50.9	0.0	27.5	-52.5	-52.5	0.0
Vacuum Hose End 6		28.7	-51.3	-51.3	0.0	27.1	-52.9	-52.9	0.0
Vacuum Hose End 7		28.4	-51.6	-51.6	0.0	26.8	-53.2	-53.2	0.0
Vacuum Hose End 8		28.0	-52.0	-52.0	0.0	28.0	-52.0	-52.0	0.0
Vacuum Hose End 9		27.7	-52.3	-52.3	0.0	27.7	-52.3	-52.3	0.0
Vacuum Hose End 10		27.4	-52.6	-52.6	0.0	27.4	-52.6	-52.6	0.0
Vacuum Hose End 11		24.6	-55.4	-55.4	0.0	24.6	-55.4	-55.4	0.0
Whisper Topwheel		4.7	-69.3	-69.3	0.0	4.6	-69.4	-69.4	0.0
2	F1	61.2	-37.1	-37.1	0.0	57.5	-41.8	-41.8	0.0
1		37.6	0.0	0.0	0.0	34.2	0.0	0.0	0.0
2		37.3	0.0	0.0	0.0	32.1	0.0	0.0	0.0
3		40.9	0.0	0.0	0.0	37.8	0.0	0.0	0.0
Air Compressor 1		15.5	-56.5	-56.5	0.0	11.4	-60.6	-60.6	0.0
Air Compressor 2		15.4	-56.6	-56.6	0.0	11.3	-60.7	-60.7	0.0
Aircannon 1		52.5	-51.5	-51.5	0.0	48.8	-55.2	-55.2	0.0
Aircannon 2		52.5	-51.5	-51.5	0.0	48.8	-55.2	-55.2	0.0
Aircannon 3		52.7	-51.3	-51.3	0.0	49.0	-55.0	-55.0	0.0
Aircannon 4		52.8	-51.2	-51.2	0.0	49.1	-54.9	-54.9	0.0
Aircannon 5		52.5	-51.5	-51.5	0.0	48.8	-55.2	-55.2	0.0
Aircannon 6		52.7	-51.3	-51.3	0.0	49.0	-55.0	-55.0	0.0
Aircannon 7		52.7	-51.3	-51.3	0.0	49.0	-55.0	-55.0	0.0
Buff and Dry 1		22.9	-51.1	-51.1	0.0	18.6	-55.4	-55.4	0.0
Buff and Dry 2		22.9	-51.1	-51.1	0.0	18.6	-55.4	-55.4	0.0

# Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
	dB(A)				dB(A)			
Buff and Dry 3	22.9	-51.1	-51.1	0.0	17.3	-56.7	-56.7	0.0
Component Pneumatic Panel 1	36.7	-56.3	-56.3	0.0	32.6	-60.4	-60.4	0.0
Component Pneumatic Panel 2	36.6	-56.4	-56.4	0.0	32.5	-60.5	-60.5	0.0
Dura Scrubber 1	17.3	-54.7	-54.7	0.0	11.9	-60.1	-60.1	0.0
Dura Scrubber 2	17.3	-54.7	-54.7	0.0	11.9	-60.1	-60.1	0.0
Duroflo Pump Station 1	23.3	-56.7	-56.7	0.0	19.2	-60.8	-60.8	0.0
Duroflo Pump Station 2	23.3	-56.7	-56.7	0.0	19.2	-60.8	-60.8	0.0
Eurovac	5.9	-69.1	-69.1	0.0	5.6	-69.4	-69.4	0.0
Eurovac	17.9	-57.1	-57.1	0.0	13.4	-61.6	-61.6	0.0
Gyro Wrap 1	21.9	-54.1	-54.1	0.0	16.7	-59.3	-59.3	0.0
Gyro Wrap 2	22.0	-54.0	-54.0	0.0	16.8	-59.2	-59.2	0.0
Gyro Wrap 3	22.7	-53.3	-53.3	0.0	17.3	-58.7	-58.7	0.0
Gyro Wrap 4	22.8	-53.2	-53.2	0.0	17.4	-58.6	-58.6	0.0
Hydraulic Unit	11.4	-68.6	-68.6	0.0	11.1	-68.9	-68.9	0.0
Hydraulic Unit1	26.2	-53.8	-53.8	0.0	21.3	-58.7	-58.7	0.0
Hydraulic Unit2	24.5	-55.5	-55.5	0.0	20.2	-59.8	-59.8	0.0
Low Side Washer 1	19.9	-54.1	-54.1	0.0	14.7	-59.3	-59.3	0.0
Low Side Washer 2	20.1	-53.9	-53.9	0.0	14.8	-59.2	-59.2	0.0
Prep Jet Pump Station	35.8	-52.2	-52.2	0.0	30.7	-57.3	-57.3	0.0
Prep Jet Wand 1	33.1	-54.9	-54.9	0.0	28.1	-59.9	-59.9	0.0
Prep Jet Wand 2	20.9	-67.1	-67.1	0.0	20.8	-67.2	-67.2	0.0
Vacuum Hose End 1	28.7	-51.3	-51.3	0.0	22.0	-58.0	-58.0	0.0
Vacuum Hose End 2	28.1	-51.9	-51.9	0.0	21.7	-58.3	-58.3	0.0
Vacuum Hose End 3	27.6	-52.4	-52.4	0.0	21.4	-58.6	-58.6	0.0
Vacuum Hose End 4	27.2	-52.8	-52.8	0.0	21.1	-58.9	-58.9	0.0
Vacuum Hose End 5	26.7	-53.3	-53.3	0.0	20.9	-59.1	-59.1	0.0
Vacuum Hose End 6	26.2	-53.8	-53.8	0.0	20.6	-59.4	-59.4	0.0
Vacuum Hose End 7	25.9	-54.1	-54.1	0.0	20.3	-59.7	-59.7	0.0
Vacuum Hose End 8	25.5	-54.5	-54.5	0.0	20.0	-60.0	-60.0	0.0
Vacuum Hose End 9	25.1	-54.9	-54.9	0.0	19.8	-60.2	-60.2	0.0
Vacuum Hose End 10	24.8	-55.2	-55.2	0.0	19.5	-60.5	-60.5	0.0
Vacuum Hose End 11	24.4	-55.6	-55.6	0.0	19.2	-60.8	-60.8	0.0
Whisper Topwheel	20.7	-53.3	-53.3	0.0	17.1	-56.9	-56.9	0.0
3	39.9	-39.4	-39.4	0.0	56.4	-43.3	-43.3	0.0
1	35.1	0.0	0.0	0.0	31.8	0.0	0.0	0.0
2	37.8	0.0	0.0	0.0	32.5	0.0	0.0	0.0
3	43.6	0.0	0.0	0.0	39.9	0.0	0.0	0.0
Air Compressor 1	9.5	-62.5	-62.5	0.0	9.5	-62.5	-62.5	0.0
Air Compressor 2	9.4	-62.6	-62.6	0.0	9.4	-62.6	-62.6	0.0
Aircannon 1	52.6	-51.4	-51.4	0.0	49.0	-55.0	-55.0	0.0
Aircannon 2	52.6	-51.4	-51.4	0.0	49.2	-54.8	-54.8	0.0
Aircannon 3	52.7	-51.3	-51.3	0.0	49.1	-54.9	-54.9	0.0
Aircannon 4	52.8	-51.2	-51.2	0.0	49.2	-54.8	-54.8	0.0
Aircannon 5	37.7	-66.3	-66.3	0.0	37.5	-66.5	-66.5	0.0
Aircannon 6	52.8	-51.2	-51.2	0.0	49.2	-54.8	-54.8	0.0
Aircannon 7	37.2	-66.8	-66.8	0.0	37.1	-66.9	-66.9	0.0
Buff and Dry 1	5.7	-68.3	-68.3	0.0	5.6	-68.4	-68.4	0.0
Buff and Dry 2	22.9	-51.1	-51.1	0.0	18.7	-55.3	-55.3	0.0
Buff and Dry 3	22.9	-51.1	-51.1	0.0	17.3	-56.7	-56.7	0.0
Component Pneumatic Panel 1	32.8	-60.2	-60.2	0.0	32.8	-60.2	-60.2	0.0
Component Pneumatic Panel 2	32.9	-60.1	-60.1	0.0	32.9	-60.1	-60.1	0.0
Dura Scrubber 1	16.6	-55.4	-55.4	0.0	11.8	-60.2	-60.2	0.0
Dura Scrubber 2	16.6	-55.4	-55.4	0.0	11.8	-60.2	-60.2	0.0
Duroflo Pump Station 1	17.4	-62.6	-62.6	0.0	17.4	-62.6	-62.6	0.0
Duroflo Pump Station 2	17.4	-62.6	-62.6	0.0	17.3	-62.7	-62.7	0.0
Eurovac	10.7	-64.3	-64.3	0.0	10.6	-64.4	-64.4	0.0
Eurovac	11.2	-63.8	-63.8	0.0	11.2	-63.8	-63.8	0.0
Gyro Wrap 1	21.2	-54.8	-54.8	0.0	16.6	-59.4	-59.4	0.0
Gyro Wrap 2	21.2	-54.8	-54.8	0.0	16.6	-59.4	-59.4	0.0
Gyro Wrap 3	21.9	-54.1	-54.1	0.0	17.0	-59.0	-59.0	0.0
Gyro Wrap 4	11.2	-64.8	-64.8	0.0	11.1	-64.9	-64.9	0.0
Hydraulic Unit	11.4	-68.6	-68.6	0.0	11.1	-68.9	-68.9	0.0
Hydraulic Unit1	27.3	-52.7	-52.7	0.0	22.1	-57.9	-57.9	0.0
Hydraulic Unit2	25.3	-54.7	-54.7	0.0	20.8	-59.2	-59.2	0.0



## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP				
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	
	dB(A)				dB(A)				
Low Side Washer 1	19.1	-54.9	-54.9	0.0	14.5	-59.5	-59.5	0.0	
Low Side Washer 2	19.3	-54.7	-54.7	0.0	14.7	-59.3	-59.3	0.0	
Prep Jet Pump Station	28.2	-59.8	-59.8	0.0	28.2	-59.8	-59.8	0.0	
Prep Jet Wand 1	32.4	-55.6	-55.6	0.0	28.1	-59.9	-59.9	0.0	
Prep Jet Wand 2	32.5	-55.5	-55.5	0.0	28.1	-59.9	-59.9	0.0	
Vacuum Hose End 1	28.5	-51.5	-51.5	0.0	21.8	-58.2	-58.2	0.0	
Vacuum Hose End 2	28.0	-52.0	-52.0	0.0	21.6	-58.4	-58.4	0.0	
Vacuum Hose End 3	10.8	-69.2	-69.2	0.0	10.8	-69.2	-69.2	0.0	
Vacuum Hose End 4	10.5	-69.5	-69.5	0.0	10.5	-69.5	-69.5	0.0	
Vacuum Hose End 5	10.3	-69.7	-69.7	0.0	10.3	-69.7	-69.7	0.0	
Vacuum Hose End 6	10.0	-70.0	-70.0	0.0	10.0	-70.0	-70.0	0.0	
Vacuum Hose End 7	9.8	-70.2	-70.2	0.0	9.8	-70.2	-70.2	0.0	
Vacuum Hose End 8	9.6	-70.4	-70.4	0.0	9.5	-70.5	-70.5	0.0	
Vacuum Hose End 9	9.2	-70.8	-70.8	0.0	9.2	-70.8	-70.8	0.0	
Vacuum Hose End 10	9.1	-70.9	-70.9	0.0	9.1	-70.9	-70.9	0.0	
Vacuum Hose End 11	8.7	-71.3	-71.3	0.0	8.6	-71.4	-71.4	0.0	
Whisper Topwheel	20.1	-53.9	-53.9	0.0	16.9	-57.1	-57.1	0.0	
4	P1	57.0	-41.7	-41.7	0.0	54.1	-44.6	-44.6	0.0
1		31.8	0.0	0.0	0.0	28.8	0.0	0.0	0.0
2		36.7	0.0	0.0	0.0	31.4	0.0	0.0	0.0
3		44.1	0.0	0.0	0.0	40.1	0.0	0.0	0.0
Air Compressor 1		7.3	-64.7	-64.7	0.0	7.3	-64.7	-64.7	0.0
Air Compressor 2		7.3	-64.7	-64.7	0.0	7.3	-64.7	-64.7	0.0
Aircannon 1		50.4	-53.6	-53.6	0.0	47.3	-56.7	-56.7	0.0
Aircannon 2		50.3	-53.7	-53.7	0.0	47.3	-56.7	-56.7	0.0
Aircannon 3		50.4	-53.6	-53.6	0.0	47.4	-56.6	-56.6	0.0
Aircannon 4		50.5	-53.5	-53.5	0.0	47.5	-56.5	-56.5	0.0
Aircannon 5		39.3	-64.7	-64.7	0.0	39.3	-64.7	-64.7	0.0
Aircannon 6		38.9	-65.1	-65.1	0.0	38.9	-65.1	-65.1	0.0
Aircannon 7		36.4	-67.6	-67.6	0.0	36.4	-67.6	-67.6	0.0
Buff and Dry 1		4.8	-69.2	-69.2	0.0	4.7	-69.3	-69.3	0.0
Buff and Dry 2		21.8	-52.2	-52.2	0.0	17.9	-56.1	-56.1	0.0
Buff and Dry 3		21.7	-52.3	-52.3	0.0	16.4	-57.6	-57.6	0.0
Component Pneumatic Panel 1		28.1	-64.9	-64.9	0.0	28.1	-64.9	-64.9	0.0
Component Pnuematic Panel 2		28.2	-64.8	-64.8	0.0	28.2	-64.8	-64.8	0.0
Dura Scrubber 1		10.9	-61.1	-61.1	0.0	10.9	-61.1	-61.1	0.0
Dura Scrubber 2		9.9	-62.1	-62.1	0.0	9.9	-62.1	-62.1	0.0
Duroflo Pump Station 1		24.9	-55.1	-55.1	0.0	20.4	-59.6	-59.6	0.0
Duroflo Pump Station 2		24.9	-55.1	-55.1	0.0	20.5	-59.5	-59.5	0.0
Eurovac		7.1	-67.9	-67.9	0.0	7.0	-68.0	-68.0	0.0
Eurovac		19.8	-55.2	-55.2	0.0	14.4	-60.6	-60.6	0.0
Gyro Wrap 1		16.1	-59.9	-59.9	0.0	16.1	-59.9	-59.9	0.0
Gyro Wrap 2		16.0	-60.0	-60.0	0.0	16.0	-60.0	-60.0	0.0
Gyro Wrap 3		14.9	-61.1	-61.1	0.0	14.9	-61.1	-61.1	0.0
Gyro Wrap 4		14.7	-61.3	-61.3	0.0	14.7	-61.3	-61.3	0.0
Hydraulic Unit		13.9	-66.1	-66.1	0.0	13.8	-66.2	-66.2	0.0
Hydraulic Unit1		27.9	-52.1	-52.1	0.0	22.5	-57.5	-57.5	0.0
Hydraulic Unit2		25.9	-54.1	-54.1	0.0	21.2	-58.8	-58.8	0.0
Low Side Washer 1		13.9	-60.1	-60.1	0.0	13.9	-60.1	-60.1	0.0
Low Side Washer 2		14.3	-59.7	-59.7	0.0	14.3	-59.7	-59.7	0.0
Prep Jet Pump Station		24.5	-63.5	-63.5	0.0	24.5	-63.5	-63.5	0.0
Prep Jet Wand 1		24.2	-63.8	-63.8	0.0	24.2	-63.8	-63.8	0.0
Prep Jet Wand 2		24.0	-64.0	-64.0	0.0	24.0	-64.0	-64.0	0.0
Vacuum Hose End 1		27.2	-52.8	-52.8	0.0	20.9	-59.1	-59.1	0.0
Vacuum Hose End 2		8.4	-71.6	-71.6	0.0	8.3	-71.7	-71.7	0.0
Vacuum Hose End 3		8.0	-72.0	-72.0	0.0	8.0	-72.0	-72.0	0.0
Vacuum Hose End 4		7.6	-72.4	-72.4	0.0	7.6	-72.4	-72.4	0.0
Vacuum Hose End 5		7.3	-72.7	-72.7	0.0	7.3	-72.7	-72.7	0.0
Vacuum Hose End 6		6.9	-73.1	-73.1	0.0	6.9	-73.1	-73.1	0.0
Vacuum Hose End 7		6.8	-73.2	-73.2	0.0	6.8	-73.2	-73.2	0.0
Vacuum Hose End 8		6.5	-73.5	-73.5	0.0	6.5	-73.5	-73.5	0.0
Vacuum Hose End 9		6.2	-73.8	-73.8	0.0	6.2	-73.8	-73.8	0.0
Vacuum Hose End 10		6.1	-73.9	-73.9	0.0	6.1	-73.9	-73.9	0.0
Vacuum Hose End 11		7.0	-73.0	-73.0	0.0	7.0	-73.0	-73.0	0.0

# Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
dB(A)								
Whisper Topwheel	18.1	-55.9	-55.9	0.0	18.1	-55.9	-55.9	0.0
5	58.4	-38.4	-38.4	0.0	58.0	-38.5	-38.5	0.0
1	44.8	0.0	0.0	0.0	44.8	0.0	0.0	0.0
2	39.8	0.0	0.0	0.0	33.2	0.0	0.0	0.0
3	42.0	0.0	0.0	0.0	36.6	0.0	0.0	0.0
Air Compressor 1	10.5	-61.5	-61.5	0.0	10.5	-61.5	-61.5	0.0
Air Compressor 2	10.6	-61.4	-61.4	0.0	10.6	-61.4	-61.4	0.0
Aircannon 1	40.6	-63.4	-63.4	0.0	40.6	-63.4	-63.4	0.0
Aircannon 2	41.7	-62.3	-62.3	0.0	41.7	-62.3	-62.3	0.0
Aircannon 3	42.0	-62.0	-62.0	0.0	42.0	-62.0	-62.0	0.0
Aircannon 4	41.1	-62.9	-62.9	0.0	41.1	-62.9	-62.9	0.0
Aircannon 5	44.1	-59.9	-59.9	0.0	44.1	-59.9	-59.9	0.0
Aircannon 6	43.6	-60.4	-60.4	0.0	43.6	-60.4	-60.4	0.0
Aircannon 7	46.1	-57.9	-57.9	0.0	46.1	-57.9	-57.9	0.0
Buff and Dry 1	18.6	-55.4	-55.4	0.0	18.6	-55.4	-55.4	0.0
Buff and Dry 2	12.0	-62.0	-62.0	0.0	12.0	-62.0	-62.0	0.0
Buff and Dry 3	21.4	-52.6	-52.6	0.0	19.4	-54.6	-54.6	0.0
Component Pneumatic Panel 1	31.3	-61.7	-61.7	0.0	31.3	-61.7	-61.7	0.0
Component Pneumatic Panel 2	31.4	-61.6	-61.6	0.0	31.4	-61.6	-61.6	0.0
Dura Scrubber 1	9.2	-62.8	-62.8	0.0	9.2	-62.8	-62.8	0.0
Dura Scrubber 2	8.9	-63.1	-63.1	0.0	8.9	-63.1	-63.1	0.0
Duroflo Pump Station 1	18.7	-61.3	-61.3	0.0	18.7	-61.3	-61.3	0.0
Duroflo Pump Station 2	18.8	-61.2	-61.2	0.0	18.8	-61.2	-61.2	0.0
Eurovac	22.8	-52.2	-52.2	0.0	22.8	-52.2	-52.2	0.0
Eurovac	13.3	-61.7	-61.7	0.0	13.3	-61.7	-61.7	0.0
Gyro Wrap 1	13.5	-62.5	-62.5	0.0	13.5	-62.5	-62.5	0.0
Gyro Wrap 2	13.6	-62.4	-62.4	0.0	13.6	-62.4	-62.4	0.0
Gyro Wrap 3	13.7	-62.3	-62.3	0.0	13.7	-62.3	-62.3	0.0
Gyro Wrap 4	14.0	-62.0	-62.0	0.0	14.0	-62.0	-62.0	0.0
Hydraulic Unit	28.3	-51.7	-51.7	0.0	28.3	-51.7	-51.7	0.0
Hydraulic Unit1	22.8	-57.2	-57.2	0.0	21.2	-58.8	-58.8	0.0
Hydraulic Unit2	19.7	-60.3	-60.3	0.0	18.0	-62.0	-62.0	0.0
Low Side Washer 1	11.7	-62.3	-62.3	0.0	11.7	-62.3	-62.3	0.0
Low Side Washer 2	11.4	-62.6	-62.6	0.0	11.4	-62.6	-62.6	0.0
Prep Jet Pump Station	27.2	-60.8	-60.8	0.0	27.2	-60.8	-60.8	0.0
Prep Jet Wand 1	22.6	-65.4	-65.4	0.0	22.6	-65.4	-65.4	0.0
Prep Jet Wand 2	26.6	-61.4	-61.4	0.0	26.6	-61.4	-61.4	0.0
Vacuum Hose End 1	29.4	-50.6	-50.6	0.0	29.4	-50.6	-50.6	0.0
Vacuum Hose End 2	31.3	-48.7	-48.7	0.0	31.3	-48.7	-48.7	0.0
Vacuum Hose End 3	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 4	30.6	-49.4	-49.4	0.0	30.6	-49.4	-49.4	0.0
Vacuum Hose End 5	30.2	-49.8	-49.8	0.0	30.2	-49.8	-49.8	0.0
Vacuum Hose End 6	29.8	-50.2	-50.2	0.0	29.8	-50.2	-50.2	0.0
Vacuum Hose End 7	29.5	-50.5	-50.5	0.0	29.5	-50.5	-50.5	0.0
Vacuum Hose End 8	29.2	-50.8	-50.8	0.0	29.2	-50.8	-50.8	0.0
Vacuum Hose End 9	28.9	-51.1	-51.1	0.0	28.9	-51.1	-51.1	0.0
Vacuum Hose End 10	28.5	-51.5	-51.5	0.0	28.5	-51.5	-51.5	0.0
Vacuum Hose End 11	25.7	-54.3	-54.3	0.0	25.7	-54.3	-54.3	0.0
Whisper Topwheel	8.5	-65.5	-65.5	0.0	8.5	-65.5	-65.5	0.0
6	50.7	-39.0	-39.0	0.0	50.7	-39.0	-39.0	0.0
1	45.1	0.0	0.0	0.0	45.1	0.0	0.0	0.0
2	37.7	0.0	0.0	0.0	37.7	0.0	0.0	0.0
3	38.9	0.0	0.0	0.0	38.9	0.0	0.0	0.0
Air Compressor 1	8.6	-63.4	-63.4	0.0	8.6	-63.4	-63.4	0.0
Air Compressor 2	8.7	-63.3	-63.3	0.0	8.7	-63.3	-63.3	0.0
Aircannon 1	38.0	-66.0	-66.0	0.0	38.0	-66.0	-66.0	0.0
Aircannon 2	38.6	-65.4	-65.4	0.0	38.6	-65.4	-65.4	0.0
Aircannon 3	38.7	-65.3	-65.3	0.0	38.7	-65.3	-65.3	0.0
Aircannon 4	38.2	-65.8	-65.8	0.0	38.2	-65.8	-65.8	0.0
Aircannon 5	39.7	-64.3	-64.3	0.0	39.7	-64.3	-64.3	0.0
Aircannon 6	39.4	-64.6	-64.6	0.0	39.4	-64.6	-64.6	0.0
Aircannon 7	40.1	-63.9	-63.9	0.0	40.1	-63.9	-63.9	0.0
Buff and Dry 1	13.2	-60.8	-60.8	0.0	13.2	-60.8	-60.8	0.0

## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP				
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	
	dB(A)				dB(A)				
Buff and Dry 2	8.2	-65.8	-65.8	0.0	8.2	-65.8	-65.8	0.0	
Buff and Dry 3	19.9	-54.1	-54.1	0.0	19.9	-54.1	-54.1	0.0	
Component Pneumatic Panel 1	29.6	-63.4	-63.4	0.0	29.6	-63.4	-63.4	0.0	
Component Pnuematic Panel 2	29.6	-63.4	-63.4	0.0	29.6	-63.4	-63.4	0.0	
Dura Scrubber 1	8.1	-63.9	-63.9	0.0	8.1	-63.9	-63.9	0.0	
Dura Scrubber 2	8.3	-63.7	-63.7	0.0	8.3	-63.7	-63.7	0.0	
Duroflo Pump Station 1	16.7	-63.3	-63.3	0.0	16.7	-63.3	-63.3	0.0	
Duroflo Pump Station 2	16.7	-63.3	-63.3	0.0	16.7	-63.3	-63.3	0.0	
Eurovac	11.3	-63.7	-63.7	0.0	11.3	-63.7	-63.7	0.0	
Eurovac	10.8	-64.2	-64.2	0.0	10.8	-64.2	-64.2	0.0	
Gyro Wrap 1	12.3	-63.7	-63.7	0.0	12.3	-63.7	-63.7	0.0	
Gyro Wrap 2	12.2	-63.8	-63.8	0.0	12.2	-63.8	-63.8	0.0	
Gyro Wrap 3	12.5	-63.5	-63.5	0.0	12.5	-63.5	-63.5	0.0	
Gyro Wrap 4	12.4	-63.6	-63.6	0.0	12.4	-63.6	-63.6	0.0	
Hydraulic Unit	17.5	-62.5	-62.5	0.0	17.5	-62.5	-62.5	0.0	
Hydraulic Unit1	18.1	-61.9	-61.9	0.0	18.1	-61.9	-61.9	0.0	
Hydraulic Unit2	17.0	-63.0	-63.0	0.0	17.0	-63.0	-63.0	0.0	
Low Side Washer 1	10.3	-63.7	-63.7	0.0	10.3	-63.7	-63.7	0.0	
Low Side Washer 2	10.4	-63.6	-63.6	0.0	10.4	-63.6	-63.6	0.0	
Prep Jet Pump Station	24.7	-63.3	-63.3	0.0	24.7	-63.3	-63.3	0.0	
Prep Jet Wand 1	24.8	-63.2	-63.2	0.0	24.8	-63.2	-63.2	0.0	
Prep Jet Wand 2	30.8	-57.2	-57.2	0.0	30.8	-57.2	-57.2	0.0	
Vacuum Hose End 1	30.9	-49.1	-49.1	0.0	30.9	-49.1	-49.1	0.0	
Vacuum Hose End 2	30.8	-49.2	-49.2	0.0	30.8	-49.2	-49.2	0.0	
Vacuum Hose End 3	30.7	-49.3	-49.3	0.0	30.7	-49.3	-49.3	0.0	
Vacuum Hose End 4	30.5	-49.5	-49.5	0.0	30.5	-49.5	-49.5	0.0	
Vacuum Hose End 5	30.3	-49.7	-49.7	0.0	30.3	-49.7	-49.7	0.0	
Vacuum Hose End 6	30.1	-49.9	-49.9	0.0	30.1	-49.9	-49.9	0.0	
Vacuum Hose End 7	30.0	-50.0	-50.0	0.0	30.0	-50.0	-50.0	0.0	
Vacuum Hose End 8	29.8	-50.2	-50.2	0.0	29.8	-50.2	-50.2	0.0	
Vacuum Hose End 9	29.6	-50.4	-50.4	0.0	29.6	-50.4	-50.4	0.0	
Vacuum Hose End 10	27.2	-52.8	-52.8	0.0	27.2	-52.8	-52.8	0.0	
Vacuum Hose End 11	26.7	-53.3	-53.3	0.0	26.7	-53.3	-53.3	0.0	
Whisper Topwheel	7.1	-66.9	-66.9	0.0	7.1	-66.9	-66.9	0.0	
7	F1	30.3	-38.3	-38.3	0.0	50.9	-38.3	-38.3	0.0
1		45.7	0.0	0.0	0.0	45.7	0.0	0.0	0.0
2		37.3	0.0	0.0	0.0	38.0	0.0	0.0	0.0
3		36.8	0.0	0.0	0.0	37.9	0.0	0.0	0.0
Air Compressor 1		8.9	-63.1	-63.1	0.0	8.9	-63.1	-63.1	0.0
Air Compressor 2		8.9	-63.1	-63.1	0.0	8.9	-63.1	-63.1	0.0
Aircannon 1		37.8	-66.2	-66.2	0.0	37.8	-66.2	-66.2	0.0
Aircannon 2		38.5	-65.5	-65.5	0.0	38.5	-65.5	-65.5	0.0
Aircannon 3		38.4	-65.6	-65.6	0.0	38.4	-65.6	-65.6	0.0
Aircannon 4		37.9	-66.1	-66.1	0.0	37.9	-66.1	-66.1	0.0
Aircannon 5		39.4	-64.6	-64.6	0.0	39.4	-64.6	-64.6	0.0
Aircannon 6		39.0	-65.0	-65.0	0.0	39.0	-65.0	-65.0	0.0
Aircannon 7		39.6	-64.4	-64.4	0.0	39.6	-64.4	-64.4	0.0
Buff and Dry 1		12.6	-61.4	-61.4	0.0	12.6	-61.4	-61.4	0.0
Buff and Dry 2		7.7	-66.3	-66.3	0.0	7.7	-66.3	-66.3	0.0
Buff and Dry 3		11.6	-62.4	-62.4	0.0	11.6	-62.4	-62.4	0.0
Component Pneumatic Panel 1		29.9	-63.1	-63.1	0.0	29.9	-63.1	-63.1	0.0
Component Pnuematic Panel 2		29.9	-63.1	-63.1	0.0	29.9	-63.1	-63.1	0.0
Dura Scrubber 1		9.2	-62.8	-62.8	0.0	9.2	-62.8	-62.8	0.0
Dura Scrubber 2		9.1	-62.9	-62.9	0.0	9.1	-62.9	-62.9	0.0
Duroflo Pump Station 1		16.9	-63.1	-63.1	0.0	16.9	-63.1	-63.1	0.0
Duroflo Pump Station 2		16.9	-63.1	-63.1	0.0	16.9	-63.1	-63.1	0.0
Eurovac		11.1	-63.9	-63.9	0.0	11.1	-63.9	-63.9	0.0
Eurovac		11.0	-64.0	-64.0	0.0	11.0	-64.0	-64.0	0.0
Gyro Wrap 1		12.8	-63.2	-63.2	0.0	12.8	-63.2	-63.2	0.0
Gyro Wrap 2		12.7	-63.3	-63.3	0.0	12.7	-63.3	-63.3	0.0
Gyro Wrap 3		12.8	-63.2	-63.2	0.0	12.8	-63.2	-63.2	0.0
Gyro Wrap 4		12.7	-63.3	-63.3	0.0	12.7	-63.3	-63.3	0.0
Hydraulic Unit		17.1	-62.9	-62.9	0.0	17.1	-62.9	-62.9	0.0
Hydraulic Unit1		15.6	-64.4	-64.4	0.0	15.6	-64.4	-64.4	0.0



# Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
Hydraulic Unit2								
Low Side Washer 1	17.2	-62.8	-62.8	0.0	17.2	-62.8	-62.8	0.0
Low Side Washer 2	10.7	-63.3	-63.3	0.0	10.7	-63.3	-63.3	0.0
Prep Jet Pump Station	10.9	-63.1	-63.1	0.0	10.9	-63.1	-63.1	0.0
Prep Jet Wand 1	25.7	-62.3	-62.3	0.0	25.7	-62.3	-62.3	0.0
Prep Jet Wand 2	31.0	-57.0	-57.0	0.0	31.0	-57.0	-57.0	0.0
Vacuum Hose End 1	36.1	-51.9	-51.9	0.0	36.1	-51.9	-51.9	0.0
Vacuum Hose End 2	30.9	-49.1	-49.1	0.0	30.9	-49.1	-49.1	0.0
Vacuum Hose End 3	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 4	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 5	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 6	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 7	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 8	31.0	-49.0	-49.0	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 9	30.9	-49.1	-49.1	0.0	31.0	-49.0	-49.0	0.0
Vacuum Hose End 10	30.8	-49.2	-49.2	0.0	30.9	-49.1	-49.1	0.0
Vacuum Hose End 11	28.4	-51.6	-51.6	0.0	30.8	-49.2	-49.2	0.0
Whisper Topwheel	28.2	-51.8	-51.8	0.0	28.4	-51.6	-51.6	0.0
8	7.6	-66.4	-66.4	0.0	28.2	-51.8	-51.8	0.0
F1	50.0	-39.2	-39.2	0.0	7.6	-66.4	-66.4	0.0
1	44.7	0.0	0.0	0.0	50.1	-39.2	-39.2	0.0
2	34.9	0.0	0.0	0.0	44.7	0.0	0.0	0.0
3	39.6	0.0	0.0	0.0	35.9	0.0	0.0	0.0
Air Compressor 1	8.5	-63.5	-63.5	0.0	40.1	0.0	0.0	0.0
Air Compressor 2	8.4	-63.6	-63.6	0.0	8.5	-63.5	-63.5	0.0
Aircannon 1	36.6	-67.4	-67.4	0.0	8.4	-63.6	-63.6	0.0
Aircannon 2	37.4	-66.6	-66.6	0.0	36.6	-67.4	-67.4	0.0
Aircannon 3	37.1	-66.9	-66.9	0.0	37.4	-66.6	-66.6	0.0
Aircannon 4	36.5	-67.5	-67.5	0.0	37.1	-66.9	-66.9	0.0
Aircannon 5	38.4	-65.6	-65.6	0.0	36.5	-67.5	-67.5	0.0
Aircannon 6	37.8	-66.2	-66.2	0.0	38.4	-65.6	-65.6	0.0
Aircannon 7	38.4	-65.6	-65.6	0.0	37.8	-66.2	-66.2	0.0
Buff and Dry 1	11.6	-62.4	-62.4	0.0	38.4	-65.6	-65.6	0.0
Buff and Dry 2	6.1	-67.9	-67.9	0.0	11.6	-62.4	-62.4	0.0
Buff and Dry 3	10.4	-63.6	-63.6	0.0	6.1	-67.9	-67.9	0.0
Component Pneumatic Panel 1	29.7	-63.3	-63.3	0.0	10.4	-63.6	-63.6	0.0
Component Pnuematic Panel 2	29.6	-63.4	-63.4	0.0	29.7	-63.3	-63.3	0.0
Dura Scrubber 1	9.7	-62.3	-62.3	0.0	29.6	-63.4	-63.4	0.0
Dura Scrubber 2	7.2	-64.8	-64.8	0.0	9.7	-62.3	-62.3	0.0
Duroflo Pump Station 1	16.4	-63.6	-63.6	0.0	7.2	-64.8	-64.8	0.0
Duroflo Pump Station 2	16.3	-63.7	-63.7	0.0	16.4	-63.6	-63.6	0.0
Eurovac	10.2	-64.8	-64.8	0.0	16.3	-63.7	-63.7	0.0
Gyro Wrap 1	10.3	-64.7	-64.7	0.0	10.2	-64.8	-64.8	0.0
Gyro Wrap 2	12.5	-63.5	-63.5	0.0	10.3	-64.7	-64.7	0.0
Gyro Wrap 3	12.3	-63.7	-63.7	0.0	12.5	-63.5	-63.5	0.0
Gyro Wrap 4	12.1	-63.9	-63.9	0.0	12.3	-63.7	-63.7	0.0
Hydraulic Unit	12.0	-64.0	-64.0	0.0	12.1	-63.9	-63.9	0.0
Hydraulic Unit1	16.1	-63.9	-63.9	0.0	12.0	-64.0	-64.0	0.0
Hydraulic Unit2	19.8	-60.2	-60.2	0.0	16.1	-63.9	-63.9	0.0
Low Side Washer 1	22.5	-57.5	-57.5	0.0	19.8	-60.2	-60.2	0.0
Low Side Washer 2	10.5	-63.5	-63.5	0.0	22.5	-57.5	-57.5	0.0
Prep Jet Pump Station	10.4	-63.6	-63.6	0.0	10.5	-63.5	-63.5	0.0
Prep Jet Wand 1	29.3	-58.7	-58.7	0.0	10.4	-63.6	-63.6	0.0
Prep Jet Wand 2	35.3	-52.7	-52.7	0.0	29.3	-58.7	-58.7	0.0
Vacuum Hose End 1	35.7	-52.3	-52.3	0.0	35.3	-52.7	-52.7	0.0
Vacuum Hose End 2	29.1	-50.9	-50.9	0.0	35.7	-52.3	-52.3	0.0
Vacuum Hose End 3	29.3	-50.7	-50.7	0.0	29.1	-50.9	-50.9	0.0
Vacuum Hose End 4	29.5	-50.5	-50.5	0.0	29.3	-50.7	-50.7	0.0
Vacuum Hose End 5	29.6	-50.4	-50.4	0.0	29.5	-50.5	-50.5	0.0
Vacuum Hose End 6	29.8	-50.2	-50.2	0.0	29.6	-50.4	-50.4	0.0
Vacuum Hose End 7	30.0	-50.0	-50.0	0.0	29.8	-50.2	-50.2	0.0
Vacuum Hose End 8	30.1	-49.9	-49.9	0.0	30.0	-50.0	-50.0	0.0
Vacuum Hose End 9	30.2	-49.8	-49.8	0.0	30.1	-49.9	-49.9	0.0
Vacuum Hose End 10	28.0	-52.0	-52.0	0.0	30.2	-49.8	-49.8	0.0
	28.1	-51.9	-51.9	0.0	28.0	-52.0	-52.0	0.0
					28.1	-51.9	-51.9	0.0

## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
	dB(A)				dB(A)			
Vacuum Hose End 11	28.2	-51.8	-51.8	0.0	28.2	-51.8	-51.8	0.0
Whisper Topwheel	7.0	-67.0	-67.0	0.0	7.0	-67.0	-67.0	0.0
9	43.4	-41.1	-41.1	0.0	43.5	-41.1	-41.1	0.0
1	42.4	0.0	0.0	0.0	42.4	0.0	0.0	0.0
2	29.5	0.0	0.0	0.0	31.7	0.0	0.0	0.0
3	39.6	0.0	0.0	0.0	40.0	0.0	0.0	0.0
Air Compressor 1	7.7	-64.3	-64.3	0.0	7.7	-64.3	-64.3	0.0
Air Compressor 2	7.6	-64.4	-64.4	0.0	7.6	-64.4	-64.4	0.0
Aircannon 1	34.8	-69.2	-69.2	0.0	34.8	-69.2	-69.2	0.0
Aircannon 2	35.8	-68.2	-68.2	0.0	35.8	-68.2	-68.2	0.0
Aircannon 3	35.5	-68.5	-68.5	0.0	35.5	-68.5	-68.5	0.0
Aircannon 4	34.7	-69.3	-69.3	0.0	34.7	-69.3	-69.3	0.0
Aircannon 5	37.0	-67.0	-67.0	0.0	37.0	-67.0	-67.0	0.0
Aircannon 6	36.3	-67.7	-67.7	0.0	36.3	-67.7	-67.7	0.0
Aircannon 7	37.0	-67.0	-67.0	0.0	37.0	-67.0	-67.0	0.0
Buff and Dry 1	10.4	-63.6	-63.6	0.0	10.4	-63.6	-63.6	0.0
Buff and Dry 2	4.3	-69.7	-69.7	0.0	4.3	-69.7	-69.7	0.0
Buff and Dry 3	9.0	-65.0	-65.0	0.0	9.0	-65.0	-65.0	0.0
Component Pneumatic Panel 1	29.4	-63.6	-63.6	0.0	29.4	-63.6	-63.6	0.0
Component Pneumatic Panel 2	29.2	-63.8	-63.8	0.0	29.2	-63.8	-63.8	0.0
Dura Scrubber 1	17.1	-54.9	-54.9	0.0	17.1	-54.9	-54.9	0.0
Dura Scrubber 2	7.9	-64.1	-64.1	0.0	7.9	-64.1	-64.1	0.0
Duroflo Pump Station 1	15.5	-64.5	-64.5	0.0	15.5	-64.5	-64.5	0.0
Duroflo Pump Station 2	15.4	-64.6	-64.6	0.0	15.4	-64.6	-64.6	0.0
Eurovac	9.0	-66.0	-66.0	0.0	9.0	-66.0	-66.0	0.0
Eurovac	9.4	-65.6	-65.6	0.0	9.4	-65.6	-65.6	0.0
Gyro Wrap 1	7.5	-68.5	-68.5	0.0	7.5	-68.5	-68.5	0.0
Gyro Wrap 2	7.9	-68.1	-68.1	0.0	7.9	-68.1	-68.1	0.0
Gyro Wrap 3	10.9	-65.1	-65.1	0.0	10.9	-65.1	-65.1	0.0
Gyro Wrap 4	10.9	-65.1	-65.1	0.0	10.9	-65.1	-65.1	0.0
Hydraulic Unit	15.0	-65.0	-65.0	0.0	15.0	-65.0	-65.0	0.0
Hydraulic Unit1	20.1	-59.9	-59.9	0.0	20.1	-59.9	-59.9	0.0
Hydraulic Unit2	22.1	-57.9	-57.9	0.0	22.1	-57.9	-57.9	0.0
Low Side Washer 1	6.9	-67.1	-67.1	0.0	6.9	-67.1	-67.1	0.0
Low Side Washer 2	4.9	-69.1	-69.1	0.0	4.9	-69.1	-69.1	0.0
Prep Jet Pump Station	33.0	-55.0	-55.0	0.0	33.0	-55.0	-55.0	0.0
Prep Jet Wand 1	33.3	-54.7	-54.7	0.0	33.3	-54.7	-54.7	0.0
Prep Jet Wand 2	33.5	-54.5	-54.5	0.0	33.5	-54.5	-54.5	0.0
Vacuum Hose End 1	26.4	-53.6	-53.6	0.0	26.4	-53.6	-53.6	0.0
Vacuum Hose End 2	26.6	-53.4	-53.4	0.0	26.6	-53.4	-53.4	0.0
Vacuum Hose End 3	26.8	-53.2	-53.2	0.0	26.8	-53.2	-53.2	0.0
Vacuum Hose End 4	27.0	-53.0	-53.0	0.0	27.0	-53.0	-53.0	0.0
Vacuum Hose End 5	27.3	-52.7	-52.7	0.0	27.3	-52.7	-52.7	0.0
Vacuum Hose End 6	27.5	-52.5	-52.5	0.0	27.5	-52.5	-52.5	0.0
Vacuum Hose End 7	27.7	-52.3	-52.3	0.0	27.7	-52.3	-52.3	0.0
Vacuum Hose End 8	27.9	-52.1	-52.1	0.0	27.9	-52.1	-52.1	0.0
Vacuum Hose End 9	25.7	-54.3	-54.3	0.0	25.7	-54.3	-54.3	0.0
Vacuum Hose End 10	25.9	-54.1	-54.1	0.0	25.9	-54.1	-54.1	0.0
Vacuum Hose End 11	26.1	-53.9	-53.9	0.0	26.1	-53.9	-53.9	0.0
Whisper Topwheel	17.4	-56.6	-56.6	0.0	17.4	-56.6	-56.6	0.0
10	56.9	-39.6	-39.6	0.0	57.0	-39.6	-39.6	0.0
1	35.5	0.0	0.0	0.0	35.9	0.0	0.0	0.0
2	36.4	0.0	0.0	0.0	37.6	0.0	0.0	0.0
3	50.9	0.0	0.0	0.0	51.1	0.0	0.0	0.0
Air Compressor 1	9.0	-63.0	-63.0	0.0	9.0	-63.0	-63.0	0.0
Air Compressor 2	9.0	-63.0	-63.0	0.0	9.0	-63.0	-63.0	0.0
Aircannon 1	47.1	-56.9	-56.9	0.0	47.1	-56.9	-56.9	0.0
Aircannon 2	47.0	-57.0	-57.0	0.0	47.0	-57.0	-57.0	0.0
Aircannon 3	47.0	-57.0	-57.0	0.0	47.0	-57.0	-57.0	0.0
Aircannon 4	47.0	-57.0	-57.0	0.0	47.0	-57.0	-57.0	0.0
Aircannon 5	46.7	-57.3	-57.3	0.0	46.7	-57.3	-57.3	0.0
Aircannon 6	46.9	-57.1	-57.1	0.0	46.9	-57.1	-57.1	0.0
Aircannon 7	46.7	-57.3	-57.3	0.0	46.7	-57.3	-57.3	0.0

# Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
	dB(A)				dB(A)			
Buff and Dry 1	15.0	-59.0	-59.0	0.0	15.0	-59.0	-59.0	0.0
Buff and Dry 2	15.3	-58.7	-58.7	0.0	15.3	-58.7	-58.7	0.0
Buff and Dry 3	12.6	-61.4	-61.4	0.0	12.6	-61.4	-61.4	0.0
Component Pneumatic Panel 1	29.9	-63.1	-63.1	0.0	29.9	-63.1	-63.1	0.0
Component Pnuematic Panel 2	29.9	-63.1	-63.1	0.0	29.9	-63.1	-63.1	0.0
Dura Scrubber 1	8.2	-63.8	-63.8	0.0	8.2	-63.8	-63.8	0.0
Dura Scrubber 2	8.9	-63.1	-63.1	0.0	8.9	-63.1	-63.1	0.0
Duroflo Pump Station 1	17.0	-63.0	-63.0	0.0	17.0	-63.0	-63.0	0.0
Duroflo Pump Station 2	17.0	-63.0	-63.0	0.0	17.0	-63.0	-63.0	0.0
Eurovac	10.1	-64.9	-64.9	0.0	10.1	-64.9	-64.9	0.0
Eurovac	10.2	-64.8	-64.8	0.0	10.2	-64.8	-64.8	0.0
Gyro Wrap 1	13.5	-62.5	-62.5	0.0	13.5	-62.5	-62.5	0.0
Gyro Wrap 2	13.4	-62.6	-62.6	0.0	13.4	-62.6	-62.6	0.0
Gyro Wrap 3	13.5	-62.5	-62.5	0.0	13.5	-62.5	-62.5	0.0
Gyro Wrap 4	13.4	-62.6	-62.6	0.0	13.4	-62.6	-62.6	0.0
Hydraulic Unit	16.8	-63.2	-63.2	0.0	16.8	-63.2	-63.2	0.0
Hydraulic Unit1	32.7	-47.3	-47.3	0.0	32.7	-47.3	-47.3	0.0
Hydraulic Unit2	35.9	-44.1	-44.1	0.0	35.9	-44.1	-44.1	0.0
Low Side Washer 1	11.4	-62.6	-62.6	0.0	11.4	-62.6	-62.6	0.0
Low Side Washer 2	11.5	-62.5	-62.5	0.0	11.5	-62.5	-62.5	0.0
Prep Jet Pump Station	24.4	-63.6	-63.6	0.0	24.4	-63.6	-63.6	0.0
Prep Jet Wand 1	35.4	-52.6	-52.6	0.0	35.4	-52.6	-52.6	0.0
Prep Jet Wand 2	35.1	-52.9	-52.9	0.0	35.1	-52.9	-52.9	0.0
Vacuum Hose End 1	10.8	-69.2	-69.2	0.0	10.8	-69.2	-69.2	0.0
Vacuum Hose End 2	10.9	-69.1	-69.1	0.0	10.9	-69.1	-69.1	0.0
Vacuum Hose End 3	10.9	-69.1	-69.1	0.0	10.9	-69.1	-69.1	0.0
Vacuum Hose End 4	11.0	-69.0	-69.0	0.0	11.0	-69.0	-69.0	0.0
Vacuum Hose End 5	11.1	-68.9	-68.9	0.0	11.1	-68.9	-68.9	0.0
Vacuum Hose End 6	11.2	-68.8	-68.8	0.0	11.2	-68.8	-68.8	0.0
Vacuum Hose End 7	11.3	-68.7	-68.7	0.0	11.3	-68.7	-68.7	0.0
Vacuum Hose End 8	11.8	-68.2	-68.2	0.0	11.8	-68.2	-68.2	0.0
Vacuum Hose End 9	26.5	-53.5	-53.5	0.0	26.5	-53.5	-53.5	0.0
Vacuum Hose End 10	26.5	-53.5	-53.5	0.0	26.5	-53.5	-53.5	0.0
Vacuum Hose End 11	26.5	-53.5	-53.5	0.0	26.5	-53.5	-53.5	0.0
Whisper Topwheel	17.2	-56.8	-56.8	0.0	17.2	-56.8	-56.8	0.0
11	54.6	-44.7	-44.7	0.0	53.8	-45.2	-45.2	0.0
1	33.3	0.0	0.0	0.0	28.9	0.0	0.0	0.0
2	38.3	0.0	0.0	0.0	31.5	0.0	0.0	0.0
3	48.3	0.0	0.0	0.0	47.5	0.0	0.0	0.0
Air Compressor 1	7.8	-64.2	-64.2	0.0	7.3	-64.7	-64.7	0.0
Air Compressor 2	7.9	-64.1	-64.1	0.0	7.4	-64.6	-64.6	0.0
Aircannon 1	44.7	-59.3	-59.3	0.0	44.1	-59.9	-59.9	0.0
Aircannon 2	44.4	-59.6	-59.6	0.0	43.9	-60.1	-60.1	0.0
Aircannon 3	44.5	-59.5	-59.5	0.0	43.8	-60.2	-60.2	0.0
Aircannon 4	44.8	-59.2	-59.2	0.0	44.0	-60.0	-60.0	0.0
Aircannon 5	43.8	-60.2	-60.2	0.0	43.3	-60.7	-60.7	0.0
Aircannon 6	44.1	-59.9	-59.9	0.0	43.5	-60.5	-60.5	0.0
Aircannon 7	46.6	-57.4	-57.4	0.0	45.5	-58.5	-58.5	0.0
Buff and Dry 1	16.0	-58.0	-58.0	0.0	13.9	-60.1	-60.1	0.0
Buff and Dry 2	14.2	-59.8	-59.8	0.0	12.7	-61.3	-61.3	0.0
Buff and Dry 3	13.5	-60.5	-60.5	0.0	10.0	-64.0	-64.0	0.0
Component Pneumatic Panel 1	28.8	-64.2	-64.2	0.0	28.2	-64.8	-64.8	0.0
Component Pnuematic Panel 2	28.8	-64.2	-64.2	0.0	28.3	-64.7	-64.7	0.0
Dura Scrubber 1	8.0	-64.0	-64.0	0.0	8.0	-64.0	-64.0	0.0
Dura Scrubber 2	7.1	-64.9	-64.9	0.0	7.1	-64.9	-64.9	0.0
Duroflo Pump Station 1	15.9	-64.1	-64.1	0.0	15.4	-64.6	-64.6	0.0
Duroflo Pump Station 2	15.9	-64.1	-64.1	0.0	15.4	-64.6	-64.6	0.0
Eurovac	8.3	-66.7	-66.7	0.0	7.8	-67.2	-67.2	0.0
Eurovac	9.1	-65.9	-65.9	0.0	8.7	-66.3	-66.3	0.0
Gyro Wrap 1	13.1	-62.9	-62.9	0.0	13.1	-62.9	-62.9	0.0
Gyro Wrap 2	12.2	-63.8	-63.8	0.0	12.2	-63.8	-63.8	0.0
Gyro Wrap 3	12.4	-63.6	-63.6	0.0	12.0	-64.0	-64.0	0.0
Gyro Wrap 4	12.3	-63.7	-63.7	0.0	12.0	-64.0	-64.0	0.0
Hydraulic Unit	17.7	-62.3	-62.3	0.0	15.5	-64.5	-64.5	0.0



## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
	dB(A)				dB(A)			
Hydraulic Unit1	27.9	-52.1	-52.1	0.0	27.9	-52.1	-52.1	0.0
Hydraulic Unit2	27.0	-53.0	-53.0	0.0	27.0	-53.0	-53.0	0.0
Low Side Washer 1	10.1	-63.9	-63.9	0.0	10.1	-63.9	-63.9	0.0
Low Side Washer 2	10.3	-63.7	-63.7	0.0	10.3	-63.7	-63.7	0.0
Prep Jet Pump Station	24.3	-63.7	-63.7	0.0	24.3	-63.7	-63.7	0.0
Prep Jet Wand 1	23.4	-64.6	-64.6	0.0	23.4	-64.6	-64.6	0.0
Prep Jet Wand 2	22.1	-65.9	-65.9	0.0	22.1	-65.9	-65.9	0.0
Vacuum Hose End 1	16.9	-63.1	-63.1	0.0	8.0	-72.0	-72.0	0.0
Vacuum Hose End 2	7.1	-72.9	-72.9	0.0	4.7	-75.3	-75.3	0.0
Vacuum Hose End 3	9.6	-70.4	-70.4	0.0	9.2	-70.8	-70.8	0.0
Vacuum Hose End 4	9.3	-70.7	-70.7	0.0	9.1	-70.9	-70.9	0.0
Vacuum Hose End 5	9.1	-70.9	-70.9	0.0	8.9	-71.1	-71.1	0.0
Vacuum Hose End 6	8.8	-71.2	-71.2	0.0	8.8	-71.2	-71.2	0.0
Vacuum Hose End 7	8.7	-71.3	-71.3	0.0	8.7	-71.3	-71.3	0.0
Vacuum Hose End 8	8.5	-71.5	-71.5	0.0	8.5	-71.5	-71.5	0.0
Vacuum Hose End 9	8.4	-71.6	-71.6	0.0	8.4	-71.6	-71.6	0.0
Vacuum Hose End 10	13.7	-66.3	-66.3	0.0	13.7	-66.3	-66.3	0.0
Vacuum Hose End 11	11.7	-68.3	-68.3	0.0	11.7	-68.3	-68.3	0.0
Whisper Topwheel	16.0	-58.0	-58.0	0.0	16.0	-58.0	-58.0	0.0

## Noise Emissions of Industry Sources

Source name	Reference	Level				Corrections		
		Leq1 dB(A)	Leq2 dB(A)	Leq3 dB(A)	Lmax dB(A)	Kwall dB(A)	CI dB(A)	CT dB(A)
Prep Jet Wand 1	Unit	88.0	-	-	-	-	-	-
Prep Jet Wand 2	Unit	88.0	-	-	-	-	-	-
Dura Scrubber 1	Unit	72.0	-	-	-	-	-	-
Dura Scrubber 2	Unit	72.0	-	-	-	-	-	-
Low Side Washer 1	Unit	74.0	-	-	-	-	-	-
Gyro Wrap 1	Unit	76.0	-	-	-	-	-	-
Gyro Wrap 2	Unit	76.0	-	-	-	-	-	-
Low Side Washer 2	Unit	74.0	-	-	-	-	-	-
Whisper Topwheel	Unit	74.0	-	-	-	-	-	-
Gyro Wrap 3	Unit	76.0	-	-	-	-	-	-
Gyro Wrap 4	Unit	76.0	-	-	-	-	-	-
Aircannon 1	Unit	104.0	-	-	-	-	-	-
Aircannon 2	Unit	104.0	-	-	-	-	-	-
Aircannon 3	Unit	104.0	-	-	-	-	-	-
Aircannon 4	Unit	104.0	-	-	-	-	-	-
Aircannon 5	Unit	104.0	-	-	-	-	-	-
Aircannon 6	Unit	104.0	-	-	-	-	-	-
Aircannon 7	Unit	104.0	-	-	-	-	-	-
Buff and Dry 1	Unit	74.0	-	-	-	-	-	-
Buff and Dry 2	Unit	74.0	-	-	-	-	-	-
Buff and Dry 3	Unit	74.0	-	-	-	-	-	-
Prep Jet Pump Station	Unit	88.0	-	-	-	-	-	-
Component Pneumatic Panel 1	Unit	93.0	-	-	-	-	-	-
Component Pneumatic Panel 2	Unit	93.0	-	-	-	-	-	-
Air Compressor 1	Unit	72.0	-	-	-	-	-	-
Air Compressor 2	Unit	72.0	-	-	-	-	-	-
Duroflo Pump Station 1	Unit	80.0	-	-	-	-	-	-
Duroflo Pump Station 2	Unit	80.0	-	-	-	-	-	-
Hydraulic Unit	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 1	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 2	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 3	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 4	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 5	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 6	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 7	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 8	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 9	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 10	Unit	80.0	-	-	-	-	-	-
Vacuum Hose End 11	Unit	80.0	-	-	-	-	-	-
Eurovac	Unit	75.0	-	-	-	-	-	-
Hydraulic Unit1	Unit	80.0	-	-	-	-	-	-
Hydraulic Unit2	Unit	80.0	-	-	-	-	-	-

## Noise Emissions of Parking Lot Traffic

Name	Parking lot type	Low noise trolleys	Size	Movements per hour				Road surface	Separate method	Level dB(A)
				Leg1	Leg2	Leg3	Lmax			
1	Fast food restaurants (Drive-	-	11	25.00	0.000	0.000	0.000	Asphaltic lanes	no	78.2
2	Fast food restaurants (Drive-	-	7	16.00	0.000	0.000	0.000	Asphaltic lanes	no	75.5
3	Fast food restaurants (Drive-	-	17	39.00	0.000	0.000	0.000	Asphaltic lanes	no	81.6



# Receiver List

No.	Receiver name	Build side	Floor	Limit				Level w/o NP				Level w. NP				Difference				Conflict			
				Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
				dB(A)				dB(A)				dB(A)				dB(A)				dB(A)			
1	1		FI	-	-	-	-	53.4	-38.6	-38.6	0.0	50.8	-41.6	-41.6	0.0	-2.6	-2.9	-2.9	0.0	-	-	-	-
2	2		FI	-	-	-	-	61.2	-37.7	-37.7	0.0	57.5	-41.6	-41.6	0.0	-3.7	-4.7	-4.7	0.0	-	-	-	-
3	3		FI	-	-	-	-	59.9	-39.4	-39.4	0.0	56.4	-43.6	-43.6	0.0	-3.5	-3.9	-3.9	0.0	-	-	-	-
4	4		FI	-	-	-	-	57.0	-41.7	-41.7	0.0	54.1	-44.6	-44.6	0.0	-2.9	-3.0	-3.0	0.0	-	-	-	-
5	5		FI	-	-	-	-	53.4	-38.6	-38.6	0.0	53.0	-38.6	-38.6	0.0	-0.4	-0.1	-0.1	0.0	-	-	-	-
6	6		FI	-	-	-	-	50.7	-39.6	-39.6	0.0	50.7	-39.6	-39.6	0.0	0.0	0.0	0.0	0.0	-	-	-	-
7	7		FI	-	-	-	-	50.8	-38.6	-38.6	0.0	50.9	-38.6	-38.6	0.0	0.1	0.0	0.0	0.0	-	-	-	-
8	8		FI	-	-	-	-	50.0	-39.6	-39.6	0.0	50.1	-39.6	-39.6	0.0	0.1	0.0	0.0	0.0	-	-	-	-
9	9		FI	-	-	-	-	48.4	-41.7	-41.7	0.0	48.5	-41.7	-41.7	0.0	0.1	0.0	0.0	0.0	-	-	-	-
10	10		FI	-	-	-	-	56.9	-39.6	-39.6	0.0	57.0	-39.6	-39.6	0.0	0.1	0.0	0.0	0.0	-	-	-	-
11	11		FI	-	-	-	-	54.6	-44.7	-44.7	0.0	53.8	-45.6	-45.6	0.0	-0.8	-0.5	-0.5	0.0	-	-	-	-

## Contribution Levels of the Receivers

Source name	Level w/o NP				Level w. NP			
	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
	dB(A)				dB(A)			
1	52.4	-42.7	-42.7	64.9	44.9	-46.5	-46.5	60.9
1	41.8	0.0	0.0	0.0	33.9	0.0	0.0	0.0
2	45.6	0.0	0.0	0.0	37.7	0.0	0.0	0.0
3	50.6	0.0	0.0	0.0	43.0	0.0	0.0	0.0
Hydraulic Unit1	29.7	-50.3	-50.3	0.0	26.2	-53.8	-53.8	0.0
Hydraulic Unit2	27.4	-52.6	-52.6	0.0	24.2	-55.8	-55.8	0.0
Lmax Event1	35.9	-44.1	-44.1	64.9	31.9	-48.1	-48.1	60.9

# Noise Emissions of Industry Sources

Source name	Reference	Level				Corrections		
		Leq1 dB(A)	Leq2 dB(A)	Leq3 dB(A)	Lmax dB(A)	Kwall dB(A)	CI dB(A)	CT dB(A)
Hydraulic Unit1	Unit	80.0	-	-	-	-	-	-
Hydraulic Unit2	Unit	80.0	-	-	-	-	-	-
Lmax Event1	Unit	80.0	-	-	109.0	-	-	-



## Noise Emissions of Parking Lot Traffic

Name	Parking lot type	Low noise trolleys	Size	Movements per hour				Road surface	Separate method	Level dB(A)
				Leq1	Leq2	Leq3	Lmax			
1	Fast food restaurants (Drive-	-	11	25.00	0.000	0.000	0.000	Asphaltic lanes	no	78.2
2	Fast food restaurants (Drive-	-	7	16.00	0.000	0.000	0.000	Asphaltic lanes	no	75.5
3	Fast food restaurants (Drive-	-	17	39.00	0.000	0.000	0.000	Asphaltic lanes	no	81.6

# Receiver List

No.	Receiver name	Build side	Floor	Limit				Level w/o NP				Level w. NP				Difference				Conflict			
				Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax	Leq1	Leq2	Leq3	Lmax
11			Fl	-	-	-	-	52.4	-42.7	-42.7	64.9	44.9	-46.7	-46.7	60.9	-7.5	-3.8	-3.8	-4.0	-	-	-	-



MEETING DATE February 22, 2017  
SITE PLAN NO. 16-140  
PARCEL MAP NO.  
SUBDIVISION  
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with  
☐ Planning ☐ Engineering prior to resubmittal plans for Site Plan Review.

☐ Solid Waste ☐ Parks and Recreation ☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☒ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.

☒ Your plans must be reviewed by:

☐ CITY COUNCIL

☐ REDEVELOPMENT

☒ PLANNING COMMISSION

☐ PARK/RECREATION

☒ CUP

☐ HISTORIC PRESERVATION

☐ OTHER -TCUP

☒ **ADDITIONAL COMMENTS: OFF AGENDA FOR INTERNAL DRIVE AISLE DESIGN**

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.2  
*Site Plan Review Committee*





**BUILDING/DEVELOPMENT PLAN  
REQUIREMENTS  
ENGINEERING DIVISION**

☐ Jason Huckleberry 713-4259  
☒ Adrian Rubalcaba 713-4271

ITEM NO: 4 DATE: FEBRUARY 22, 2017

SITE PLAN NO.: 16-140 RESUBMITTAL  
PROJECT TITLE: CAMERON CREEK ARCO AMPM  
DESCRIPTION: NEW SERVICE STATION W/ 6000SF  
CONVENIENCE STORE, 10 DISPENSER CANOPY  
AND AUTOMATED CARWASH. STORE TO  
OPERATE 24 HOUR W/ SALES OF BEER AND  
WINE  
APPLICANT: CAMERON CREEK ARCO AMPM  
PROP OWNER: CAMERON CREEK MARKETPLACE LLC  
LOCATION:  
APN: 126-870-037

**SITE PLAN REVIEW COMMENTS**

☒ REQUIREMENTS (indicated by checked boxes)

- ☒ Install curb return with ramp, with 35' radius; **WEST & CALDWELL, SEE ADDL COMMENTS**
- ☒ Install curb; ☒ gutter **WEST ST.**
- ☒ Drive approach size: ☒ Use radius return; **REFER TO CITY COMMERCIAL STDS**
- ☒ Sidewalk: 6' width; ☒ 10' parkway width at **WEST ST.**
- ☒ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- ☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- ☐ Right-of-way dedication required. A title report is required for verification of ownership.
- ☒ Deed required prior to issuing building permit; **SIDEWALK EASEMENTS**
- ☒ City Encroachment Permit Required. **FOR ALL WORK IN PUBLIC RIGHT-OF-WAY**  
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
- ☐ CalTrans Encroachment Permit required. ☐ CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;
- ☐ Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
- ☒ Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- ☒ Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. ☒ Prepared by registered civil engineer or project architect. ☒ All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) ☒ directed to the City's existing storm drainage system; b) ☐ directed to a permanent on-site basin; or c) ☐ directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: : maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance. **CONNECT TO ONSITE STORM DRAIN DESIGN**
- ☒ Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- ☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- ☒ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- ☒ All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications. **WEST ST**

- ☒ Traffic indexes per city standards: **REFER TO CITY COLLECTOR STREET STDS & SPECS**
- ☒ Install street striping as required by the City Engineer. **FURTHER COORDINATE WITH TRAFFIC DEPT.**
- ☒ Install landscape curbing (typical at parking lot planters). **ONSITE AS NECESSARY**
- ☒ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☒ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☒ Provide "R" value tests: **1 each at 300' INTERVALS**
- ☐ Written comments required from ditch company      Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide      wide riparian dedication from top of bank.
- ☐ Show Oak trees with drip lines and adjacent grade elevations. ☐ Protect Oak trees during construction in accordance with City requirements.
- ☐ A permit is required to remove oak trees. Contact Joel Hooyer at 713-4295 for an Oak tree evaluation or permit to remove. ☐ A pre-construction conference is required.
- ☒ Relocate existing utility poles and/or facilities.
- ☐ Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer:
- ☒ Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- ☒ If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- ☒ If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- ☐ Comply with prior comments. ☐ Resubmit with additional information. ☒ Redesign required.

**Additional Comments:**

- 1. Existing curb ramp at West & Caldwell is required to comply with current City std's. Modify or remove & replace accordingly.**
- 2. Street section for West St. shall meet City collector design standards. A 10' parkway and 6' sidewalk is required. An easement for the sidewalk area shall be deeded to the City, refer to City document format and procedures available on the City website.**
- 3. West St. shall be improved to Kimball Ave. Curb, gutter, parkway landscaping, street lights, sidewalk, and pavement is required. The northeast corner curb ramp return at Kimball and West shall be installed with City std. 30' radius return.**
- 4. Additional street striping will be required on West St.**
- 5. Proposed drive approach location on West St. is adequate.**
- 6. An access drive onsite to surround the parcel and connect to existing onsite drive aisle east is required. A copy of the granted egress/ingress from adjacent parcels owners is required.**
- 7. Street frontages shall be landscaped and comply with State MWELO regulations. Improvement plans shall accompany onsite building plans.**
- 8. The trash enclosure location and position will allow for a direct-stab by a Solid Waste vehicle however additional pavement is required in front of the enclosure for adequate Solid Waste vehicle maneuverability. Comply with City 24' enclosure standards with concrete apron and gates.**

9. Sewer and storm drain utility connections shall be from existing onsite master design.
10. Refer to Traffic Safety Dept. comments for additional requirements.
11. Refer to Planning Dept. comments for additional requirements.
12. An accessible path of travel from the public sidewalk to building entrance is required.
13. It is typical to see an emergency escape exit for vehicles in carwash queue.
14. Proposed development will incur impact fees associated with the land development, fueling stations, convenience store and internal food services, and carwash. Refer to page 4 for applicable fees.
15. Resubmit Off-Agenda for required onsite drive aisle and connection to existing to the east.



## SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: 16-140 RESUBMITTAL

Date: 2/22/2017

### Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date: 10/1/2016)

(Project type for fee rates: VARIES)

☐ Existing uses may qualify for credits on Development Impact Fees.

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	\$1,226/AC X 1.6 = \$1,961.60
<input checked="" type="checkbox"/> Transportation Impact Fee	\$13,825/1KSF X 10.9 = \$150,693 GAS STATION 20 POSITIONS = \$332,504
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	RETAIL \$23/1KSF X 6 = \$138 CARWASH \$18,588/EACH X 1 WALK-UP \$1,278/EACH X TBD TREATMENT PLANT FEES: RETAIL \$53/1KSF X 6 = \$318 CARWASH \$20,239/EACH X 1 WALK-UP \$5,613/EACH X TBD
<input checked="" type="checkbox"/> Sewer Front Foot Fee	\$41/LF X 200' (CALDWELL) = \$8,200
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	\$6,554/AC X 1.6 = \$10,486.40
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	\$4,811/AC X 1.6 = \$7,697.60
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	\$8,223/AC X 1.6 = \$13,156.80
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	\$1,799/AC X 1.6 = \$2,878.40
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$316/1000SF X 10.9 = \$3,444.40
<input type="checkbox"/> Parking In-Lieu	

### Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.

  
Adrian Rubalcaba

# SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

February 22, 2017

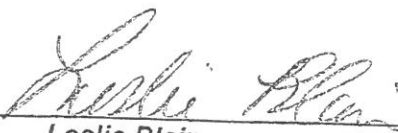
ITEM NO: 4	RESUBMTL
SITE PLAN NO:	SPR16140
PROJECT TITLE:	CAMERON CREEK ARCO AMPM
DESCRIPTION:	NEW SERVICE STATION W/ 6000SF CONVENIENCE STORE, 10 DISPENSER CANOPY AND AUTOMATED CARWASH. STORE TO OPERATE 24 HOUR W/ SALES OF BEER AND WINE (CN) (X) (A)
APPLICANT:	CAMERON CREEK ARCO AMPM
PROP. OWNER:	CAMERON CREEK MARKETPLACE LLC
LOCATION:	SWC OF EAST CALDWELL AVE AND S COURT STREET
APN(S):	126-870-037

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- ☐ No Comments
- ☒ See Previous Site Plan Comments
- ☒ Install Street Light(s) per City Standards.
- ☐ Install Street Name Blades at                      Locations.
- ☒ Install Stop Signs at **driveway exit** Locations.
- ☒ Construct parking per City Standards PK-1 through PK-4.
- ☒ Construct drive approach per City Standards.
- ☐ Traffic Impact Analysis required.
  - ☐ Provide more traffic information such as                      . Depending on development size, characteristics, etc.,  
a TIA may be required.

## Additional Comments:

- Show access queuing.
- Connect to main drive aisle off Caldwell

  
Leslie Blair

City of Visalia  
Building: Site Plan  
Review Comments

ITEM NO: 4

DATE: February 22, 2017

SITE PLAN NO:

SPR16140 RESUBMIT

PROJECT TITLE:

CAMERON CREEK ARCO AMPM

DESCRIPTION:

NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)

APPLICANT:

CAMERON CREEK ARCO AMPM

PROP OWNER:

CAMERON CREEK MARKETPLACE LLC

LOCATION:

SWC OF EAST CALDWELL AVE AND S COURT  
126-870-037

NOTE: These are general comments and DO NOT const APN(S):

Please refer to the applicable California Codes & local ordinance for additional requirements.

- ☐ Business Tax Certification is required. *For information call (559) 713-4326*
- ☒ A building permit will be required. *For information call (559) 713-4444*
- ☒ Submit 4 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
- ☐ Submit 4 sets of plans prepared by an architect or engineer. Must comply with 2013 California Building Cod Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.
- ☐ Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- ☒ Meet State and Federal requirements for accessibility for persons with disabilities.
- ☒ A path of travel, parking, common area and public right of way must comply with requirements for access for persons with disabilities.
- ☐ Multi family units shall be accessible or adaptable for persons with disabilities.
- ☐ Maintain sound transmission control between units minimum of 50 STC.
- ☐ Maintain fire-resistive requirements at property lines.
- ☐ A demolition permit & deposit is required. *For information call (559) 713-4444*
- ☐ Obtain required clearance from San Joaquin Valley Air Pollution Board. Prior to am demolition work
- For information call (661) 392-5500*
- ☒ Location of cashier must provide clear view of gas pump island
- ☒ Plans must be approved by the Tulare County Health Department. *For information call (559) 624-7400*
- ☐ Project is located in flood zone \_\_\_\_\_ \* ☐ Hazardous materials report.
- ☐ Arrange for an on-site inspection. (Fee for inspection \$157.00) *For information call (559) 713-4444*
- ☒ School Development fees. Commercial \$0.56 per square foot. Residential \$3.75 per square foot.
- ☐ Existing address must be changed to be consistent with city address. *For information call (559) 713-4320*
- ☐ Acceptable as submitted
- ☐ No comments
- ☐ See previous comments dated: \_\_\_\_\_

Special comments:

MEET THE MWELD REQUIREMENTS FOR THE  
NEW LANDSCAPING. PROVIDE CREESE  
INTERCEPTOR.

Signature

Date:

2/22/17

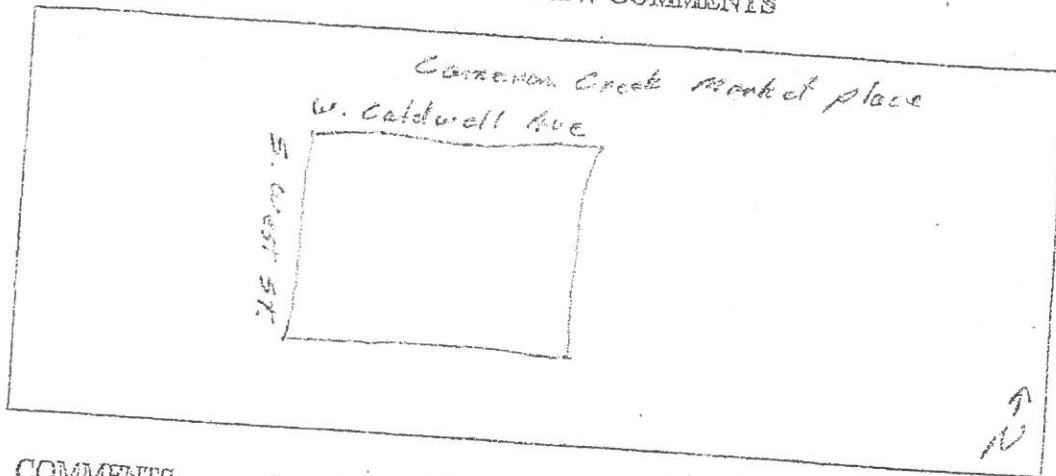


City of Visalia  
Parks and Urban Forestry  
336 N. Ben Maddox Way  
Visalia, CA 93292

Date: 10-4-16

Site Plan Review # 161910

### SITE PLAN REVIEW COMMENTS



#### COMMENTS:

See Below

☐

None

☒

- ☐ Please plot and protect all Valley Oak Trees.
- ☐ Landscape along parkway to be planted by developer and maintained by a maintenance district.
- ☐ All drainage from curb and gutter along streets to be connected to storm drain system.
- ☐ All trees planted in street right-of-way to be approved by the Public Works Superintendent of Parks.
- ☐ Tie-ins to existing infrastructure may require a bore. Check with the Public Works Department prior to any street cut.

Other Comments:

*Joel Hooyer*  
Joel Hooyer

Parks and Urban Forestry Supervisor  
559 713-4295 Fax 559 713-4818

Email: jhooyer@ci.visalia.ca.us

# SITE PLAN REVIEW COMMENTS

Paul Bernal, Planning Division (559) 713-4025

Date: February 22, 2017

SITE PLAN NO: 2016-140 RESUBMITTAL  
PROJECT TITLE: CAMERON CREEK MARKETPLACE  
DESCRIPTION: ARCO MINIMART, CAR WASH (CN ZONED) (X) (DISTRICT A)  
APPLICANT: Camron Creek  
PROP. OWNER: CAMERON CREEK MARKETPLACE LLC  
LOCATION TITLE: W CALDWELL AVE  
APN TITLE: 126-870-037  
GENERAL PLAN: Neighborhood Commercial  
EXISTING ZONING: NC (Neighborhood Commercial)

## Planning Division Recommendation:

- ☒ Revise and Proceed – Off Agenda w/ master site plan circulation  
☐ Resubmit

## Prior Entitlements

- Jack-N-Box CUP No. 2015-18 (SPR15-045)
- CUP 2005-22/Var. 2005-08/PM 2005-14
- CUP 2008-06/Var. 2008-02 (Entitlements have expired)

NOTE: All CUP required entitlements may be done under one CUP unless otherwise indicated.

## Project Requirements

- Conditional Use Permit for Gas Station
- Conditional Use Permit for Car Wash
- Conditional Use Permit Amendment for CUP No. 2005-22, which showed this portion (126-870-037) of the shopping center as a drive-thru restaurant
- Conditional Use Permit Amendment for CUP No. 2005-22, to add the access point shown on Caldwell east of the project site. This access point was conceptually shown in SPR 2015-045, but not entitled with any of their actions.
- Initial Study (new initial study required for gas station and car wash)
- Nosie Study required for the car wash
- Light Study required for car wash and service station lights
- Traffic Impact Study (see Traffic SPR comments)
- Operational Statement for both proposed components required
- Meet Sign Program for the shopping center
- Direct Stab Trash Enclosure required, see Solid Waste comments
- ADA connection to public sidewalk
- Additional Information as Needed
- Connection of main drive-way to the east as depicted on a master site plan exhibit

## PROJECT SPECIFIC INFORMATION: 02/22/2017

1. Modifications/revisions to the master site plan approved with CUP No. 2005-22 will require a CUP amendment.
2. A new initial study is required as part of the CUP entitlements.
3. Staff will require that street frontage improvements along Caldwell and West Streets (i.e., curb, gutter, sidewalk, park-strip landscaping) be installed to limits determined through the Site Plan Review process. Sidewalk, curb and gutter shall be extended down to Kimball Avenue.
4. Provide a master plan for the site integrating the proposed project. Master Plan shall show all approved access drives, limit of existing improvements, and all proposed improvements with

this project. Connection to the existing primary drive-way along Caldwell Avenue that provides vehicular access to the Jack and the Box shall be extended to the west providing vehicular access to the ARCO service station.

5. The CUP site plan shall provide depiction of the car wash queue area and payment kiosk.
6. The site plan exhibit shall call out dimensions for parking stalls and provide distance call-outs on landscape setbacks that are consistent with the setbacks approved as part of the CUP 2005-22/Variance 2005-08 approval.
7. Direct Stab Trash Enclosure is required.
8. Provide and depict the ADA connection to public sidewalk.
9. A Noise study shall be submitted with the CUP application.
10. The Photometric study shall be submitted with the CUP application.
11. NOTE: The residential area to the west is a light and noise sensitive land use.
12. Comply with all conditions for CUP No. 2005-22, Variance No. 2005-08 and Tentative Parcel Map No. 2005-14.

Staff initial finding is that the proposed site plan IS CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

#### **Design District: "K" [17.30.270]**

The following development standards shall apply to property located in district K.

New development of property within this district shall comply with the conditions of approval of the applicable planned unit development permit. New development of property not situated within a planned unit development shall conform with development standards as determined by the site plan review committee as provided in Section 17.18.020.

#### **Parking:**

1. Parking shall be provided at the unified shopping center rate of one space per 225 square feet of gross floor area (see Zoning Ordinance Section 17.34.020).
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
3. Provide handicapped space(s) (see Zoning Ordinance Section 17.34.030.H).
4. Provide bike rack(s).
5. Parking at the Pumps and under the Car Wash Drying Canopy is not counted towards meeting parking requirements.

#### **Landscaping:**

1. The City has adopted the State Water Efficient Landscape Ordinance. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance. **NOTE: Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.**
2. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
3. In the P(R-M) multi-family residential zone, all multiple family developments shall have landscaping including plants, and ground cover to be consistent with surrounding landscaping in the vicinity. Landscape plans to be approved by city staff prior to installation and occupancy

- of use and such landscaping to be permanently maintained. (Zoning Ordinance Section 17.16.180)
4. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
  5. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
  6. Provide a detailed landscape and irrigation plan as a part of the building permit package (Zoning Ordinance Section 17.34.040).
  7. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
  8. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
  9. Provide a conceptual landscape plan for resubmittal or planning commission review.
  10. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
  11. Maintenance of landscaped areas. - A landscaped area provided in compliance with the regulations prescribed in this title or as a condition of a use permit or variance shall be planted with materials suitable for screening or ornamenting the site, whichever is appropriate, and plant materials shall be maintained and replaced as needed, to screen or ornament the site. (Prior code § 7484)

**Lighting:** NOTE: Gas Island Canopy Lighting (fascia) and Illuminated Signage may be restricted or be required to be turned off based upon Light Study for the residential areas to the west.

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

#### **Drive-thru Performance Standards (Section 17.32.162) CAR WASH**

- A. Purpose and Intent: It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in association with specified use. This section does not apply to carwashes and lube and oil changing stations.
- B. Performance Standards:
  1. Separation from residences: The drive-thru lane shall be no less than 250 feet from the nearest residence or residentially zoned property.
  2. Stacking: The drive-thru lane shall contain no less than ten vehicle stacking, measured from the pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicle spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
  3. Circulation: No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive thru shall not take ingress or egress from a local residential road.



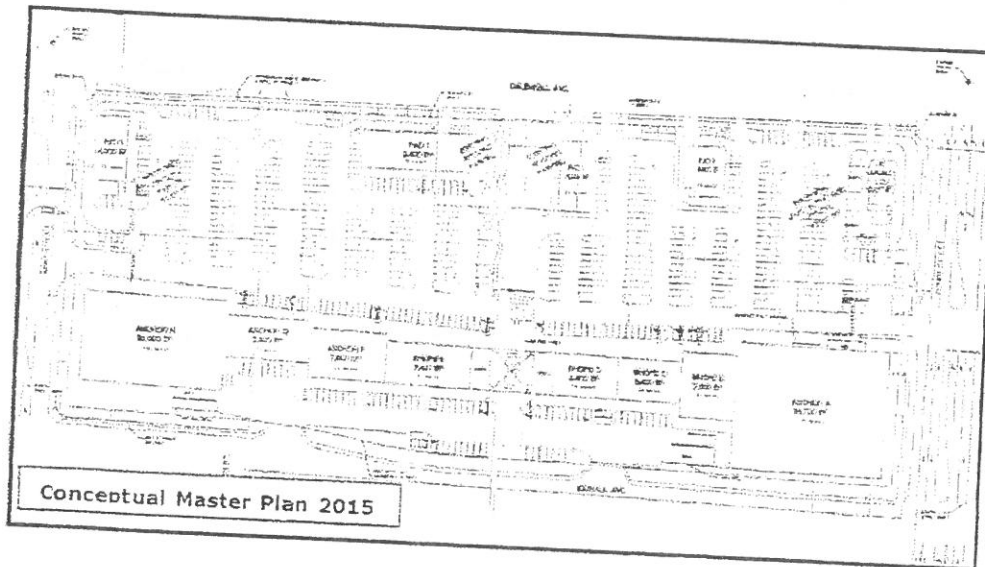
4. Noise: no component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60db between the hours of 7:00 p.m. and 6:00 a.m. daily.
5. Screening: The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.
6. Menu boards and signage: Shall be oriented or screened to avoid direct visibility from adjacent public streets.

The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

**NOTE:** Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments.

Signature \_\_\_\_\_

Direct Stab Trash Enclosure required, see Solid Waste comments  
ADA connection to public sidewalk





### Site Plan Review Comments For:

Visalia Fire Department  
Kurtis A. Brown, Fire Marshal  
707 W Acequia Avenue  
Visalia, CA 93291  
(559) 713-4261 office  
(559) 713-4808 fax

ITEM NO: 4

DATE: February 22, 2017

SITE PLAN NO:

SPR16140

RESUBMIT

PROJECT TITLE:

CAMERON CREEK ARCO AMPM

DESCRIPTION:

NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)  
CAMERON CREEK ARCO AMPM

APPLICANT:

CAMERON CREEK MARKETPLACE LLC

PROP OWNER:

SWC OF EAST CALDWELL AVE AND S COURT

LOCATION:

126-870-037

APN(S):

### The following comments are applicable when checked:

- ☒ The Site Plan Review comments are issued as general overview of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2016 California Fire Code (CFC), 2016 California Building Codes (CBC) and City of Visalia Municipal Codes.
- ☐ All fire detection, alarm, and extinguishing systems in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If a building has been vacant for a significant amount of time, the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. 2016 CFC 901.6.
- ☐ No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire & life safety requirements including fire protection.
- ☒ Construction and demolition sites prior to and during construction shall comply with the following:
  - Water Supply for fire protection, either temporary or permanent, shall be made available as soon as combustible materials arrive on the site. 2016 CFC 3312
  - An all-weather, 20 feet width Construction Access Road capable of holding a 75,000 pound fire apparatus. Fire apparatus access shall be provided within 100 feet of temporary or permanent fire department connections. 2016 CFC 3310
- ☐ More information is needed before Site Plan Review can be conducted. Please submit plans with more detailed information. Please include information on

### General

- ☒ Address numbers must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses served are by a common driveway, the range of numbers shall be posted at the roadway/driveway. 2016 CFC 505.1
- ☒ All hardware on exit doors, illuminated exit signs and emergency lighting shall comply with the 2016 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- ☒ Commercial dumpsters with 1.5 cubic yards or more shall not be stored in building or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. 2016 CFC 304.3.3

- ☒ A Knox Box key lock system is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. The key box shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. Please allow adequate time for shipping and installation. 2016 CFC 506.1

- ☐ If your business handles hazardous material in amounts that exceed the Maximum Allowable Quantities listed on *Table 5003.1.1(1), 5003.1.1(2), 5003.1.1(3) and 5003.1.1(4) of the 2016 California Fire Code*, you are required to submit an emergency response plan to the Tulare County Health Department. Also you shall indicate the quantities on your building plans and prior to the building final inspection a copy of your emergency response plan and Safety Data Sheets shall be submitted to the Visalia Fire Department.

### Water Supply for Residential, Commercial & Industrial

#### Residential

- ☐ Fire hydrant spacing and location shall comply with the following requirements:  
The exact location and number of fire hydrants shall be at the discretion of the fire marshal, fire chief and/or their designee. *Visalia Municipal Code 16.36.120(5)*
- ☐ Single-family residential developments shall be provided with fire hydrants every six hundred (600) lineal feet of residential frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- ☐ Multi-family, zero lot line clearance, mobile home park or condominium developments shall be provided with fire hydrants every four hundred (400) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- ☐ Multi-family or condominium developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every six (600) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.

#### Commercial & Industrial

- ☒ Where a portion of the facility or building is more than 400 feet from a hydrant on a fire apparatus access road, on-site fire hydrant(s) shall be provided. 2016 CFC 507.5.1
- ☐ Due to insufficient building information, the number and distance between fire hydrants cannot be determined by the Site Plan Review process. The number of fire hydrants and distance between required fire hydrants shall be determined by utilizing type of construction and square footage in accordance with *CFC 2016 Appendix C102 & C103 & CFC 507.5.1*
- ☐ To determine fire hydrant location(s) and distribution the following information was provided to the Site Plan Review committee: Type of construction \_\_\_\_\_ Square footage \_\_\_\_\_

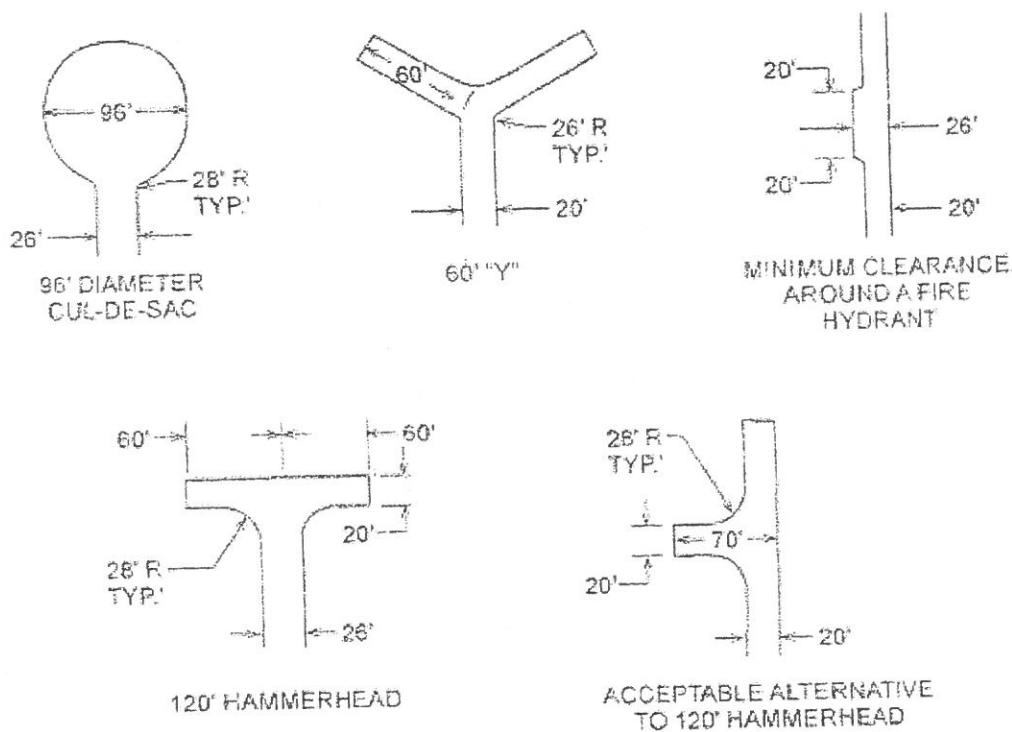
### Emergency Access

- ☐ A fire apparatus access roads shall be provided and must comply with the 2016 CFC and extend within 150 of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. Fire apparatus access roads shall have an unobstructed width of not less than 20 feet. Minimum turning radius for emergency fire apparatus shall be 20 feet inside radius and 43 feet outside radius. 2016 CFC 503.1.1

- ☐ Buildings or portions of buildings or facilities with a vertical distance between the grade plan and the highest roof surface that exceed 30 feet shall provide an approved fire apparatus access roads capable of accommodating fire department aerial apparatus.

- Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders.
- Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building.
- Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and the building.

- ☐ Fire apparatus access roads in excess of 150 feet and dead end shall be provided with a turnaround. Fire apparatus access roads with a length of 151-500 feet shall be a minimum of 20 feet in width. Length of 501-750 feet shall be 26 feet in width. *2016 CFC Table D103.4*



- ☐ Approved No PARKING – FIRE LANE signs shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. Signs shall have a minimum dimension of 12 inches wide by 18 inches high and have red letters on a white reflective background. *2013 CFC 503.3/D103.6*

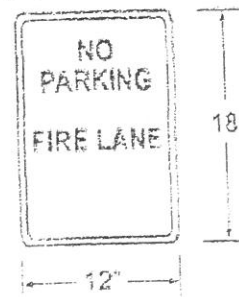
SIGN TYPE "A"



SIGN TYPE "C"



SIGN TYPE "D"



- ☒ On site Fire Apparatus Access Roads



shall be provided and have an unobstructed width of not less than the following;

- 20 feet width, exclusive of shoulders (No Parking)
- More than 26 feet width, exclusive of shoulders (No Parking one side)
- More than 32 feet wide, exclusive of shoulders (Parking permitted on both sides)

☒ Marking- approved signs, other approved notices or marking that include the words "NO PARKING- FIRE LANE shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. *CFC 503.3*

☐ Gates on access roads shall be a minimum width of 20 feet and shall comply with the following:  
*2016 CFC D103.5*

- Gates shall be of the swinging or sliding type.
- Gates shall allow manual operation by one person (power outages).
- Gates shall be maintained in an operative condition at all times.
- Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. (Note: Knox boxes shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. Please allow adequate time for shipping and installation.)

☐ Streets shall meet the City of Visalia's Design & Improvement Standards for streets to ensure that fire apparatus can make access to all structures in the event of an emergency.

#### Fire Protection Systems


☐ An automatic fire sprinkler system will be required for this building. Also, a fire hydrant is required within 50 feet of the Fire Department Connection (FDC). Where an existing building is retrofitted with a sprinkler system (NFPA 13 or NFPA 13R) a fire hydrant shall be provided within 75 feet of the FDC. An additional 25 feet of distance between a fire hydrant and FDC may be granted when a fire sprinkler Density is designed with an additional 25%. *2016 CFC 912 and Visalia Municipal Code 8.20.010 subsection C103.4*

☐ Locking fire department connection (FDC) caps are required. The caps shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. *2016 CFC 912.4.1*

☒ Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produces grease laden vapors shall be provided with a Type 1 Hood, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. *2016 CFC 904.12 & 609.2*

#### Special Comments

☒ Drive approach shall be a minimum of 20'

  
Kurtis A. Brown  
Marshal

**City of Visalia**  
**Police Department**  
303 S. Johnson St.  
Visalia, Ca. 93292  
(559) 713-4370

ITEM NO: 4

DATE: February 22, 2017

SITE PLAN NO:

SPR16140

RESUBMIT

PROJECT TITLE:

CAMERON CREEK ARCO AMPM

DESCRIPTION:

NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)

APPLICANT:

CAMERON CREEK ARCO AMPM

PROP OWNER:

CAMERON CREEK MARKETPLACE LLC

LOCATION:

SWC OF EAST CALDWELL AVE AND S COURT

APN(S):

126-670-037

## Site Plan Review Comments



No Comment at this time.



Request opportunity to comment or make recommendations as to safety issues as plans are developed.



Public Safety Impact fee:

Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code  
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. \*Refer to Engineering Site Plan comments for fee estimation.



Not enough information provided. Please provide additional information pertaining to:



Territorial Reinforcement: Define property lines (private/public space).



Access Controlled / Restricted etc:



Lighting Concerns:



Landscaping Concerns:



Traffic Concerns:



Surveillance Issues:

SECURITY / CAMERA SYSTEM



Line of Sight Issues:



Other Concerns:

O. DOMINGUEZ 1166

Visalia Police Department

QUALITY ASSURANCE DIVISION  
SITE PLAN REVIEW COMMENTS

ITEM NO: 4      DATE: February 22, 2017  
SITE PLAN NO:      SPR16140      **RESUBMIT**  
PROJECT TITLE:      CAMERON CREEK ARCO AMPM  
DESCRIPTION:      NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)  
APPLICANT:      CAMERON CREEK ARCO AMPM  
PROP OWNER:      CAMERON CREEK MARKETPLACE LLC  
LOCATION:      SWC OF EAST CALDWELL AVE AND S COURT  
APN(S):      126-870-037

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- ☒ WASTEWATER DISCHARGE PERMIT APPLICATION FOR CAR WASH
- ☒ SAND AND GREASE INTERCEPTOR - 3 COMPARTMENT FOR CAR WASH
- ☐ GREASE INTERCEPTOR min. 1000 GAL
- ☐ GARBAGE GRINDER - ¾ HP. MAXIMUM \_\_\_\_\_
- ☐ SUBMISSION OF A DRY PROCESS DECLARATION \_\_\_\_\_
- ☒ NO SINGLE PASS COOLING WATER IS PERMITTED \_\_\_\_\_
- ☐ OTHER \_\_\_\_\_
- ☐ SITE PLAN REVIEWED - NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA  
PUBLIC WORKS DEPARTMENT  
QUALITY ASSURANCE DIVISION  
7579 AVENUE 288  
VISALIA, CA 93277

*[Signature]*  
AUTHORIZED SIGNATURE  
2-22-17  
DATE



CITY OF VISALIA  
NONSIGNIFICANT WASTEWATER  
DISCHARGE PERMIT APPLICATION

SPR 16140

Agency Use:  
Permit No: \_\_\_\_\_  
Code No: \_\_\_\_\_  
Data Entry By: \_\_\_\_\_

**PLEASE PRINT OR TYPE**

APPLICANT BUSINESS NAME: \_\_\_\_\_ PHONE: \_\_\_\_\_  
BUSINESS ADDRESS: \_\_\_\_\_ CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_  
BUSINESS OWNER: \_\_\_\_\_ PHONE: \_\_\_\_\_  
MAILING ADDRESS: \_\_\_\_\_ CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_  
CONTACT PERSON: \_\_\_\_\_ TITLE: \_\_\_\_\_  
NATURE OF BUSINESS: (auto repair, car wash, machine shop, painting, battery dealer, etc.) \_\_\_\_\_

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

Does your facility conduct automotive servicing operations that involve the exchange or replacement of fluids (e.g. oil, transmission or brake fluid, radiator coolant etc.)? YES NO

Does your facility have any floor drains? YES NO

Does your facility have a steam cleaner? YES NO

Does your facility wash vehicles on site? YES NO

**If generated, how do you dispose of the following:**

Grease, oil and sand interceptor contents \_\_\_\_\_

I hereby affirm that all information furnished is true and correct  
to the best of my knowledge.

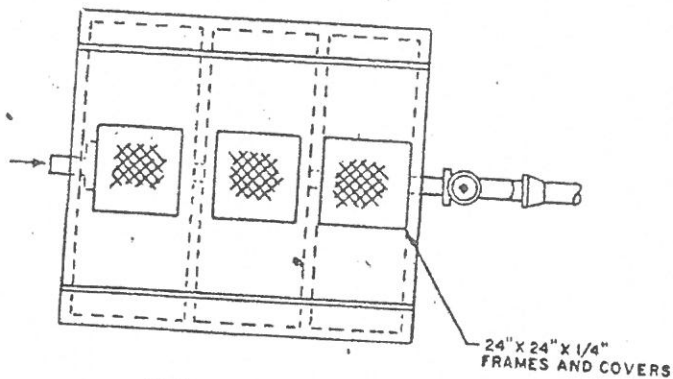
\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

Public Works Department  
Quality Assurance Division  
7579 Ave. 288  
Visalia CA 93277  
(559) 713-4487

cb\wordforms\applicationwo04



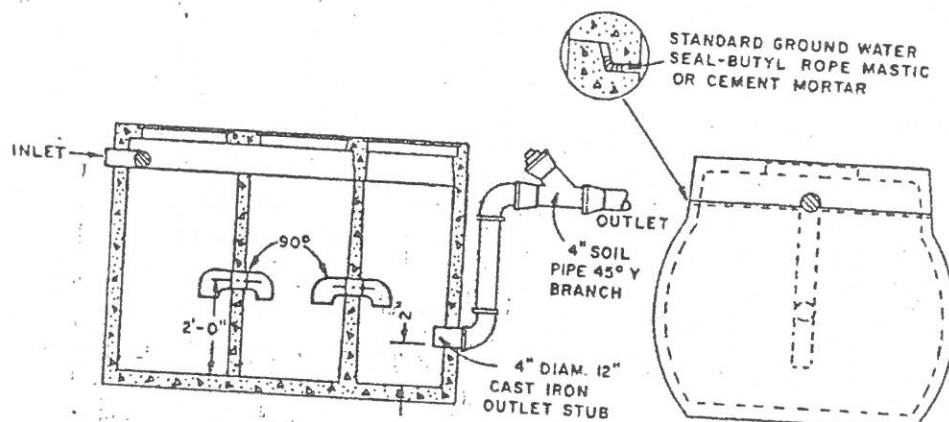


PLAN VIEW

TYPICAL PRECAST 1000 GAL.  
NO SCALE

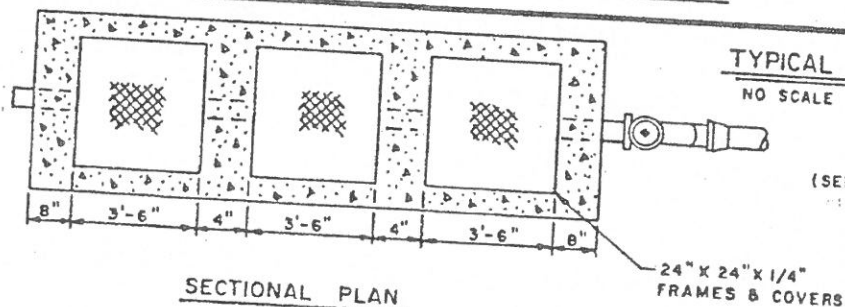
(SEE DETAIL S-9A FOR NOTES)

SEP 16 1940



LONGITUDINAL SECTION

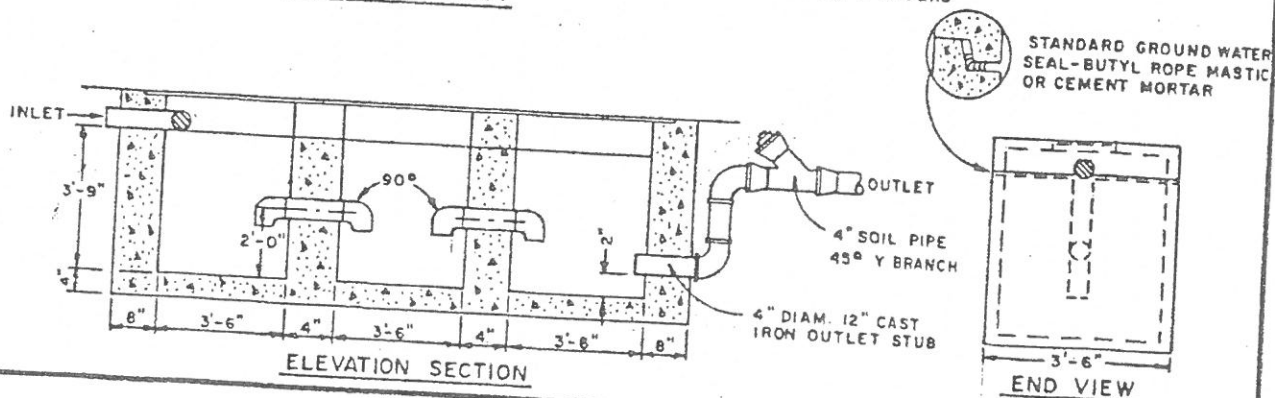
END VIEW



SECTIONAL PLAN

TYPICAL POUR-IN-PLACE  
NO SCALE  
1000 GAL.

(SEE DETAIL S-9A FOR NOTES)



ELEVATION SECTION

END VIEW

Approved by Visalia City Council on \_\_\_\_\_ DATE \_\_\_\_\_

City of Visalia  
Improvement Standards

Standard Outside Industrial Sand,  
Grease & Oil Interceptor

REVISIONS

S-9

**CITY OF VISALIA  
ORDINANCE 13.08**

**13.08.570 Traps required.**

Grease, oil and sand traps shall be provided when, in the opinion of the City, they are necessary for the protection of the sewerage system from liquid wastes containing grease in excessive amounts, or any flammable wastes, sand and other harmful ingredients; except that such traps shall not be required for buildings used solely for residential purposes. Such traps shall be required for example, on discharges from all service stations, automotive repair garages, car washes, restaurants, eating establishments and food preparation establishments, and such other commercial or industrial establishments as the city may designate. (Prior code § 4254)

**13.08.580 Construction of traps.**

All traps shall be of a type and capacity approved by the city, and shall be so located as to be readily and easily accessible for cleaning and inspection. Restaurant traps shall be gas-tight, of a type approved for restaurant use by the division of building safety. Traps for all other facilities, including service stations and garages, shall be in accordance with the adopted plan of the city for such traps or shall be the approved equal thereof as determined by the director. (Prior code § 4255)

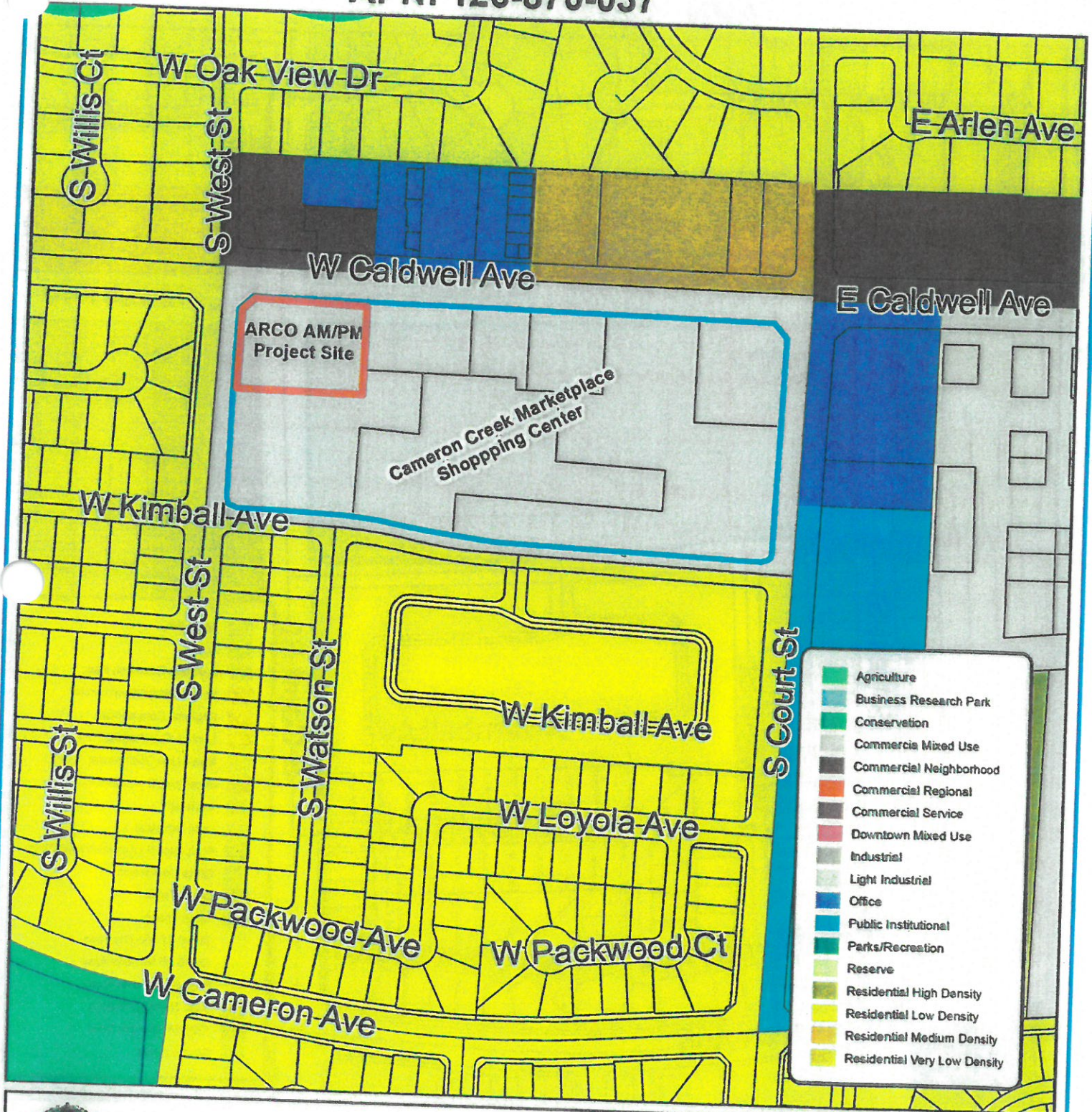
**13.08.590 Maintenance of traps.**

When installed, all grease, oil and sand traps shall be maintained by the owner, at owner's expense, in continuously efficient operation at all times. (Prior code § 4256)



# Conditional Use Permit No. 2017-22

## APN: 126-870-037



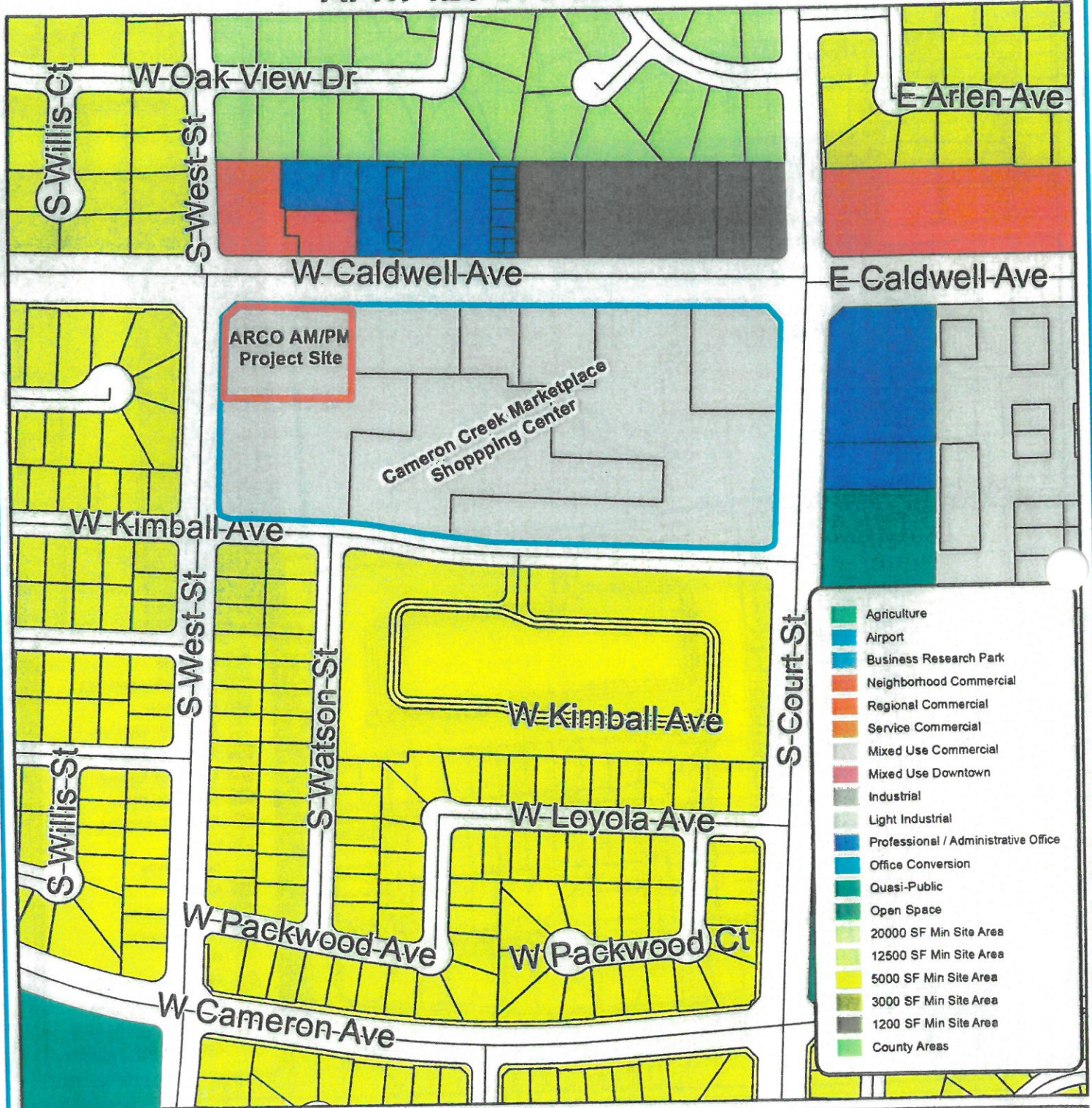
# General Plan Land Use Map





# Conditional Use Permit No. 2017-22

APN: 126-870-037



## Zoning Map





# Conditional Use Permit No. 2017-22

APN: 126-870-037



## 2016 Aerial Photo





# **City of Visalia**

## **Memo**



**To:** Visalia Planning Commission  
**From:** Paul Bernal, Principal Planner (713-4025)  
**Date:** August 28, 2017  
**Re:** Item No. 9 CUP No. 2017-22 – Late Correspondence

---

Attached is a supplemental letter received by the Planning Division on Wednesday August 23, 2017 from William Resser commenting on Conditional Use Permit No. 2017-22, a request by a request by ARCO AM/PM to construct a new ARCO AM/PM gas station consisting of a 6,500 square foot convenience store building, a 3,850 square foot automated carwash building, and a 6,800 square foot fuel canopy with 10 fuel dispensers within the Cameron Creek Shopping Center.

Mr. William Resser's original received on August 21, 2017 is attached to the staff report as Exhibit G. Both letter state concerns and objections to the mitigated negative declaration and the project.

The letter was received after the Planning Commission packet was copied for distribution.

August 22, 2017

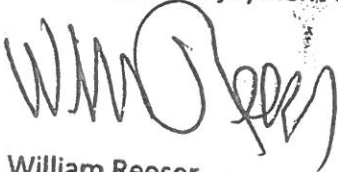
RECEIVED

AUG 23 2017

COMM. DEVELOP.  
CITY OF VISALIA

Comments on proposed Mitigated Negative Declaration

1. Noise is defined as unwanted or objectionable sound. The effects of noise on people can include general annoyance, interference with speech communication, sleep disturbance, and in extreme circumstances, hearing impairment. All of which will occur if this MND is allowed to be approved by the City.
2. Distance from noise emitting sources is too close to existing homes. Evening and nighttime hours for levels of noise (7:00PM to 10:00 PM) is in excess of CNEL.
3. Vibration, from heavy trucks during the operating hours exceed the standard.
4. As stated in the MNC the noise level for proposed dryers in above the city's standards.
5. No remedy for home owners with small children and varying hours of work, not all families work from 8 to 5 Monday through Friday. Noise levels created by this proposed MND will be everyday not just on a normal work schedule meaning Monday to Friday.
6. Added traffic on West Street will increase vibrations and noise levels
7. Construction will add dust and possible health issues due to air borne elements.
8. Crime will increase within the proposed site, has the Visalia Police Dept been allowed to evaluate the site.
9. Proposed curb cuts shift traffic on West to single family area.
10. Speed limits are not consistent with existing city requirements.
11. Traffic counts will significantly increase on existing road way further creating noise.
12. Lighting from proposed MND will create glare and unreasonable lighting for homes on West street. Lighting has not been address in the MND.
13. No need for another gas station in this area, 4 exist with a 1 mile radius.
14. Quiet enjoyment of life is the main reason people choose Visalia.



William Reeser



MEETING DATE February 22, 2017  
SITE PLAN NO. 16-140  
PARCEL MAP NO.  
SUBDIVISION  
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

☐ **RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

☐ During site plan design/policy concerns were identified, schedule a meeting with  
☐ Planning ☐ Engineering prior to resubmittal plans for Site Plan Review.

☐ Solid Waste ☐ Parks and Recreation ☐ Fire Dept.

☒ **REVISE AND PROCEED** (see below)

☒ A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

☐ Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.

☒ Your plans must be reviewed by:

☐ CITY COUNCIL

☐ REDEVELOPMENT

☒ PLANNING COMMISSION

☐ PARK/RECREATION

☒ CUP

☐ HISTORIC PRESERVATION

☐ OTHER -TCUP

☒ **ADDITIONAL COMMENTS: OFF AGENDA FOR INTERNAL DRIVE AISLE DESIGN**

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.2  
*Site Plan Review Committee*





**BUILDING/DEVELOPMENT PLAN  
REQUIREMENTS  
ENGINEERING DIVISION**

☐ Jason Huckleberry 713-4259  
☒ Adrian Rubalcaba 713-4271

ITEM NO: 4 DATE: FEBRUARY 22, 2017

SITE PLAN NO.: 16-140 RESUBMITTAL  
PROJECT TITLE: CAMERON CREEK ARCO AMPM  
DESCRIPTION: NEW SERVICE STATION W/ 6000SF  
CONVENIENCE STORE, 10 DISPENSER CANOPY  
AND AUTOMATED CARWASH. STORE TO  
OPERATE 24 HOUR W/ SALES OF BEER AND  
WINE  
APPLICANT: CAMERON CREEK ARCO AMPM  
PROP OWNER: CAMERON CREEK MARKETPLACE LLC  
LOCATION:  
APN: 126-870-037

**SITE PLAN REVIEW COMMENTS**

☒ REQUIREMENTS (indicated by checked boxes)

- ☒ Install curb return with ramp, with 35' radius; **WEST & CALDWELL, SEE ADDL COMMENTS**
- ☒ Install curb; ☒ gutter **WEST ST.**
- ☒ Drive approach size; ☒ Use radius return; **REFER TO CITY COMMERCIAL STDS**
- ☒ Sidewalk: 6' width; ☒ 10' parkway width at **WEST ST.**
- ☒ Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- ☒ Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.

☐ Right-of-way dedication required. A title report is required for verification of ownership.

☒ Deed required prior to issuing building permit; **SIDEWALK EASEMENTS**

☒ City Encroachment Permit Required. **FOR ALL WORK IN PUBLIC RIGHT-OF-WAY**

Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.

☐ CalTrans Encroachment Permit required. ☐ CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;

☐ Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.

☒ Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.

☒ Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. ☒ Prepared by registered civil engineer or project architect. ☒ All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) ☒ directed to the City's existing storm drainage system; b) ☐ directed to a permanent on-site basin; or c) ☐ directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: : maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance. **CONNECT TO ONSITE STORM DRAIN DESIGN**

☒ Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.

☒ Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)

☒ Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.

☒ All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications. **WEST ST**

- ☒ Traffic indexes per city standards: **REFER TO CITY COLLECTOR STREET STDS & SPECS**
- ☒ Install street striping as required by the City Engineer. **FURTHER COORDINATE WITH TRAFFIC DEPT.**
- ☒ Install landscape curbing (typical at parking lot planters). **ONSITE AS NECESSARY**
- ☒ Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- ☒ Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- ☒ Provide "R" value tests: 1 each at **300' INTERVALS**
- ☐ Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- ☐ Access required on ditch bank, 15' minimum ☐ Provide wide riparian dedication from top of bank.
- ☐ Show Oak trees with drip lines and adjacent grade elevations. ☐ Protect Oak trees during construction in accordance with City requirements.
- ☐ A permit is required to remove oak trees. Contact Joel Hooyer at 713-4295 for an Oak tree evaluation or permit to remove. ☐ A pre-construction conference is required.
- ☒ Relocate existing utility poles and/or facilities.
- ☐ Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- ☐ Subject to existing Reimbursement Agreement to reimburse prior developer:
- ☒ Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- ☒ If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- ☒ If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- ☐ Comply with prior comments. ☐ Resubmit with additional information. ☒ Redesign required.

**Additional Comments:**

**1. Existing curb ramp at West & Caldwell is required to comply with current City std's. Modify or remove & replace accordingly.**

**2. Street section for West St. shall meet City collector design standards. A 10' parkway and 6' sidewalk is required. An easement for the sidewalk area shall be deeded to the City, refer to City document format and procedures available on the City website.**

**3. West St. shall be improved to Kimball Ave. Curb, gutter, parkway landscaping, street lights, sidewalk, and pavement is required. The northeast corner curb ramp return at Kimball and West shall be installed with City std. 30' radius return.**

**4. Additional street striping will be required on West St.**

**5. Proposed drive approach location on West St. is adequate.**

**6. An access drive onsite to surround the parcel and connect to existing onsite drive aisle east is required. A copy of the granted egress/ingress from adjacent parcels owners is required.**

**7. Street frontages shall be landscaped and comply with State MWELO regulations. Improvement plans shall accompany onsite building plans.**

**8. The trash enclosure location and position will allow for a direct-stab by a Solid Waste vehicle however additional pavement is required in front of the enclosure for adequate Solid Waste vehicle maneuverability. Comply with City 24' enclosure standards with concrete apron and gates.**

9. Sewer and storm drain utility connections shall be from existing onsite master design.
10. Refer to Traffic Safety Dept. comments for additional requirements.
11. Refer to Planning Dept. comments for additional requirements.
12. An accessible path of travel from the public sidewalk to building entrance is required.
13. It is typical to see an emergency escape exit for vehicles in carwash queue.
14. Proposed development will incur impact fees associated with the land development, fueling stations, convenience store and internal food services, and carwash. Refer to page 4 for applicable fees.
15. Resubmit Off-Agenda for required onsite drive aisle and connection to existing to the east.



## SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: **16-140 RESUBMITTAL**

Date: **2/22/2017**

### **Summary of applicable Development Impact Fees to be collected at the time of building permit:**

**(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)**

(Fee Schedule Date: **10/1/2016**)

(Project type for fee rates: **VARIES**)

☐ Existing uses may qualify for credits on Development Impact Fees.

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	\$1,226/AC X 1.6 = \$1,961.60
<input checked="" type="checkbox"/> Transportation Impact Fee	\$13,825/1KSF X 10.9 = \$150,693 GAS STATION 20 POSITIONS = \$332,504
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	RETAIL \$23/1KSF X 6 = \$138 CARWASH \$18,588/EACH X 1 WALK-UP \$1,278/EACH X TBD TREATMENT PLANT FEES: RETAIL \$53/1KSF X 6 = \$318 CARWASH \$20,239/EACH X 1 WALK-UP \$5,613/EACH X TBD
<input checked="" type="checkbox"/> Sewer Front Foot Fee	\$41/LF X 200' (CALDWELL) = \$8,200
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	\$6,554/AC X 1.6 = \$10,486.40
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	\$4,811/AC X 1.6 = \$7,697.60
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	\$8,223/AC X 1.6 = \$13,156.80
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	\$1,799/AC X 1.6 = \$2,878.40
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$316/1000SF X 10.9 = \$3,444.40
<input type="checkbox"/> Parking In-Lieu	

### **Reimbursement:**

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.

  
Adrian Rubalcaba

# SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

February 22, 2017

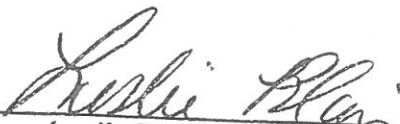
ITEM NO: 4	RESUBMTL
SITE PLAN NO:	SPR16140
PROJECT TITLE:	CAMERON CREEK ARCO AMPM
DESCRIPTION:	NEW SERVICE STATION W/ 6000SF CONVENIENCE STORE, 10 DISPENSER CANOPY AND AUTOMATED CARWASH. STORE TO OPERATE 24 HOUR W/ SALES OF BEER AND WINE (CN) (X) (A)
APPLICANT:	CAMERON CREEK ARCO AMPM
PROP. OWNER:	CAMERON CREEK MARKETPLACE LLC
LOCATION:	SWC OF EAST CALDWELL AVE AND S COURT STREET
APN(S):	126-870-037

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- ☐ No Comments
- ☒ See Previous Site Plan Comments
- ☒ Install Street Light(s) per City Standards.
- ☐ Install Street Name Blades at                      Locations.
- ☒ Install Stop Signs at *driveway exit* Locations.
- ☒ Construct parking per City Standards PK-1 through PK-4.
- ☒ Construct drive approach per City Standards.
- ☐ Traffic Impact Analysis required.
  - ☐ Provide more traffic information such as                      . Depending on development size, characteristics, etc., a TIA may be required.

## Additional Comments:

- Show access queuing.
- Connect to main drive aisle off Caldwell

  
Leslie Blair

City of Visalia

Building: Site Plan  
Review Comments

ITEM NO: 4

DATE: February 22, 2017

SITE PLAN NO:

SPR16140

RESUBMIT

PROJECT TITLE:

CAMERON CREEK ARCO AMPM

DESCRIPTION:

NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)

APPLICANT:

CAMERON CREEK ARCO AMPM

PROP OWNER:

CAMERON CREEK MARKETPLACE LLC

LOCATION:

SWC OF EAST CALDWELL AVE AND S COURT

126-870-037

NOTE: These are general comments and DO NOT const APN(S):

Please refer to the applicable California Codes & local ordinance for additional requirements.

☐ Business Tax Certification is required.

For information call (559) 713-4326

☒ A building permit will be required.

For information call (559) 713-4444

☒ Submit 4 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)

☐ Submit 4 sets of plans prepared by an architect or engineer. Must comply with 2013 California Building Cod Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.

☐ Indicate abandoned wells, septic systems and excavations on construction plans.

You are responsible to ensure compliance with the following checked items:

☒ Meet State and Federal requirements for accessibility for persons with disabilities.

☒ A path of travel, parking, common area and public right of way must comply with requirements for access for persons with disabilities.

☐ Multi family units shall be accessible or adaptable for persons with disabilities.

☐ Maintain sound transmission control between units minimum of 50 STC.

☐ Maintain fire-resistive requirements at property lines.

☐ A demolition permit & deposit is required.

For information call (559) 713-4444

☐ Obtain required clearance from San Joaquin Valley Air Pollution Board. Prior to any demolition work

For information call (661) 392-5500

☒ Location of cashier must provide clear view of gas pump island

☒ Plans must be approved by the Tulare County Health Department.

For information call (559) 624-7400

☐ Project is located in flood zone \_\_\_\_\_ \* ☐ Hazardous materials report.

☐ Arrange for an on-site inspection. (Fee for inspection \$157.00)

For information call (559) 713-4444

☒ School Development fees. Commercial \$0.56 per square foot. Residential \$3.75 per square foot.

☐ Existing address must be changed to be consistent with city address.

For information call (559) 713-4320

☐ Acceptable as submitted

☐ No comments

☐ See previous comments dated: \_\_\_\_\_

Special comments: MEET THE NVELO REQUIREMENTS FOR THE  
NEW LANDSCAPING. PROVIDE CREEK  
INTERCEPTOR.

Signature

Date:

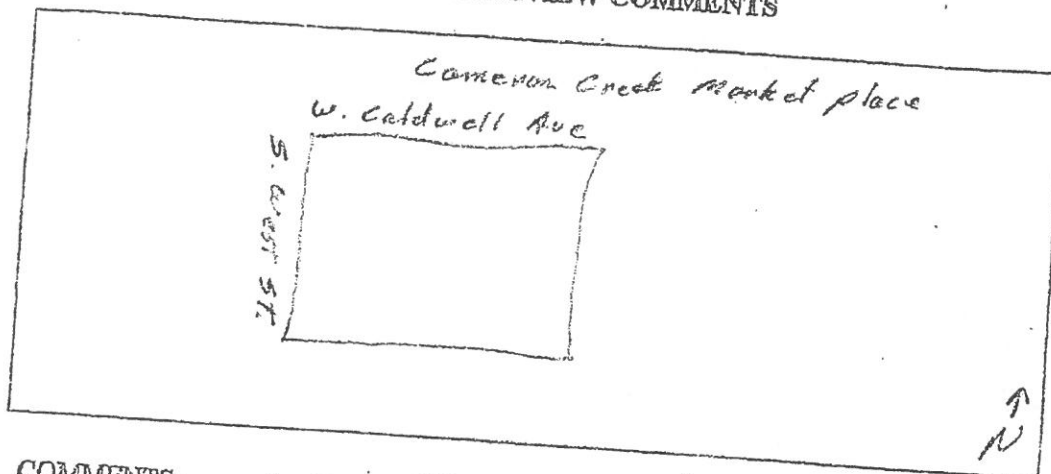
2/22/17



Date: 10-4-16

Site Plan Review # 16140

SITE PLAN REVIEW COMMENTS



COMMENTS:

See Below

☐

None

☒

- ☐ Please plot and protect all Valley Oak Trees.
- ☐ Landscape along parkway to be planted by developer and maintained by a maintenance district.
- ☐ All drainage from curb and gutter along streets to be connected to storm drain system.
- ☐ All trees planted in street right-of-way to be approved by the Public Works Superintendent of Parks.
- ☐ Tie-ins to existing infrastructure may require a bore. Check with the Public Works Department prior to any street cut.

Other Comments:

*Joel Hooyer*  
Joel Hooyer

Parks and Urban Forestry Supervisor  
559 713-4295 Fax 559 713-4818

Email: jhooyer@ci.visalia.ca.us

# SITE PLAN REVIEW COMMENTS

Paul Bernal, Planning Division (559) 713-4025

Date: February 22, 2017

SITE PLAN NO: 2016-140 RESUBMITTAL  
PROJECT TITLE: CAMERON CREEK MARKETPLACE  
DESCRIPTION: ARCO MINIMART, CAR WASH (CN ZONED) (X) (DISTRICT A)  
APPLICANT: Camron Creek  
PROP. OWNER: CAMERON CREEK MARKETPLACE LLC  
LOCATION TITLE: W CALDWELL AVE  
APN TITLE: 126-870-037  
GENERAL PLAN: Neighborhood Commercial  
EXISTING ZONING: NC (Neighborhood Commercial)

## Planning Division Recommendation:

- ☒ Revise and Proceed – Off Agenda w/ master site plan circulation  
☐ Resubmit

## Prior Entitlements

- Jack-N-Box CUP No. 2015-18 (SPR15-045)
- CUP 2005-22/Var. 2005-08/PM 2005-14
- CUP 2008-06/Var. 2008-02 (Entitlements have expired)

NOTE: All CUP required entitlements may be done under one CUP unless otherwise indicated.

## Project Requirements

- Conditional Use Permit for Gas Station
- Conditional Use Permit for Car Wash
- Conditional Use Permit Amendment for CUP No. 2005-22, which showed this portion (126-870-037) of the shopping center as a drive-thru restaurant
- Conditional Use Permit Amendment for CUP No. 2005-22, to add the access point shown on Caldwell east of the project site. This access point was conceptually shown in SPR 2015-045, but not entitled with any of their actions.
- Initial Study (new initial study required for gas station and car wash)
- Nosie Study required for the car wash
- Light Study required for car wash and service station lights
- Traffic Impact Study (see Traffic SPR comments)
- Operational Statement for both proposed components required
- Meet Sign Program for the shopping center
- Direct Stab Trash Enclosure required, see Solid Waste comments
- ADA connection to public sidewalk
- Additional Information as Needed
- Connection of main drive-way to the east as depicted on a master site plan exhibit

## PROJECT SPECIFIC INFORMATION: 02/22/2017

1. Modifications/revisions to the master site plan approved with CUP No. 2005-22 will require a CUP amendment.
2. A new initial study is required as part of the CUP entitlements.
3. Staff will require that street frontage improvements along Caldwell and West Streets (i.e., curb, gutter, sidewalk, park-strip landscaping) be installed to limits determined through the Site Plan Review process. Sidewalk, curb and gutter shall be extended down to Kimball Avenue.
4. Provide a master plan for the site integrating the proposed project. Master Plan shall show all approved access drives, limit of existing improvements, and all proposed improvements with

- this project. Connection to the existing primary drive-way along Caldwell Avenue that provides vehicular access to the Jack and the Box shall be extended to the west providing vehicular access to the ARCO service station.
5. The CUP site plan shall provide depiction of the car wash queue area and payment kiosk.
  6. The site plan exhibit shall call out dimensions for parking stalls and provide distance call-outs on landscape setbacks that are consistent with the setbacks approved as part of the CUP 2005-22/Variance 2005-08 approval.
  7. Direct Stab Trash Enclosure is required.
  8. Provide and depict the ADA connection to public sidewalk.
  9. A Noise study shall be submitted with the CUP application.
  10. The Photometric study shall be submitted with the CUP application.
  11. NOTE: The residential area to the west is a light and noise sensitive land use.
  12. Comply with all conditions for CUP No. 2005-22, Variance No. 2005-08 and Tentative Parcel Map No. 2005-14.

Staff initial finding is that the proposed site plan IS CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

#### **Design District: "K" [17.30.270]**

The following development standards shall apply to property located in district K.

New development of property within this district shall comply with the conditions of approval of the applicable planned unit development permit. New development of property not situated within a planned unit development shall conform with development standards as determined by the site plan review committee as provided in Section 17.18.020.

#### **Parking:**

1. Parking shall be provided at the unified shopping center rate of one space per 225 square feet of gross floor area (see Zoning Ordinance Section 17.34.020).
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
3. Provide handicapped space(s) (see Zoning Ordinance Section 17.34.030.H).
4. Provide bike rack(s).
5. Parking at the Pumps and under the Car Wash Drying Canopy is not counted towards meeting parking requirements.

#### **Landscaping:**

1. The City has adopted the State Water Efficient Landscape Ordinance. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance. **NOTE: Prior to a final for the project, a signed Certificate of Compliance for the MWEL standards is required indicating that the landscaping has been installed to MWEL standards.**
2. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
3. In the P(R-M) multi-family residential zone, all multiple family developments shall have landscaping including plants, and ground cover to be consistent with surrounding landscaping in the vicinity. Landscape plans to be approved by city staff prior to installation and occupancy

- of use and such landscaping to be permanently maintained. (Zoning Ordinance Section 17.16.180)
4. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
  5. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
  6. Provide a detailed landscape and irrigation plan as a part of the building permit package (Zoning Ordinance Section 17.34.040).
  7. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
  8. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
  9. Provide a conceptual landscape plan for resubmittal or planning commission review.
  10. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
  11. Maintenance of landscaped areas. - A landscaped area provided in compliance with the regulations prescribed in this title or as a condition of a use permit or variance shall be planted with materials suitable for screening or ornamenting the site, whichever is appropriate, and plant materials shall be maintained and replaced as needed, to screen or ornament the site. (Prior code § 7484)

**Lighting:** NOTE: Gas Island Canopy Lighting (fascia) and Illuminated Signage may be restricted or be required to be turned off based upon Light Study for the residential areas to the west.

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

#### **Drive-thru Performance Standards (Section 17.32.162) CAR WASH**

- A. Purpose and Intent: It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in association with specified use. This section does not apply to carwashes and lube and oil changing stations.
- B. Performance Standards:
  1. **Separation from residences:** The drive-thru lane shall be no less than 250 feet from the nearest residence or residentially zoned property.
  2. **Stacking:** The drive-thru lane shall contain no less than ten vehicle stacking, measured from the pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicle spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
  3. **Circulation:** No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive thru shall not take ingress or egress from a local residential road.



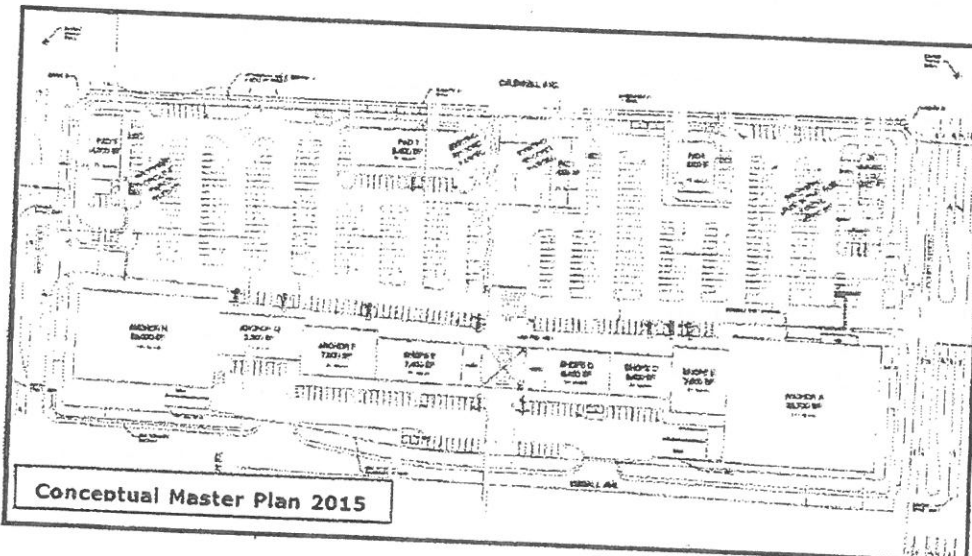
4. Noise: no component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60db between the hours of 7:00 p.m. and 6:00 a.m. daily.
5. Screening: The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.
6. Menu boards and signage: Shall be oriented or screened to avoid direct visibility from adjacent public streets.

The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments.

Signature 

Direct Stab Trash Enclosure required, see Solid Waste comments  
ADA connection to public sidewalk



**Site Plan Review Comments For:**

Visalia Fire Department  
Kurtis A. Brown, Fire Marshal  
707 W Acequia Avenue  
Visalia, CA 93291  
(559) 713-4261 office  
(559) 713-4808 fax

ITEM NO: 4

DATE: February 22, 2017

SITE PLAN NO:

SPR16140

**RESUBMIT**

PROJECT TITLE:

CAMERON CREEK ARCO AMPM

DESCRIPTION:

NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)

APPLICANT:

CAMERON CREEK ARCO AMPM

PROP OWNER:

CAMERON CREEK MARKETPLACE LLC

LOCATION:

SWC OF EAST CALDWELL AVE AND S COURT

APN(S):

126-870-037

**The following comments are applicable when checked:**

- ☒ The Site Plan Review comments are issued as general overview of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2016 California Fire Code (CFC), 2016 California Building Codes (CBC) and City of Visalia Municipal Codes.
- ☐ All fire detection, alarm, and extinguishing systems in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If a building has been vacant for a significant amount of time, the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. *2016 CFC 901.6.*
- ☐ No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire & life safety requirements including fire protection.
- ☒ Construction and demolition sites prior to and during construction shall comply with the following:
- Water Supply for fire protection, either temporary or permanent, shall be made available as soon as combustible materials arrive on the site. *2016 CFC 3312*
  - An all-weather, 20 feet width Construction Access Road capable of holding a 75,000 pound fire apparatus. Fire apparatus access shall be provided within 100 feet of temporary or permanent fire department connections. *2016 CFC 3310*
- ☐ More information is needed before Site Plan Review can be conducted. Please submit plans with more detailed information. Please include information on

**General**

- ☒ Address numbers must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses served are by a common driveway, the range of numbers shall be posted at the roadway/driveway. *2016 CFC 505.1*
- ☒ All hardware on exit doors, illuminated exit signs and emergency lighting shall comply with the 2016 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- ☒ Commercial dumpsters with 1.5 cubic yards or more shall not be stored in building or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. *2016 CFC 304.3.3*

- ☒ A Knox Box key lock system is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. The key box shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. Please allow adequate time for shipping and installation. *2016 CFC 506.1*

- ☐ If your business handles hazardous material in amounts that exceed the Maximum Allowable Quantities listed on *Table 5003.1.1(1), 5003.1.1(2), 5003.1.1(3) and 5003.1.1(4) of the 2016 California Fire Code*, you are required to submit an emergency response plan to the Tulare County Health Department. Also you shall indicate the quantities on your building plans and prior to the building final inspection a copy of your emergency response plan and Safety Data Sheets shall be submitted to the Visalia Fire Department.

### Water Supply for Residential, Commercial & Industrial

#### Residential

- ☐ Fire hydrant spacing and location shall comply with the following requirements:  
The exact location and number of fire hydrants shall be at the discretion of the fire marshal, fire chief and/or their designee. *Visalia Municipal Code 16.36.120(5)*
- ☐ Single-family residential developments shall be provided with fire hydrants every six hundred (600) lineal feet of residential frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- ☐ Multi-family, zero lot line clearance, mobile home park or condominium developments shall be provided with fire hydrants every four hundred (400) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- ☐ Multi-family or condominium developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every six (600) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.

#### Commercial & Industrial

- ☒ Where a portion of the facility or building is more than 400 feet from a hydrant on a fire apparatus access road, on-site fire hydrant(s) shall be provided. *2016 CFC 507.5.1*
- ☐ Due to insufficient building information, the number and distance between fire hydrants cannot be determined by the Site Plan Review process. The number of fire hydrants and distance between required fire hydrants shall be determined by utilizing type of construction and square footage in accordance with *CFC 2016 Appendix C102 & C103 & CFC 507.5.1*
- ☐ To determine fire hydrant location(s) and distribution the following information was provided to the Site Plan Review committee: **Type of construction** \_\_\_\_\_ **Square footage** \_\_\_\_\_

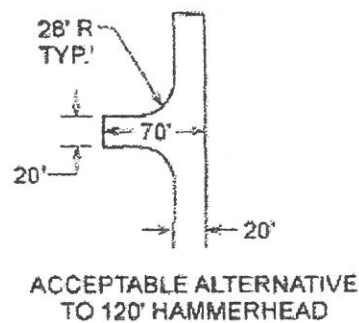
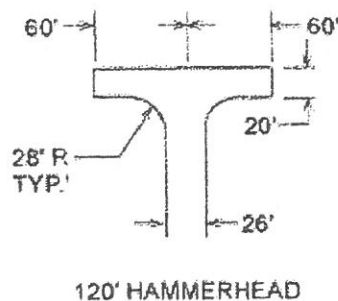
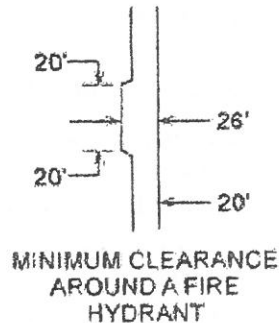
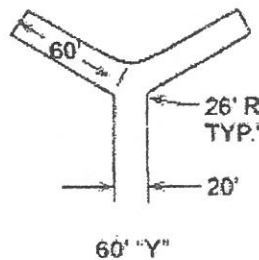
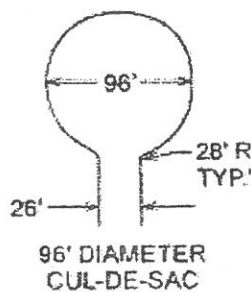
### Emergency Access

- ☐ A fire apparatus access roads shall be provided and must comply with the 2016 CFC and extend within 150 of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. Fire apparatus access roads shall have an unobstructed width of not less than 20 feet. Minimum turning radius for emergency fire apparatus shall be 20 feet inside radius and 43 feet outside radius. *2016 CFC 503.1.1*

- ☐ Buildings or portions of buildings or facilities with a vertical distance between the grade plan and the highest roof surface that exceed 30 feet shall provide an approved fire apparatus access roads capable of accommodating fire department aerial apparatus.

- Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders.
- Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building.
- Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and the building.

- ☐ Fire apparatus access roads in excess of 150 feet and dead end shall be provided with a turnaround. Fire apparatus access roads with a length of 151-500 feet shall be a minimum of 20 feet in width. Length of 501-750 feet shall be 26 feet in width. 2016 CFC Table D103.4



- ☐ Approved No PARKING – FIRE LANE signs shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. Signs shall have a minimum dimension of 12 inches wide by 18 inches high and have red letters on a white reflective background. 2013 CFC 503.3/D103.6

SIGN TYPE "A"



SIGN TYPE "C"



SIGN TYPE "D"



- ☒ On site Fire Apparatus Access Roads



shall be provided and have an unobstructed width of not less than the following;

- 20 feet width, exclusive of shoulders (No Parking)
- More than 26 feet width, exclusive of shoulders (No Parking one side)
- More than 32 feet wide, exclusive of shoulders (Parking permitted on both sides)

☒ Marking- approved signs, other approved notices or marking that include the words "NO PARKING-FIRE LANE shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. *CFC 503.3*

☐ Gates on access roads shall be a minimum width of 20 feet and shall comply with the following:  
*2016 CFC D103.5*

- Gates shall be of the swinging or sliding type.
- Gates shall allow manual operation by one person (power outages).
- Gates shall be maintained in an operative condition at all times.
- Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. (Note: Knox boxes shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. Please allow adequate time for shipping and installation.)

☐ Streets shall meet the City of Visalia's Design & Improvement Standards for streets to ensure that fire apparatus can make access to all structures in the event of an emergency.

#### Fire Protection Systems


☐ An automatic fire sprinkler system will be required for this building. Also, a fire hydrant is required within 50 feet of the Fire Department Connection (FDC). Where an existing building is retrofitted with a sprinkler system (NFPA 13 or NFPA 13R) a fire hydrant shall be provided within 75 feet of the FDC. An additional 25 feet of distance between a fire hydrant and FDC may be granted when a fire sprinkler Density is designed with an additional 25%. *2016 CFC 912 and Visalia Municipal Code 8.20.010 subsection C103.4*

☐ Locking fire department connection (FDC) caps are required. The caps shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 707 W. Acequia, Visalia, CA 93291. *2016 CFC 912.4.1*

☒ Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produces grease laden vapors shall be provided with a Type 1 Hood, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. *2016 CFC 904.12 & 609.2*

#### Special Comments

☒ Drive approach shall be a minimum of 20'

  
Kurtis A. Brown  
Fire Marshal

**City of Visalia**  
**Police Department**  
303 S. Johnson St.  
Visalia, Ca. 93292  
(559) 713-4370

ITEM NO: 4      DATE: February 22, 2017  
SITE PLAN NO: SPR16140      **RESUBMIT**  
PROJECT TITLE: CAMERON CREEK ARCO AMPM  
DESCRIPTION: NEW SERVICE STATION W/ 6000SF CONVENIENCE  
STORE, 10 DISPENSER CANOPY AND AUTOMATED  
CARWASH. STORE TO OPERATE 24 HOUR W/  
SALES OF BEER AND WINE (CN) (X) (A)  
APPLICANT: CAMERON CREEK ARCO AMPM  
PROP OWNER: CAMERON CREEK MARKETPLACE LLC  
LOCATION: SWC OF EAST CALDWELL AVE AND S COURT  
APN(S): 126-870-037

## Site Plan Review Comments

- ☒ No Comment at this time.
- ☐ Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- ☐ Public Safety Impact fee:  
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code  
Effective date - August 17, 2001
- Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. \*Refer to Engineering Site Plan comments for fee estimation.
- ☐ Not enough information provided. Please provide additional information pertaining to:
- ☐ Territorial Reinforcement: Define property lines (private/public space).
- ☐ Access Controlled / Restricted etc:
- ☐ Lighting Concerns:
- ☐ Landscaping Concerns:
- ☐ Traffic Concerns:
- ☒ Surveillance Issues: SECURITY / CAMERA SYSTEM
- ☐ Line of Sight Issues:
- ☐ Other Concerns:  
O. DOMINGUEZ 466

Visalia Police Department

QUALITY ASSURANCE DIVISION  
SITE PLAN REVIEW COMMENTS

ITEM NO: 4      DATE: February 22, 2017  
SITE PLAN NO:      SPR16140      **RESUBMIT**  
PROJECT TITLE:      CAMERON CREEK ARCO AMPM  
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STORE, 10 DISPENSER CANOPY AND AUTOMATED  
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SALES OF BEER AND WINE (CN) (X) (A)  
CAMERON CREEK ARCO AMPM  
APPLICANT:      CAMERON CREEK MARKETPLACE LLC  
PROP OWNER:      SWC OF EAST CALDWELL AVE AND S COURT  
LOCATION:      126-870-037  
APN(S):

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- ☒ WASTEWATER DISCHARGE PERMIT APPLICATION FOR CAR WASH
- ☒ SAND AND GREASE INTERCEPTOR - 3 COMPARTMENT FOR CAR WASH
- ☐ GREASE INTERCEPTOR min. 1000 GAL
- ☐ GARBAGE GRINDER -  $\frac{3}{4}$  HP. MAXIMUM \_\_\_\_\_
- ☐ SUBMISSION OF A DRY PROCESS DECLARATION \_\_\_\_\_
- ☒ NO SINGLE PASS COOLING WATER IS PERMITTED \_\_\_\_\_
- ☐ OTHER \_\_\_\_\_
- ☐ SITE PLAN REVIEWED - NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA  
PUBLIC WORKS DEPARTMENT  
QUALITY ASSURANCE DIVISION  
7579 AVENUE 288  
VISALIA, CA 93277

*John P. King*  
AUTHORIZED SIGNATURE  
2-22-17  
DATE



**CITY OF VISALIA  
NONSIGNIFICANT WASTEWATER  
DISCHARGE PERMIT APPLICATION**

SP6 16140

Agency Use:

Permit No: \_\_\_\_\_

Code No: \_\_\_\_\_

Data Entry By: \_\_\_\_\_

**PLEASE PRINT OR TYPE**

APPLICANT BUSINESS NAME: \_\_\_\_\_ PHONE: \_\_\_\_\_

BUSINESS ADDRESS: \_\_\_\_\_ CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_

BUSINESS OWNER: \_\_\_\_\_ PHONE: \_\_\_\_\_

MAILING ADDRESS: \_\_\_\_\_ CITY: \_\_\_\_\_ STATE: \_\_\_\_\_ ZIP: \_\_\_\_\_

CONTACT PERSON: \_\_\_\_\_ TITLE: \_\_\_\_\_

NATURE OF BUSINESS: (auto repair, car wash, machine shop, painting, battery dealer, etc.)

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

Does your facility conduct automotive servicing operations that involve the exchange or replacement of fluids (e.g. oil, transmission or brake fluid, radiator coolant etc.)? YES NO

Does your facility have any floor drains? YES NO

Does your facility have a steam cleaner? YES NO

Does your facility wash vehicles on site? YES NO

**If generated, how do you dispose of the following:**

Grease, oil and sand interceptor contents \_\_\_\_\_

I hereby affirm that all information furnished is true and correct  
to the best of my knowledge.

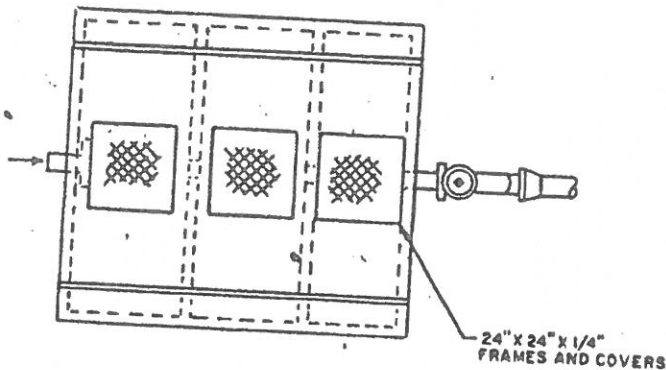
\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

Public Works Department  
Quality Assurance Division  
7579 Ave. 288  
Visalia CA 93277  
(559) 713-4487

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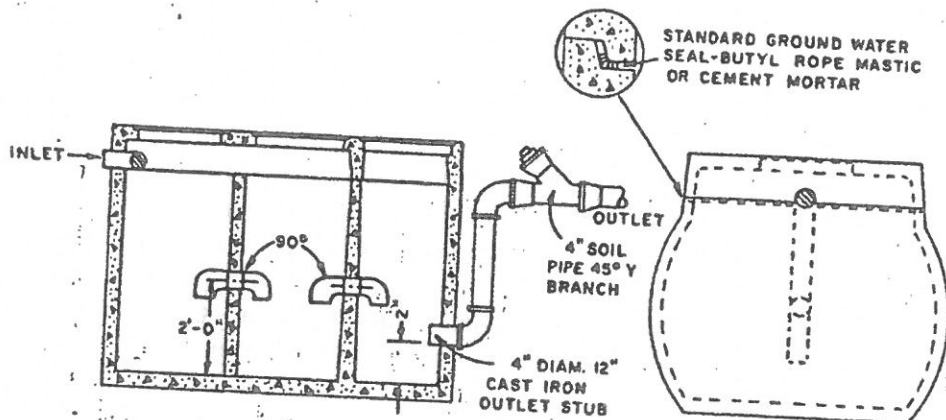


PLAN VIEW

**TYPICAL PRECAST 1000 GAL.**  
NO SCALE

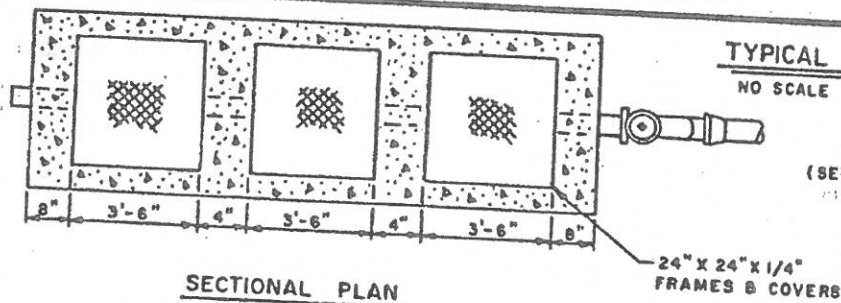
(SEE DETAIL S-9A FOR NOTES)

SPA 16140



LONGITUDINAL SECTION

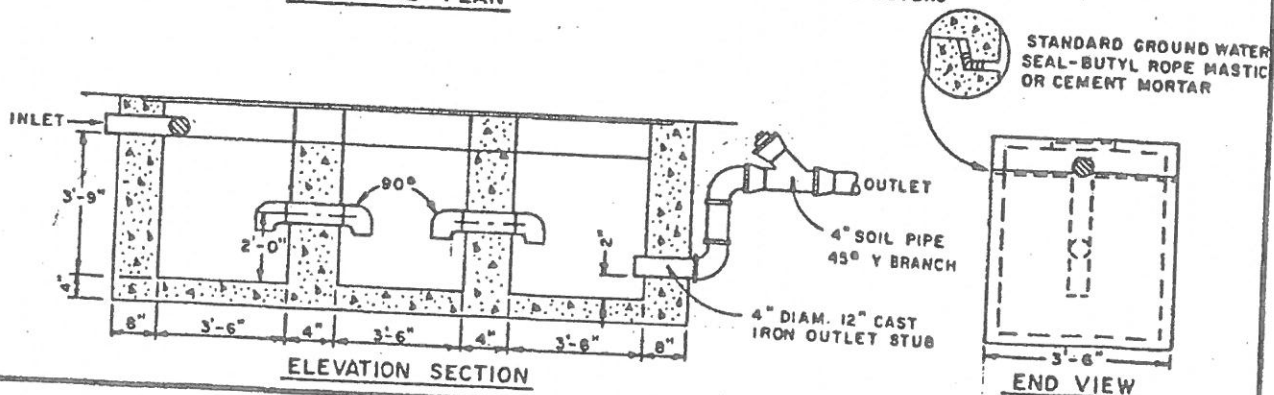
END VIEW



SECTIONAL PLAN

**TYPICAL POUR-IN-PLACE**  
NO SCALE  
**1000 GAL.**

(SEE DETAIL S-9A FOR NOTES)



ELEVATION SECTION

END VIEW

Approved by Visalia City Council on \_\_\_\_\_  
DATE

**City of Visalia**  
**Improvement Standards**

**Standard Outside Industrial Sand,  
Grease & Oil Interceptor**

REVISIONS

**S-9**

**CITY OF VISALIA**  
**ORDINANCE 13.08**

**13.08.570     Traps required.**

Grease, oil and sand traps shall be provided when, in the opinion of the City, they are necessary for the protection of the sewerage system from liquid wastes containing grease in excessive amounts, or any flammable wastes, sand and other harmful ingredients; except that such traps shall not be required for buildings used solely for residential purposes. Such traps shall be required for example, on discharges from all service stations, automotive repair garages, car washes, restaurants, eating establishments and food preparation establishments, and such other commercial or industrial establishments as the city may designate. (Prior code § 4254)

**13.08.580     Construction of traps.**

All traps shall be of a type and capacity approved by the city, and shall be so located as to be readily and easily accessible for cleaning and inspection. Restaurant traps shall be gas-tight, of a type approved for restaurant use by the division of building safety. Traps for all other facilities, including service stations and garages, shall be in accordance with the adopted plan of the city for such traps or shall be the approved equal thereof as determined by the director. (Prior code § 4255)

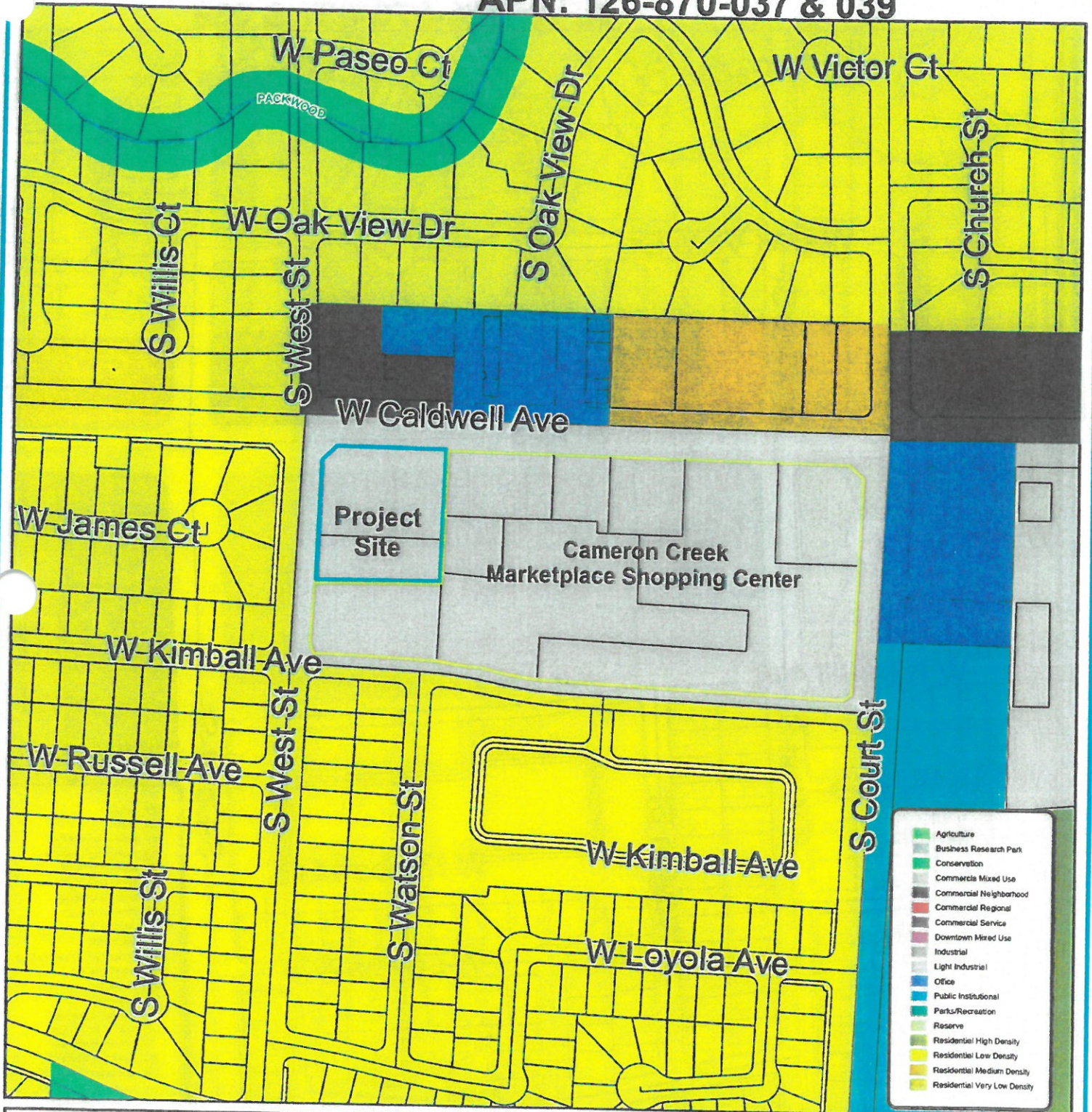
**13.08.590     Maintenance of traps.**

When installed, all grease, oil and sand traps shall be maintained by the owner, at owner's expense, in continuously efficient operation at all times. (Prior code § 4256)



# Conditional Use Permit No. 2017-22

APN: 126-870-037 & 039



## General Plan Land Use Map

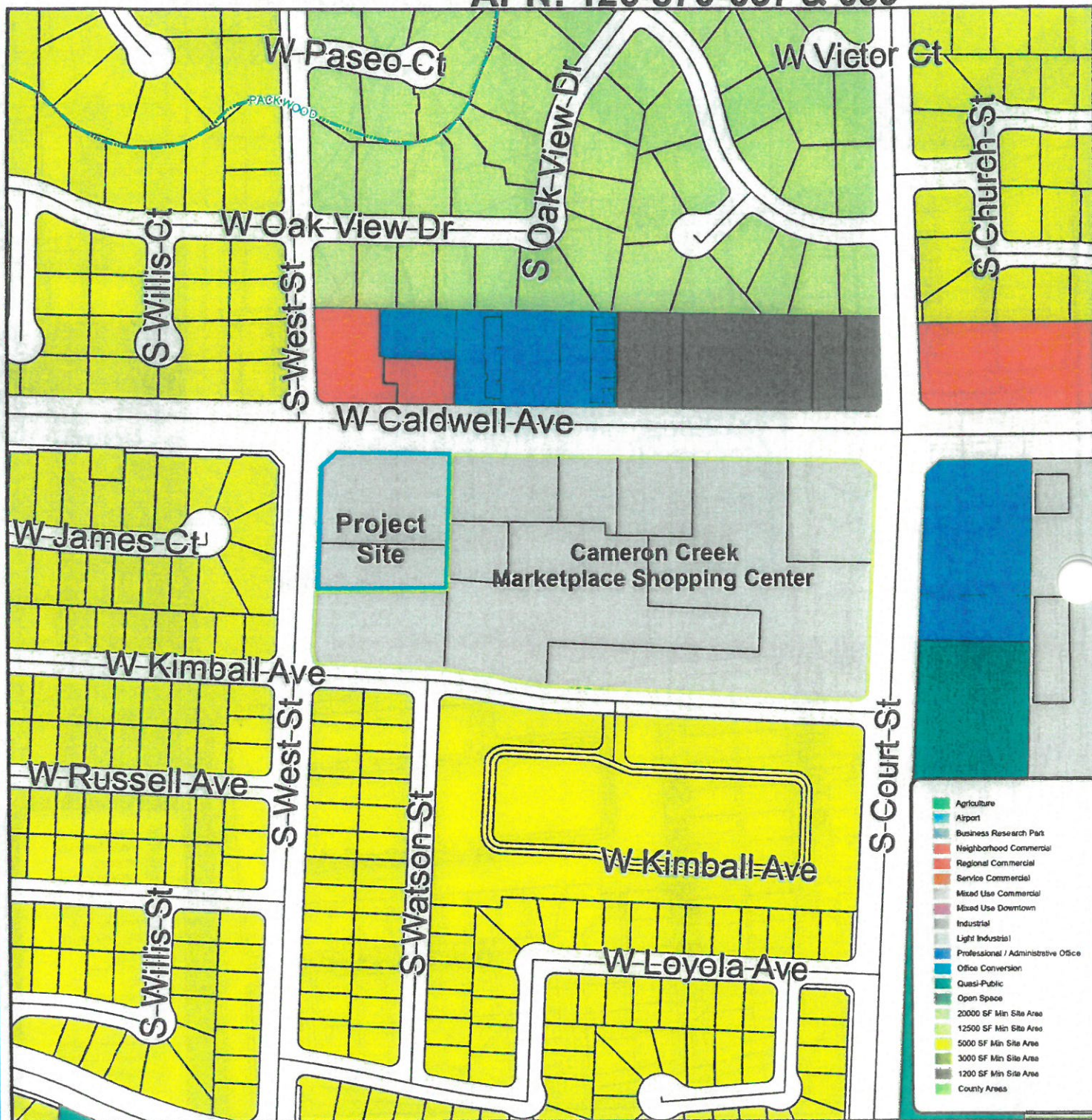
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# Conditional Use Permit No. 2017-22

APN: 126-870-037 & 039



## Zoning Map

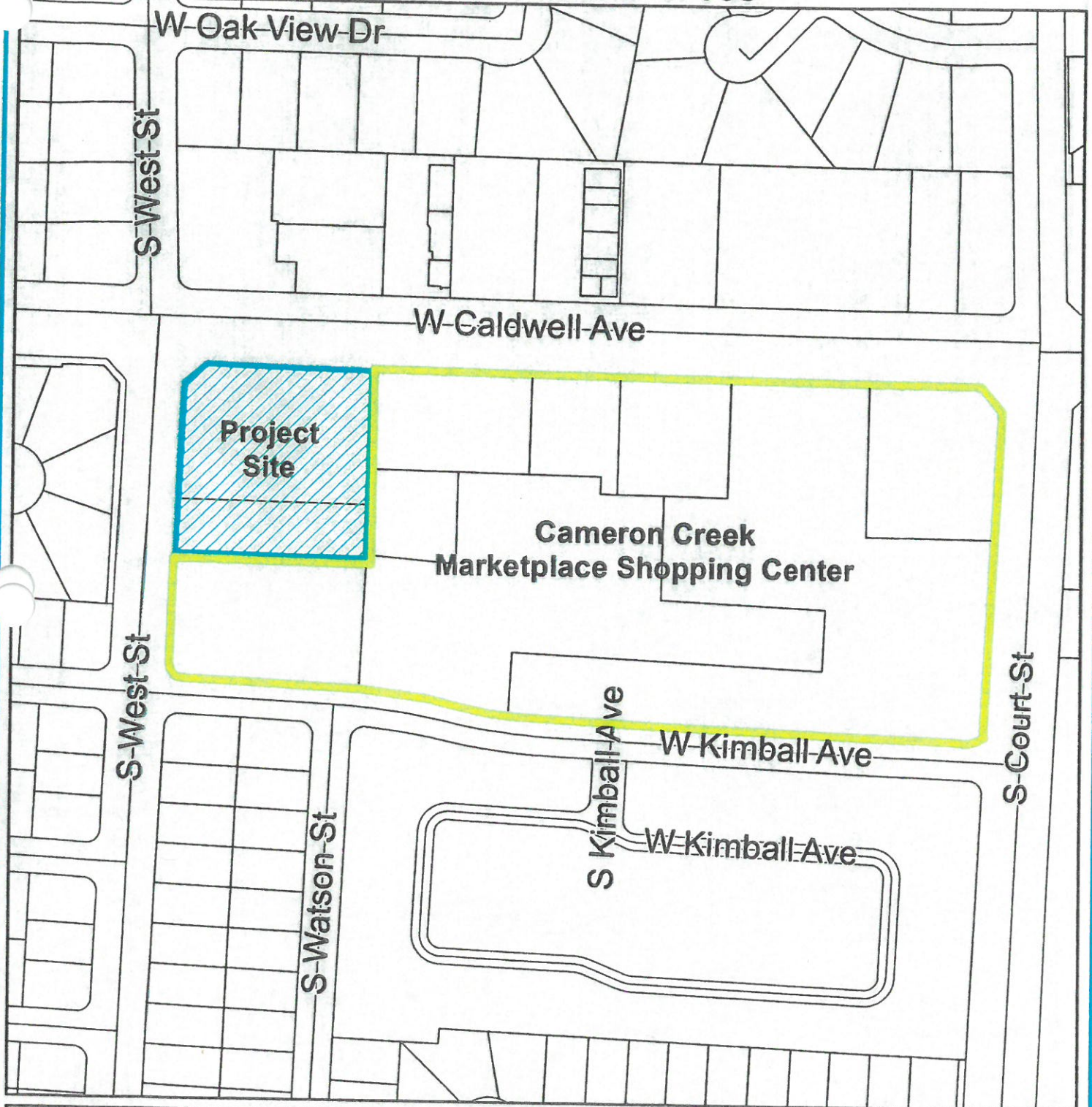
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# Conditional Use Permit No. 2017-22

APNs: 126-870-037 & 039



## Vicinity Map

100 50 0 100 200 Feet





# Conditional Use Permit No. 2017-22

APN: 126-870-037 & 039



## Aerial Map

200 100 0 200 400 Feet

